
Appendix N
West Roseville Specific Plan

WEST ROSEVILLE

Specific Plan

DRAFT

September 15, 2003



Draft

WEST ROSEVILLE SPECIFIC PLAN

Prepared for:



Prepared by:



September 15, 2003

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1 INTRODUCTION

1.1 OVERVIEW

The West Roseville Specific Plan (WRSP) guides the development of approximately 3,162 acres located to the west of Fiddymont Road. The WRSP is planned primarily as a residential community with an overall mix and intensity of uses similar to that found in adjacent portions of the City. The project incorporates a unique mixed-use village center, forming the centerpiece of the community. The WRSP also provides for recreation, open space, employment and educational opportunities available to residents both within and outside the Plan Area.

Emphasis has been placed on creating a vibrant, comprehensively planned community: one that generates a sense of place for residents and users, complements the existing community, ensures realization of City policies, and defines Roseville's western edge.

Given the unique nature of the WRSP, emphasis has been placed on creating a vibrant, comprehensively planned community: one that generates a sense of place for residents and users, complements the existing community, ensures realization of City policies, and defines Roseville's western edge.

1.2 THE SPECIFIC PLAN TOOL

The West Roseville Specific Plan is the primary land use, policy and regulatory document used to guide development of the project area. The Specific Plan establishes a development framework for land use, affordable housing, resource protection, circulation, utilities and services, implementation and design. The intent is to promote the systematic and orderly development of the Plan Area. All subsequent development projects and related activities in the WRSP area are required to be consistent with this Specific Plan.



Village Center

1.3

RELATIONSHIP TO CITY PLANS & REGULATIONS

General Plan

The West Roseville Specific Plan implements the goals and policies of the City of Roseville General Plan, and augments these goals and policies by providing specific direction to reflect conditions unique to the project and Plan Area. The General Plan serves as the long-term policy guide for the physical, economic and environmental growth of Roseville. The City's core values are the foundation of the General Plan and the underlying basis for its vision and direction. The WRSP is incorporated by reference as a component of the City's Land Use Element, and is consistent with the General Plan.

West Roseville Guiding Principles

The West Roseville Guiding Principles were adopted by the City Council in May 2001 to supplement the General Plan by identifying the City's expectations for any potential development proposed west of Roseville, including the WRSP. The intent of the Guiding Principles is to ensure that new development meets or exceeds the City's policies, standards and expectations, and does not unduly impact services to existing residents. Issues addressed include: fiscal health; community planning; community identity; infrastructure; health, safety and security; services and organizations; recreational opportunities; citizen involvement; and quality of life. The West Roseville Specific Plan is consistent with and furthers the intent of the Guiding Principles.

City-County Memorandum of Understanding

In 1997, the City of Roseville and Placer County entered into a Memorandum of Understanding (MOU) to promote interagency communication and to foster cooperative land use planning. The MOU applies to a "Transition Area" west of Fiddymont Road and north of Baseline Road. Approximately 95-percent of the WRSP is located within the MOU area (see Plan Setting & Influences, Figure 2-1). The MOU specifies requirements for processing development proposals within the Transition Area including provisions for City-County consultation and review, application submittal, mitigation of impacts and minimum development standards. The WRSP was processed in accordance with the City-County MOU.

1.4**SPECIFIC PLAN RELATED DOCUMENTS** **Environmental Impact Report**

The *Environmental Impact Report for the West Roseville Specific Plan and Sphere of Influence Amendment* (EIR), was certified concurrent with adoption of the West Roseville Specific Plan. The EIR, prepared in accordance with the California Environmental Quality Act (CEQA), examines the environmental impacts of the plan and the changes in the environment that would result from its implementation. The EIR evaluates the WRSP and associated Sphere of Influence (SOI) amendment and annexation at a project-specific level, and evaluates amending the sphere of influence for the remaining MOU area at a program level. The EIR serves as the base environmental document for purposes of evaluating subsequent entitlements associated with the WRSP.

 Development Agreements

The West Roseville Specific Plan property owners (Westpark Associates and Signature Properties, Inc.) have executed individual development agreements with the City of Roseville in accordance with applicable state and local codes. The project development agreements vest development rights, set forth obligated infrastructure improvements and dedication requirements, secure the timing and methods for financing improvements, and specify other performance obligations as related to development of the WRSP. The development agreements constitute legal and binding contracts between the City of Roseville, the property owners and their assigned successors in interest.

2 PLAN SETTING & INFLUENCES

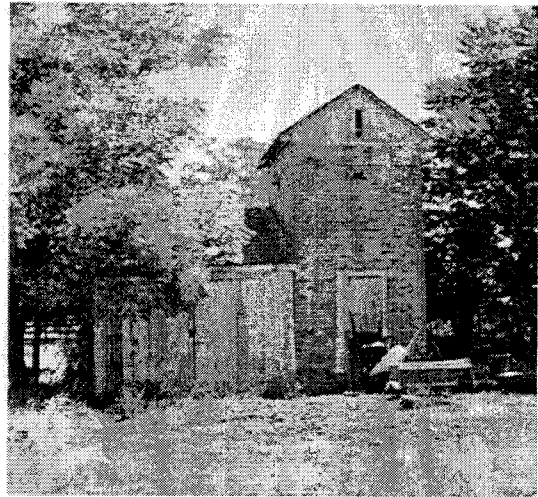
2.1 PROJECT LOCATION

The West Roseville Specific Plan is located west of Fiddymment Road, approximately 1-mile north of Baseline Road. A majority of the 3,162-acre site is within the City-County MOU Transition Area. Approximately 167 acres (5%) of the WRSP, which is designated as permanent open space, is located west of the MOU Transition Area. The WRSP project location and MOU Area are reflected on Figure 2-1.

2.2 PRE-SPECIFIC PLAN SETTING

Site Conditions

At the time of WRSP approval, the Plan Area was primarily undeveloped, with previous uses consisting of limited agricultural enterprises including grazing, dry farming and poultry operations. Several residences and other structures associated with past and ongoing agricultural activities were located in the central and northern portions of the site. These include the historic *Fiddymment Ranch House* and outbuildings. While agricultural operations decreased over time, a portion of a working pistachio orchard and seasonal livestock grazing still existed on site.

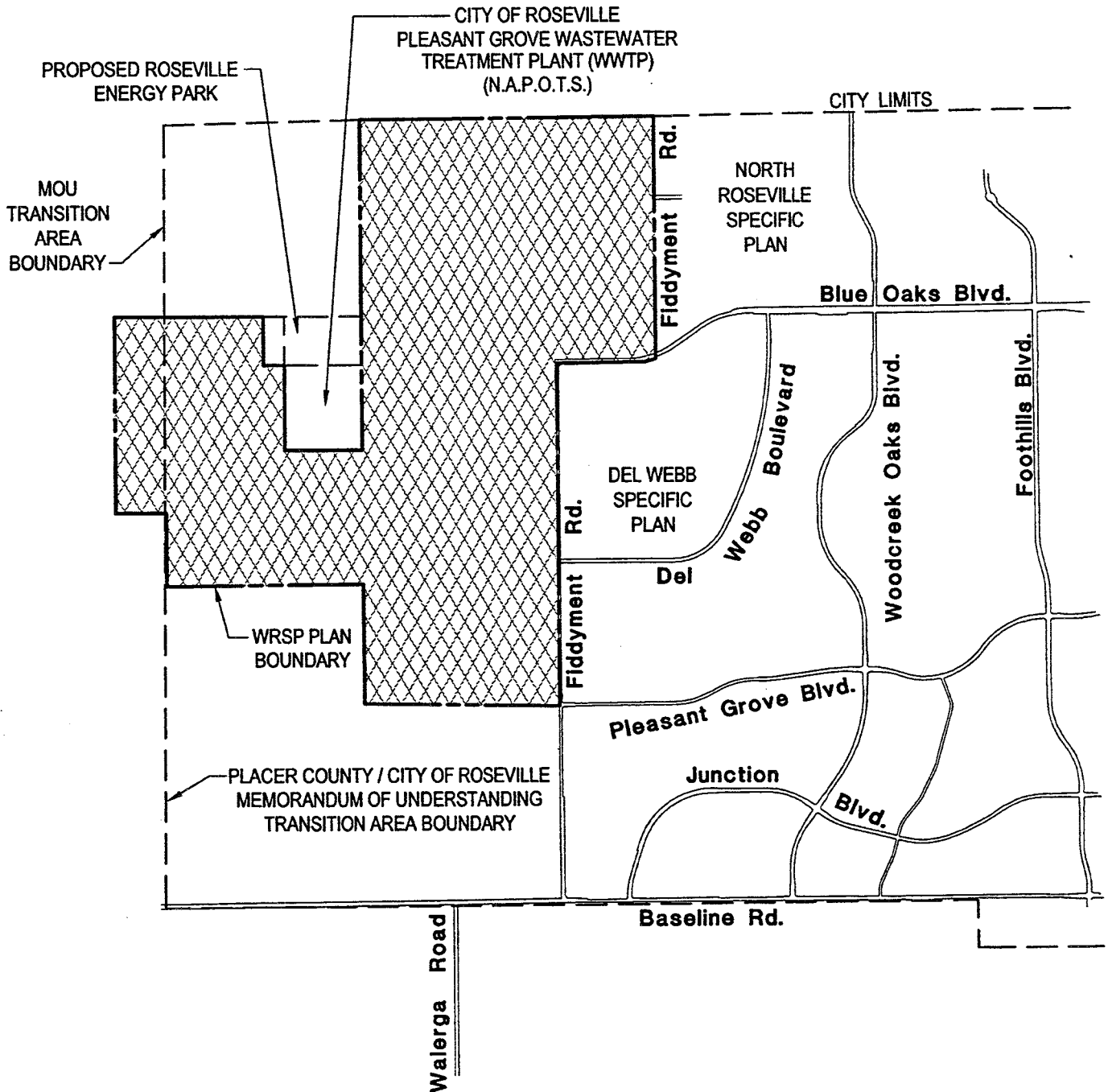


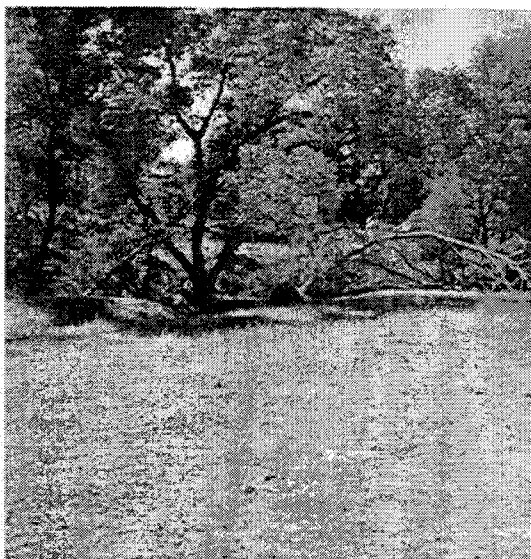
Fiddymment Ranch Smokehouse

Gently rolling topography and large open annual grassland areas defined the undeveloped character of the WRSP. Kaseberg Creek, Curry Creek and Pleasant Grove Creek (main and southern branches) traversed the property, with oak woodlands lining portions of the creek corridors. Clusters of seasonal wetlands, including vernal pools, were dispersed throughout the site. The WRSP has targeted a majority of the creek corridors, associated woodlands, and a portion of the seasonal wetlands and historic structures, for preservation in permanent open space/park use.

Figure 2-1

Project Location, City/County MOU Area





Grassland/Oak Woodland Interface

□ Adjacent Uses

Similar to the undeveloped WRSP environment, lands to the north, south and west consist primarily of agricultural and rural residential uses. To the east, existing and planned neighborhoods are found in the City's Del Webb and North Roseville Specific Plan areas. The Pleasant Grove Regional Wastewater Treatment Plant (PGWWTP), and other potential intensive public uses (including the proposed Roseville Energy Park), are adjacent to and partially surrounded by the central portion of the WRSP. A 1,000-foot non-residential buffer encumbers the WRSP to the south, east and west of the PGWWTP.

2.3

FACTORS THAT INFLUENCE THE SPECIFIC PLAN

The basic form of the WRSP is influenced by a number of factors including City policy and desires, adjacent uses, facility needs, environmental resources, circulation and infrastructure connections, market demand and fiscal realities. Some of the key WRSP influences include:

- **City Policy** – The need to ensure consistency with and implementation of General Plan Policy and the West Roseville Guiding Principles.
- **Smart Growth Principles** – The desire to incorporate smart growth principles in the WRSP including the creation of a traditionally modeled Village Center with a compact form and strong interconnectivity between uses.
- **Adjacent Residential Neighborhoods** - The intent to ensure compatibility with residential and related uses in the adjacent Del Webb and North Roseville Specific Plans through inclusion of similar types and intensities of uses along the eastern edge of the WRSP.
- **Adjacent Pleasant Grove Regional Wastewater Treatment Plant and Roseville Energy Park** – The requirement to ensure compatibility with the PGWWTP through inclusion of appropriate uses within the

1000-foot non-residential buffer area, and need to adequately buffer other adjacent intensive public uses including the proposed Roseville Energy Park.

- **Regional Sports Park** – The desire to create a regional sports park to accommodate citywide need and utilize a portion of the PGWWTP non-residential buffer area.
- **Schools** - The need to provide for a new high school site to house students generated both within and outside the WRSP, and the opportunity to support joint use with the adjacent Regional Sports Park and nearby Fiddymment Park. In addition, the need to provide for new elementary schools and a middle school to house students from the WRSP and provide additional joint use opportunities.
- **Western Edge Treatment** – The interest in creating an identifiable western edge of the City of Roseville through inclusion of substantial permanent open space areas.
- **Natural Resources** - The existence of and opportunity to ensure preservation of significant resources including oak woodlands, creek corridors, vernal pool concentrations and seasonal wetlands in open space and park uses.
- **Arterial and Infrastructure Connections** – The ability to connect to and extend major circulation corridors, Blue Oaks Boulevard and Pleasant Grove Boulevard, and associated infrastructure.
- **Fiddymment Road** - The opportunity to provide a new alignment for Fiddymment Road north of Blue Oaks Boulevard to improve safety and traffic flow.
- **Market Demand** – The desire to respond to the short-term buildout of residential inventory in the City, and to accommodate projected long-term residential market demand.
- **Fiscal Responsibility** - The need to create a project that includes a mix of uses and facilities that are fiscally feasible to implement and do not negatively impact the City's General Fund.

The above factors create opportunities and constraints, in the context of identified City values, that guide the general shape of the WRSP as further defined in Community Form & Planning Principles (Section 3).

3 COMMUNITY FORM & PLANNING PRINCIPLES

3.1 COMMUNITY FORM

The West Roseville Specific Plan is designed as a residential community supplemented by a mix of support and employment uses. The WRSP balances the various factors that influence the plan to create a master planned community grounded in solid planning principles: one that will be desirable and functional for future residents and businesses, and will provide new opportunities and benefits accessible to all of Roseville.

The Village Center is envisioned as the heart of the WRSP, a destination where residents will meet, shop, eat, recreate, obtain services and socialize.

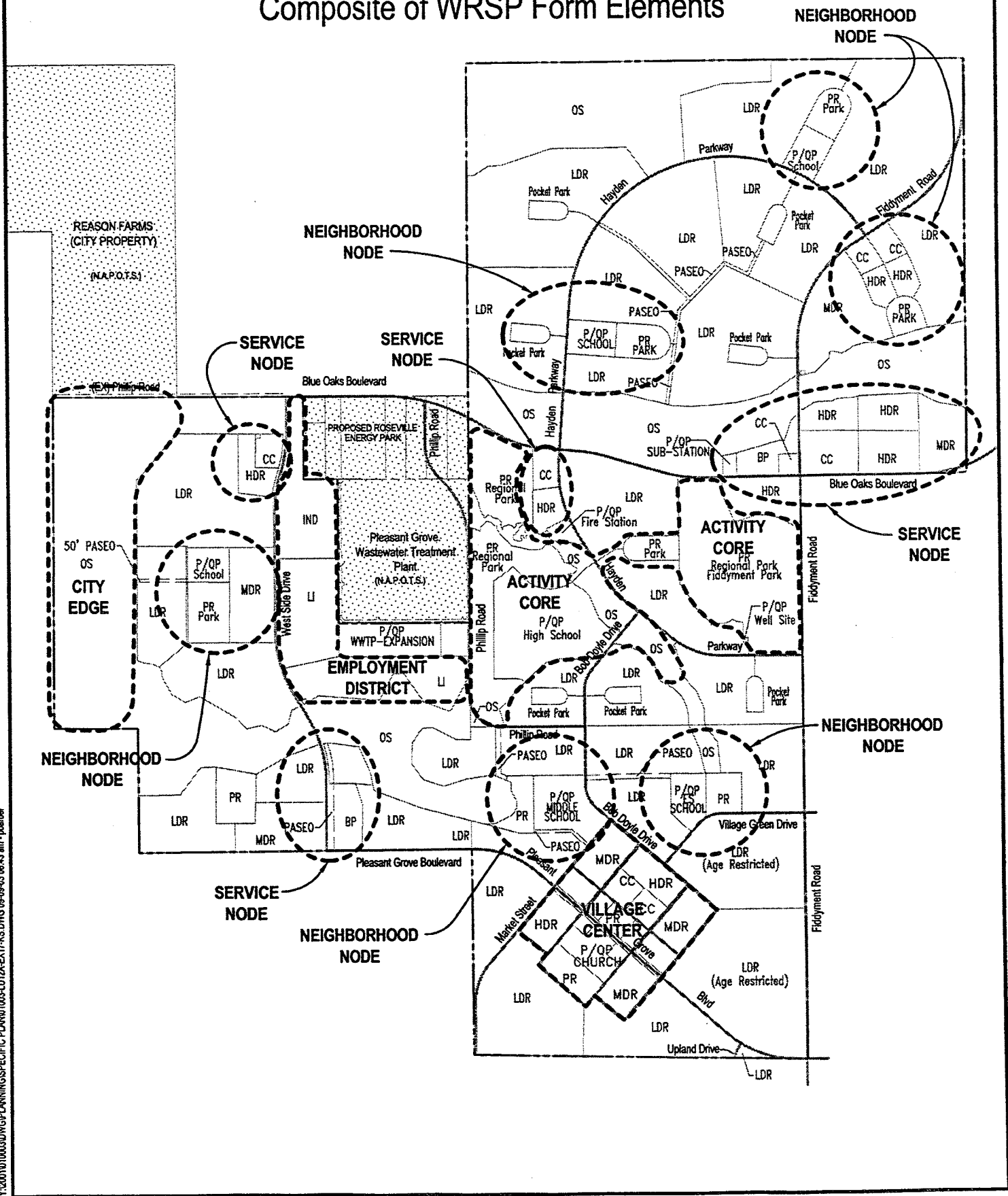
The primary elements that comprise the form of the WRSP land use plan include: the Community Focal Points (Village Center and Activity Core); Residential Neighborhoods; a hierarchy of Service and Neighborhood Nodes; the Employment District; and the City Edge. A composite of the WRSP form elements is reflected on Figure 3-1.



Village Center

Figure 3-1

Composite of WRSP Form Elements



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Community Focal Points

The Village Center and Activity Core are to be a central focus of activity, and will have a highly visible community presence.

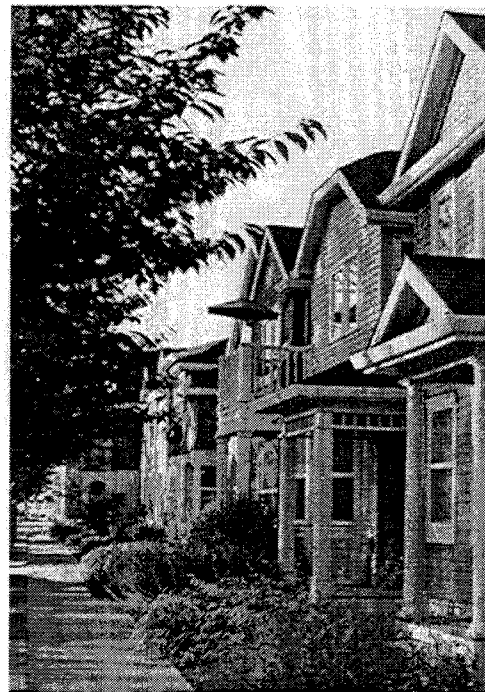
Village Center - The Village Center is planned as a unique and diverse mixed-use hub of activities. The Village Center is envisioned as the heart of the WRSP, a destination where residents will meet, shop, eat, recreate, obtain services and socialize. The anticipated mix of uses may include retail, restaurant, service, office, public, theater, church, school, park and high/medium density housing. Emphasis is placed on the pedestrian, rather than the auto, and on consistency with the City's General Plan Pedestrian District level of service policy. The Village Center is modeled towards a traditional urban town center rather than a suburban shopping center.

Activity Core - The City's Regional Sports Park is planned east of the Pleasant Grove Wastewater Treatment Plant, encompassing a portion of the required 1000-foot non-residential buffer. Immediately to the east of the Regional Sports Park is a planned high school site, and further to the east and close by, Fiddymt Park. The central proximity of these facilities, along with the inclusion of multiple vehicular, pedestrian and bicycle connection points, are intended to facilitate accessibility between facilities and to nearby residents. Combined, the park, school and adjacent open space areas generate a distinct central core of active and passive recreation, education, joint use opportunities, and community activity. The Activity Core will draw users from both within and outside the WRSP.

Residential Neighborhoods

Low density single-family residential is the predominant land use within the WRSP, and a defining characteristic of the community. The Residential Neighborhoods surround the Activity Core with approximately 40% of Plan Area units to the north and east of the Activity Core, and 60% to the south and west.

Neighborhoods include a mix of low, medium and high density residential uses consistent with the character of the City. Schools and parks are located in neighborhoods within walking distance of most residences. Medium and high- density residential is incorporated, proximate to services and recreational areas and to provide a separation between single-family



Residential Neighborhoods

residential and more intense land uses. A variety of housing styles similar to that found elsewhere in the City are planned, including affordable housing and designated age restricted neighborhoods.

Service and Neighborhood Nodes

Outside of the Village Center and Activity Core, service uses and community facilities are dispersed throughout the WRSP in a hierarchy of Service and Neighborhood Nodes. The WRSP is designed to create interconnectivity between the various nodes and the surrounding neighborhoods. These linkages include pedestrian and bicycle pathways along adjacent open space, paseos and roadway corridors. In most cases, high-density residential uses have been sited adjacent to or in close proximity to the service nodes.



Service Nodes

Service Nodes— Retail, office and other commercial/service uses are provided in Service Nodes within the WRSP. The Service Nodes consist primarily of commercial centers located along major circulation corridors. In most cases, high-density residential use has been sited adjacent to or in close proximity to the service nodes. Two Business Professional parcels have been included that may accommodate small office complexes intended to provide services to Plan Area residents.

Neighborhood Nodes - Located internally within the residential areas, the Neighborhood Nodes generally consist of a park combined with an elementary school. Where feasible, the Neighborhood Nodes are located adjacent and connected to open space areas. The Neighborhood Nodes act as a local activity amenity within each neighborhood. Neighborhood streets are planned to be organized around the nodes to provide easy vehicle and pedestrian access, and to establish the node as the visual center of the neighborhood.

Employment District

Employment opportunities, consisting of industrial and light industrial uses, are planned to the south and west of the PGWWTP. These uses are intended to provide employment potential within the City. The Employment District has good regional access via Blue Oaks Boulevard, Pleasant Grove Boulevard and West Side Drive, and expands the City's job base and industrial economic development potential. Approximately 3,726 jobs (1,575 of those industrial/light industrial, 931 business professional and 1,220 commercial) are projected within the WRSP. Land uses within the industrial and light industrial area are restricted within the 1,000-foot non-residential buffer to ensure compatibility with the PGWWTP.

City Edge

The WRSP represents the planned western extent of development in the City of Roseville. As a result, the Specific Plan has been configured to include a substantial open space buffer (267 acres) along its western edge. In addition to visually defining the western limits of the City, the open space area creates a transition between urban uses in Roseville and uses in unincorporated Placer County. The City Edge open space buffer may accommodate resource preservation/mitigation programs and other City sponsored activities.



City Edge

3.2 PLANNING PRINCIPLES

General:

1. Create a comprehensively planned residential based community balanced by a mix of employment, commercial, business professional, service, recreation, open space, and public uses.
2. Provide a logical and orderly extension of the City of Roseville that is compatible with surrounding land uses, that complements the pattern and intensity of existing development in the City and provides new benefits to the City.
3. Satisfy City policies, regulations and expectations as defined in the General Plan, West Roseville Guiding Principles, City-County MOU, Zoning Ordinance, Improvement and Construction Standards and other applicable plans, documents and programs adopted by the City.
4. Aid the City in meeting its recognized obligation to accommodate a percentage of future population growth in the region by increasing the residential holding capacity by 8,430 residential units in an area identified in the City's feasibility analyses as feasible for such development.
5. Ensure compatibility with the adjacent Pleasant Grove Wastewater Treatment Plant and other potential future intensive public uses, including the proposed Roseville Energy Park, through inclusion of appropriate uses within the 1000 foot non-residential buffer area around the plant.
6. Offer a low-density age-restricted community as a component to the WRSP to compliment the age-restricted community in the Del Webb Specific Plan and to respond to the market preferences and needs of senior adults as they relate to type, size, cost of housing, and recreational and social amenities.

Uses and Form:

7. Shape a physical form and character of development that is functional and creates a sense of place that will:
 - a) Establish an identifiable western edge of the City of Roseville through inclusion of contiguous permanent open space areas;

- b) Enhance Roseville's supply of and reputation for quality housing that provides a diversity of housing opportunities available to residents from a wide range of economic levels and all stages of life;
- c) Organize neighborhoods to be identifiable, walkable and to incorporate gathering places, such as parks and schools, for neighborhood activities and interaction;
- d) Encourage the concentration of recreation, education, service, public and community activities, as focal points of the community including:
 - Development of a mixed use Village Center that incorporates "Smart Growth" principles and that is modeled as a traditional hub of services, activity and people gathering;
 - Creation of a central Activity Core that includes two regional parks (Regional Sports Park and Fiddymont Park) and a high school; and
 - Provision of adequate school facilities to serve students generated in the WRSP.
- e) Expand the City's employment base through the inclusion of commercial, industrial, light industrial and business professional uses; and
- f) Balance development with resource protection, including preservation of cultural resources, significant creek corridors, sensitive habitat, oak woodlands and wetlands in interconnected permanent open space.

Infrastructure and Services:

- 8. Provide a safe and efficient circulation system that interconnects uses and provides opportunities for alternate transportation options.
- 9. Create an interconnected Class 1 bikeway system that links the Plan Area with the rest of the City as well as residential neighborhoods, open space, parks, schools, service and activity areas.
- 10. Ensure that the WRSP development pattern and circulation system do not preclude an alignment for proposed Placer Parkway.

11. Provide and maintain services and infrastructure that satisfy City standards, integrate with existing and planned facilities and connections, consider potential development in the remainder of the City-County MOU area, and do not diminish services to existing residents of the City.

Implementation:

12. Ensure that the WRSP includes a mix of uses and facilities that are fiscally feasible and implement funding mechanisms to avoid negative impacts to the City's General Fund.
13. Phase development to link the provision of services with the timing of development to respond to the short-term buildout of residential land use inventory in the City and to accommodate projected long-term residential market demand.

4 LAND USE PLAN & REGULATIONS

4.1 OVERVIEW

The West Roseville Specific Plan land use plan includes a blend of residential, service, employment, open space and public uses. The Plan Area will house approximately 20,810 residents and 3,726 employees. WRSP land uses are reflected on Figure 4-1, and are summarized on Table 4-1 below. A parcel-by-parcel land use, zoning, and unit summary is included on Table 4-2. Given its unique nature, a more detailed discussion of the proposed Village Center is included as Section 10, Village Center Plan.

Table 4-1: Land Use Summary

General Plan Designation	Land Use	Acres	% of Total Acres	Units
RESIDENTIAL				
LDR	Low Density Residential	1354.6	42.8%	4,842
LDR	LDR – Age Restricted	146.9	4.7%	710
MDR	Medium Density Residential	142.9	4.5%	1,064
HDR	High Density Residential	109.7	3.4%	1,774
<i>Subtotal</i>		1,754.1	55.5%	8,390
SERVICE/EMPLOYMENT				
CC	Community Commercial	48.5	1.5%	40
BP	Business Professional	19.6	0.6%	
LI	Light Industrial	74.2	2.3%	
IND	General Industrial	34.3	1.1%	
<i>Subtotal</i>		176.6	5.5%	40
OPEN SPACE/PUBLIC				
OS	Open Space Preserve	670.1	21.3%	
OS	Paseo	14.5	0.4%	
P/R	Park & Recreation	251.0	8.0%	
P/R	Pocket Park	19.4	0.6%	
P/QP	Public/Quasi-Public	148.0	4.7%	
	Road Right of Way	128.3	4.0%	
<i>Subtotal</i>		1,231.3	39.0%	
TOTAL		3,162.0	100%	8,430

Table 4-2: Land Use, Zoning & Units by Parcel

Parcel #	Land Use	Zoning	Acres	Density	Units ¹	Parcel #	Land Use	Zoning	Acres	Density	Units ¹
F-1	LDR	R1/DS	41.4	4.2	174	W-1	LDR (Age-Restricted)	RS/DS	85.4	4.8	410
F-2	LDR	R1/DS	32.8	3.9	129	W-2	LDR (Age-Restricted)	RS/DS	61.5	4.9	300
F-3	LDR	R1/DS	23.5	4.6	108	W-3	LDR	RS/DS	38.1	5.4	205
F-4	LDR	R1/DS	38.2	2.9	109	W-4	LDR	R1/DS	31.4	4.8	150
F-5	LDR	R1/DS	51.5	2.9	148	W-5	LDR	R1/DS	23.0	3.3	75
F-6	LDR	R1/DS	67.8	2.9	197	W-6	LDR	R1/DS	22.8	3.1	70
F-7	LDR	R1/DS	46.4	3.3	152	W-7	LDR	R1/DS	27.9	4.1	115
F-8	LDR	R1/DS	25.4	3.1	78	W-8	LDR	R1/DS	42.3	3.8	160
F-9	LDR	R1/DS	78.6	3.7	287	W-9	LDR	R1/DS	31.9	3.0	95
F-10	LDR	R1/DS	88.5	3.1	270	W-10	LDR	R1/DS	54.1	5.0	270
F-11	LDR	R1/DS	57.4	3.1	180	W-11	LDR	R1/DS	32.3	3.9	125
F-12	LDR	R1/DS	34.3	3.4	117	W-12	LDR	R1/DS	18.9	3.4	65
F-13	LDR	R1/DS	66.5	3.6	240	W-13	LDR	R1/DS	17.0	3.5	60
F-14	LDR	R1/DS	99.5	2.9	290	W-14	LDR	R1/DS	31.7	3.6	115
F-15	LDR	R1/DS	45.2	2.9	130	W-15	LDR	R1/DS	27.6	2.9	80
F-16	MDR	R1/DS	37.0	5.0	185	W-16	MDR	RS/DS	20.6	7.8	160
F-17	MDR	RS/DS	17.3	10.1	174	W-17	LDR	R1/DS	46.0	4.6	210
F-19	LDR	R1/DS	40.9	3.9	158	W-18	LDR	R1/DS	71.2	3.9	280
F-20	HDR	R3	6.9	19.7	136	W-19	MDR	RS/DS	21.9	7.5	165
F-21	HDR	R3	16.8	14.3	240	W-20	LDR	R1/DS	1.1	0	0
F-22	HDR	R3	9.7	11.3	110	W-28	HDR	R3	9.0	19.4	175
F-23	HDR	R3	12.1	13.2	160	W-29	HDR (Senior)	R3	8.0	18.8	150
F-24	HDR	R3	12.7	15.8	200	W-30	CC	CC	4.0		
F-25	HDR	R3	5.5	18.2	100	W-50	Park	P/R	14.1		
F-26	HDR	R3	5.7	17.2	98	W-51	Park	P/R	12.1		
F-30	Business Prof.	BP	9.1			W-52	Park	P/R	7.9		
F-31	CC	CC	13.4			W-55	Park	P/R	8.2		
F-32	CC	CC	4.9			W-60	Industrial	M2/SA	34.3		
F-33	CC	CC	4.4			W-61	Light Industrial	M1/SA	35.9		
F-34	CC	CC	5.4			W-62	Light Industrial	M1/SA	38.3		
F-35	CC	CC	2.0			W-63	Business Prof.	BP	10.5		
F-50	Park	P/R	7.8			W-70	Elementary School	P/QP	8.0		
F-51	Park	P/R	8.9			W-71	WWTP	P/QP	20.1		
F-52	Park	P/R	5.9			W-73	Middle School	P/QP	19.9		
F-53	Park	P/R	6.0			W-74	Water Tank	P/QP	5.1		
F-54	Fiddymnt Park	P/R	91.0			W-75	Elementary School	P/QP	10.0		
F-55	Sports Complex	P/R	29.8			W-76	Well	P/QP	0.1		
F-56	Sports Complex	P/R	45.8			W-77	Well	P/QP	0.1		
F-65	Substation	P/QP	1.6			W-81	Open Space	OS	267.0		
F-66	Well	P/QP	0.1			W-82	Open Space	OS	5.2		
F-67	Well	P/QP	0.1			W-83	Open Space	OS	77.2		
F-70	Elementary School	P/QP	8.1			W-84	Paseo	OS	0.9		
F-71	Elementary School	P/QP	8.7			W-85	Paseo	OS	1.1		
F-72	High School	P/QP	53.0			W-87	Paseo	OS	2.6		
F-73	Fire Station	P/QP	3.1			W-88	Paseo	OS	1.5		
F-80	Open Space	OS	132.7			W-89	Paseo	OS	2.3		
F-83	Open Space	OS	54.9			W-200	Right of Way	R/W	53.0		
F-84	Open Space	OS	81.2			W-VC-21	VC-MDR	R3/DS	16.8	8.6	145
F-85	Open Space	OS	26.4			W-VC-22	VC-MDR	R3/DS	16.8	7.1	120
F-86	Open Space	OS	12.2			W-VC-24	VC-MDR	R3/DS	12.5	9.2	115
F-87	Open Space	OS	10.3			W-VC-25	VC-HDR	R3/DS	12.4	19.4	240
F-88	Open Space	OS	16.7			W-VC-26	VC-HDR	R3/DS	10.0	16.5	165
F-89	Open Space	OS	0.8			W-VC-32	VC-CC	CC/SA-WR	7.2	2.8	20
F-90	Paseo	OS	1.1			W-VC-33	VC-CC	CC/SA-WR	7.2	2.8	20
F-91	Paseo	OS	0.9			W-VC-53	VC-Park	P/R	9.1		
F-92	Paseo	OS	3.0			W-VC-54	VC-Village Green	P/R	3.5		
F-93	Paseo	OS	1.1			W-VC-72	VC-Church	P/QP/SA-WR	10.9		
F-94	Pocket Park	R1/DS	3.2			W-VC-201	VC-Right of Way	R/W	14.1		
F-95	Pocket Park	R1/DS	3.0								
F-96	Pocket Park	R1/DS	3.0								
F-97	Pocket Park	R1/DS	3.0								
F-98	Pocket Park	R1/DS	2.4								
F-99	Pocket Park	R1/DS	2.4								
F-100	Pocket Park	R1/DS	2.4								
F-200	Right of Way	R/W	47.0								
Fiddymnt Unit Total											4,170
Westpark Unit Total											4,260

1. The WRSP allows for Minor Density Adjustments (Section 4.3). Confirm current unit counts with the City of Roseville Planning Department.

4.2 PERMITTED USES & DEVELOPMENT STANDARDS

Land uses within the West Roseville Specific Plan will be implemented by the application of permitted, conditionally permitted and/or administratively permitted uses designated by the zoning district applied to each parcel. Except as otherwise provided in this Specific Plan, permitted uses, development standards, processing requirements and other regulations are as specified by the City of Roseville Zoning Ordinance (Municipal Code Chapter 19).

In accordance with the Zoning Ordinance, the WRSP has applied the Development Standards (DS) and Special Area (SA) overlay zones to customize development standards and/or permitted uses of general zone districts where needed. A summary of zoning districts and overlay zones applied by the WRSP is included in Table 4-3. Guidance relating to the application of these overlay zones is provided in this section.

The WRSP Design Guidelines (Section 12) include additional detail to be considered in the design, review and approval of individual projects. The WRSP Design Guidelines supplement the Citywide Design Guidelines and other City requirements by identifying considerations unique to the character and development objectives of the Plan Area. Development within the Plan Area is required to comply with the WRSP Design Guidelines, and all applicable City requirements.

Table 4-3: Summary of WRSP Applied Zoning Districts

Land Use	Applied Zoning Districts	Permitted Uses	Development Standards
RESIDENTIAL USES			
LDR – <i>Low Density Residential</i>	R1/DS – RS/DS	Per Zoning Ordinance	Modified by WRSP ¹
MDR – <i>Medium Density Residential</i>	RS/DS	Per Zoning Ordinance	Modified by WRSP ¹
HDR – <i>High Density Residential</i>	R3	Per Zoning Ordinance	Per Zoning Ordinance
SERVICE AND EMPLOYMENT USES			
VC – <i>Village Center</i>	See Section 10	Modified by WRSP ²	Modified by WRSP ²
CC – <i>Community Commercial</i>	CC	Per Zoning Ordinance	Per Zoning Ordinance
BP – <i>Business Professional</i>	BP	Per Zoning Ordinance	Per Zoning Ordinance
LI – <i>Light Industrial</i>	M1/SA-WR	Modified by WRSP ³	Per Zoning Ordinance
IND – <i>General Industrial</i>	M2/SA-WR	Modified by WRSP ³	Per Zoning Ordinance
OPEN SPACE AND PUBLIC USES			
OS – <i>Open Space</i>	OS	Per Zoning Ordinance	Per Zoning Ordinance
P/R – <i>Parks & Recreation</i>	P/R	Per Zoning Ordinance	Per Zoning Ordinance
P/QP – <i>Public/Quasi-Public</i>	P/QP	Per Zoning Ordinance	Per Zoning Ordinance

1. Section 4.2.
 2. Village Center Plan (Section 10).
 3. Section 4.2, Table 4-6.

RESIDENTIAL USES □

Residential uses in the West Roseville Specific Plan consist primarily of single-family neighborhoods. Approximately two-thirds of WRSP units are planned for Low Density Residential, including designated age-restricted housing. Remaining units in the WRSP are proposed for Medium Density Residential and High Density Residential (inclusive of units within the Village Center). The WRSP provides for internal park and school sites (Neighborhood Nodes), trail linkages and paseos, separated sidewalks, unique lighting fixtures, alternate garage configurations and other elements to enhance the neighborhood environment.

Medium and High Density Residential uses are also incorporated within the Village Center. See Section 10, Village Center Plan, for further discussion.



Residential Neighborhood

Low Density Residential (LDR)															
<i>Density Range:</i> <i>Applied Zoning Districts:</i>	0.5 to 6.9 dwelling units per acre R1/DS: Single-Family Residential/Development Standard Overlay RS/DS: Small Lot Residential/Development Standard Overlay														
<i>Description:</i>	Single-family detached housing on conventional lots are anticipated as the primary product type. The applied R1 zoning also permits certain related and compatible uses inclusive of half-plexes. Portions of the LDR units (Parcels W-1 and W-2) are designated for age-restricted housing limited to residents age 55 and above in accordance with California Civil Code Section 51.3.														
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.														
<i>Development Standards:</i>	<p>The Design Standards (DS) overlay zone has been applied to LDR parcels as follows:</p> <p>R1/DS: Single-Family Residential/Development Standard Overlay – The R1/DS zone district has been applied to allow variations to development standards at lower LDR densities (5.0 du/ac and below). The intent of these variations is to provide additional flexibility to accommodate single family detached product types and to facilitate the use of separated sidewalks to enhance the local street scene and overall neighborhood environment. Variations have been identified for the following Zoning Ordinance Standards:</p> <table style="width: 100%; border: none;"> <thead> <tr> <th style="text-align: left; border-bottom: 1px solid black;"><u>Standard Single Family</u></th> <th style="text-align: left; border-bottom: 1px solid black;"><u>With Detached Sidewalk</u></th> </tr> </thead> <tbody> <tr> <td>■ Front yard setback</td> <td>■ Lot size</td> </tr> <tr> <td>■ Average rear lot depth</td> <td>■ Corner lot width</td> </tr> <tr> <td></td> <td>■ Front setback</td> </tr> <tr> <td></td> <td>■ Side setback for corner</td> </tr> <tr> <td></td> <td>■ Average rear lot depth</td> </tr> <tr> <td></td> <td>■ Coverage</td> </tr> </tbody> </table> <p>Permitted variations to lot standards are included on Table 4-4 with illustratives included on Figure 4-2. No further entitlement to revise R1/DS standards is required if consistent with Table 4-4 and the WRSP Design Guidelines.</p> <p>RS/DS: Small Lot Residential/Development Standard Overlay – The RS/DS zone district has been applied to allow variations to development standards for age restricted housing (Parcels W-1 and W-2) and at upper LDR densities (5.1-6.9 du/ac). The intent of these variations is to facilitate the use of separated sidewalks and to accommodate various single-family product types. This includes the unique requirements associated with age restricted housing which is anticipated to include a mix of product types and lot sizes. Variations have been identified for the following Zoning Ordinance Standards:</p>	<u>Standard Single Family</u>	<u>With Detached Sidewalk</u>	■ Front yard setback	■ Lot size	■ Average rear lot depth	■ Corner lot width		■ Front setback		■ Side setback for corner		■ Average rear lot depth		■ Coverage
<u>Standard Single Family</u>	<u>With Detached Sidewalk</u>														
■ Front yard setback	■ Lot size														
■ Average rear lot depth	■ Corner lot width														
	■ Front setback														
	■ Side setback for corner														
	■ Average rear lot depth														
	■ Coverage														

<p><i>Development Standards (cont.)</i></p>	<p><u>Standard Small Lot Single Family</u></p>	<p><u>With Separated Sidewalk</u></p>
	<ul style="list-style-type: none"> ■ Rear setback and useable open space 	<ul style="list-style-type: none"> ■ Lot size ■ Corner lot width ■ Front setback ■ Side setback for corner ■ Rear setback and useable open space
	<p>Permitted variations to lot standards are included on Table 4-5 with illustratives included on Figure 4-3. No further entitlement to revise RS/DS standards is required if consistent with Table 4-5 and the WRSP Design Guidelines.</p>	
	<p>Parcel F-4 - An existing residence (Corin Residence) is located towards the eastern edge of Parcel F-4 that will be incorporated into any subsequent subdivision map. Inclusion of this existing residence may result in the creation of some unique parcel configurations and sizes. The R1/DS has been applied to Parcel F-4 to prohibit the parcel for the existing residence, and any other larger parcels created by the initial subdivision map for Parcel F-4, from being split or further parcelized in the future.</p>	



Residential Neighborhood

Table 4-4: Modified R1 Development Standards and Modified R1 Development Standards for Separated Sidewalks (R1/DS)¹

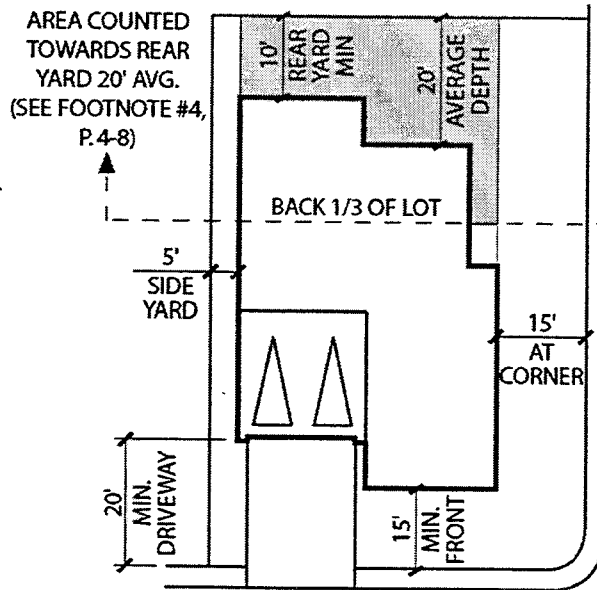
	Standard Single Family	With Separated Sidewalk ²
Lot Size (minimum)		
Area, Interior Lot	6,000 sq. ft.	<i>5,700 sq. ft.</i>
Area, Corner Lot	7,500 sq. ft.	<i>6,650 sq. ft.</i>
Width, Interior	60 ft.	60 ft.
Width, Corner	75 ft.	<i>70 ft.</i>
Permitted Density (maximum per lot)		
Residential Density	1 dwelling; 1 second unit	1 dwelling; 1 second unit
Setbacks (minimum) See Chapter 19.22 of Zoning Ordinance for accessory structure setbacks		
Front ³	<i>15 ft. to living space or side wall of garage;</i> 20 ft. minimum driveway depth	<i>10 ft. to single-story living space or side wall of garage;</i> <i>15 ft. to 2-story living space</i> 20 ft. min. driveway depth
Sides ⁵	5 ft. interior; 15 ft. street side on corner	5 ft. interior; <i>10 ft. street side on corner</i>
Rear ⁵	<i>20 ft. average depth;⁴</i> 10 ft. minimum	<i>20 ft. average depth;⁴</i> 10 ft. minimum
Coverage (maximum)		
Site Coverage	35% for 2 story; 45% for 1 story	Interior Lot: <i>37% for 2 story;</i> <i>50% for 1 story</i> Corner Lot: <i>39% for 2 story;</i> <i>50% for 1 story</i>
Height (maximum)		
Height Limit	35 ft.	35 ft.

1. Items in italics represent modifications to City Zoning Ordinance Standards.
2. Sidewalk separated from back of curb by 5-foot planter strip.
3. Front setback measured from back of walk.
4. Depth to be determined by calculating the average perpendicular distance between the full width of the primary structure and the rear lot line (as defined by the City of Roseville Zoning Ordinance). Only those portions of the primary structure within the rear one-third of the lot may be used for purposes of calculating the rear yard setback (see Figure 4-2).
5. LDR and MDR parcels directly abutting West Side Drive (within large lot parcels W-13, W-16, W-17, W-18 and W-19) shall include a 15-foot wide supplemental setback easement in addition to the required setback. See Circulation Plan, Section 7.

Note: Variations of the above and other product types may be permitted subject to processing of a Design Review Permit for Residential Subdivisions (DRRS) concurrent with the approval of a tentative subdivision map and review of product type.

Figure 4-2: Illustratives for Modified R1 Development Standards and Modified R1 Development Standards for Separated Sidewalks (R1/DS)

Standard Single-Family



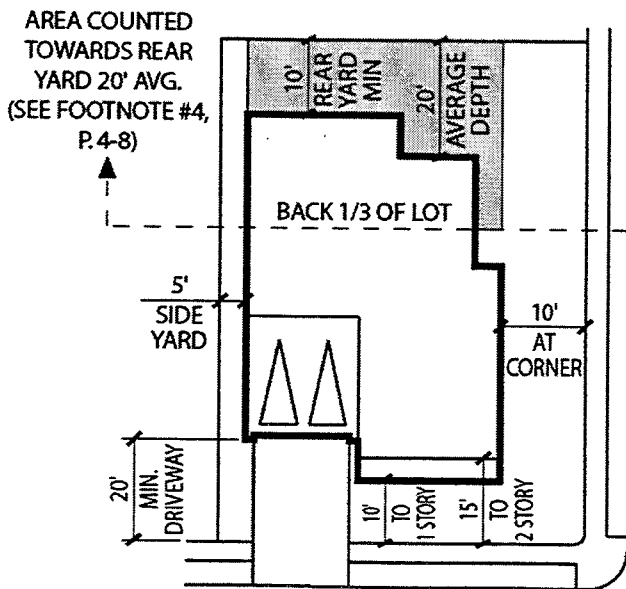
INTERIOR LOT:

AREA: 6000sq ft
 WIDTH: 60 ft
 SITE COVERAGE: 35% for 2 story
 45% for 1 story

CORNER LOT:

AREA: 7500sq ft
 WIDTH: 75 ft
 SITE COVERAGE: 35% for 2 story
 45% for 1 story

Single-Family with Separated Sidewalk



INTERIOR LOT:

AREA: 5700sq ft
 WIDTH: 60 ft
 SITE COVERAGE: 37% for 2 story
 50% for 1 story

CORNER LOT:

AREA: 6650sq ft
 WIDTH: 70 ft
 SITE COVERAGE: 39% for 2 story
 50% for 1 story

Table 4-5: Modified RS Development Standards and Modified RS Development Standards for Separated Sidewalks (RS/DS)¹

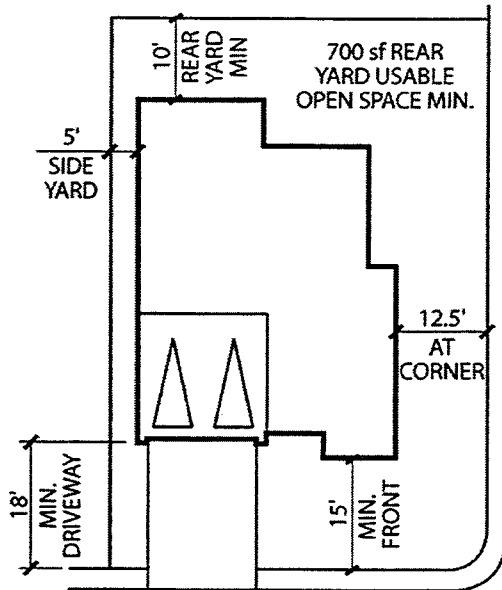
	Standard Single Family	With Separated Sidewalk ²
Lot Size (minimum)		
Area, Interior Lot	4,500 sq. ft.	4,275 sq. ft.
Area, Corner Lot	5,500 sq. ft.	4,710 sq. ft.
Width, Interior	45 ft.	45 ft.
Width, Corner	55 ft.	50 ft.
Permitted Density (maximum per lot)		
Residential Density	1 dwelling; 1 second unit	1 dwelling; 1 second unit
Setbacks (minimum) See Chapter 19.22 of Zoning Ordinance for accessory structure setbacks		
Front ³	15 ft. to living space and side wall of garage; 18 ft. minimum driveway depth with roll-up garage	<i>10 ft. to single story living space or side wall of garage; 15 ft. to 2 story living space; 18 ft. minimum driveway depth with roll-up garage</i>
Sides ⁵	5 ft. interior; 12.5 ft. street side on corner	<i>5 ft. interior; 10 ft. street side on corner</i>
Rear ⁵	<i>10 ft. minimum with minimum useable open space of 700 s.f.⁴</i>	<i>10 ft. minimum with minimum useable open space of 700 s.f.⁴</i>
Coverage (maximum)		
Site Coverage	None ⁴	None ⁴
Height (maximum)		
Height Limit	35 ft.	35 ft.

1. Items in italics represent modifications to City Zoning Ordinance Standards.
2. Sidewalk separated from back of curb by 5-foot planter strip.
3. Front setback measured from back of walk.
4. The rear and side yards may be utilized to meet the minimum usable open space provided the minimum dimension, measured perpendicular to the applicable rear or side yard lot is *ten (10)* feet. Maximum coverage is a function of lot size, required setbacks and useable open space.
5. LDR and MDR parcels directly abutting West Side Drive (within large lot parcels W-13, W-16, W-17, W-18 and W-19) shall include a 15-foot wide supplemental setback easement in addition to the required setback. See Circulation Plan, Section 7.

Note: Variations of the above and other product types may be permitted subject to processing of a Design Review Permit for Residential Subdivisions (DRRS) concurrent with the approval of a tentative subdivision map and review of product type.

Figure 4-3: Illustratives for Modified RS Development Standards and Modified RS Development Standards for Separated Sidewalks (RS/DS)

Standard Single-Family



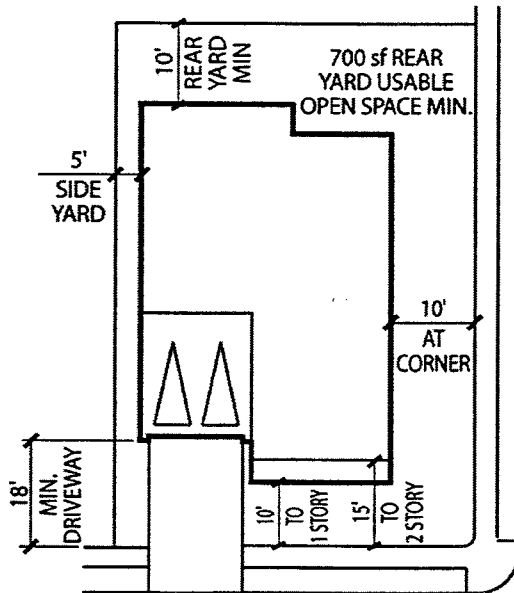
INTERIOR LOT:

AREA: 4500sq ft
WIDTH: 45 ft

CORNER LOT:

AREA: 5500sq ft
WIDTH: 55 ft

Single-Family with Separated Sidewalk



INTERIOR LOT:

AREA: 4275sq ft
WIDTH: 45 ft

CORNER LOT:

AREA: 4710sq ft
WIDTH: 50 ft

Medium Density Residential (MDR)	
<i>Density Range:</i>	7.0 to 12.9 dwelling units per acre
<i>Applied Zoning District:</i>	RS/DS: Small Lot Residential/Development Standard Overlay
<i>Description:</i>	Medium density residential (MDR) land use provides an opportunity to accommodate a variety of housing types. This density allows for single family detached housing, however, alternative housing types may also be accommodated within the medium density range. Housing types may include cluster housing, duplexes/half-plexes, zero lot line, townhomes, courtyard housing, bungalow courts and other attached and detached housing products. The incorporation of innovative housing types are strongly encouraged by the WRSP to provide a variety of housing alternatives, maximize common open space/recreation opportunities, and enhance the neighborhood environment. In addition to housing, the RS zoning also permits similar and related compatible uses.
<i>Permitted Uses:</i>	As Specified in the City of Roseville Zoning Ordinance
<i>Development Standards:</i>	<p>Given the variety of housing types that can be accommodated by medium density land use, the Design Standards overlay zone has been applied to all MDR parcels. The intent is to allow additional flexibility in the placement, design and configuration of buildings and lots for individual projects. In addition, similar to LDR parcels, the use of separated sidewalks, alternative garage configurations and other elements are encouraged.</p> <p>Specific development standards will be established through the processing of a Design Review Permit for Residential Subdivisions (DRRS) concurrent with the approval of a tentative subdivision map, or as part of the Design Review process if a subdivision map is not required, for each designated RS/DS parcel. See Circulation Plan, Section 7, for additional supplemental setback easement requirements for MDR parcels abutting West Side Drive (parcels W-16 and W-19). Within the Village Center, additional development standards apply to MDR (see Section 10, Village Center Plan).</p>
High Density Residential (HDR)	
<i>Density Range:</i>	13.0 dwelling units per acre and higher
<i>Applied Zoning District:</i>	R3: Attached Housing
<i>Description:</i>	High density residential (HDR) land use accommodates attached multi-family housing including apartments, townhomes and condominiums. The applied R3 zone also allows similar and compatible uses including community care facilities. High density residential may provide opportunities for either rental or purchase housing, and is a critical component in ensuring affordable housing opportunities in compliance with the City's Housing Element.
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As specified in the City of Roseville Zoning Ordinance. Within the Village Center, additional development standards apply to HDR (see Section 10, Village Center Plan).

SERVICE AND EMPLOYMENT USES



Service and employment uses in the West Roseville Specific Plan consist of commercial, office, general industrial and light industrial. Included is the Village Center, envisioned as the primary focal point of the community. The Specific Plan emphasizes compatibility and interconnectivity between uses. The WRSP Design Guidelines promote the creation of projects that are desirable, functional, secure, create a strong street presence, and incorporate elements (pathways, access connections, plazas, lighting elements, shading, etc.) that promote pedestrian activity.



Village Center

Village Center (VC)	
<p><i>Density Range:</i> <i>Typical FAR:</i> <i>Applied Zoning District:</i></p>	See Village Center Plan, Section 10
<p><i>Description:</i></p>	<p>The Village Center is planned to accommodate a broad mix and configuration of uses that form the commercial, service, social and activity focus for the WRSP. It is modeled after a traditional urban small town center rather than a suburban shopping complex. The Village Center will have a visible presence in the community and create a destination where residents will come to meet, shop, eat, recreate and obtain services. It is the gathering spot of the community: a district where preferences are given to the pedestrian and not the auto, and where interaction and socialization are primary goals and a catalyst for formal and informal activities.</p> <p>The Village Center will accommodate a variety of service, commercial and housing uses. Given the unique nature of the Village Center, and the importance it plays in the overall form of the WRSP, detailed provisions have been established to guide its development. These include unique land use, zoning, improvement and development standards, design guidelines and project processing provisions. Section 10, Village Center Plan, provides a detailed discussion and the requirements associated with this use.</p>
<p><i>Permitted Uses:</i></p>	See Village Center Plan, Section 10
<p><i>Development Standards:</i></p>	See Village Center Plan, Section 10

Community Commercial (CC)	
<i>Typical FAR:</i> <i>Applied Zoning District:</i>	20 to 40% CC – Community Commercial
<i>Description:</i>	<p>Community Commercial (CC) land use accommodates a mix of retail and service uses to meet the needs of the surrounding community. Organized in a series of Service Nodes, the CC land uses are located along major circulation routes, and are intended to be both pedestrian and auto oriented. In most cases, CC uses have been located adjacent to or nearby to HDR uses. While the permitted uses for each CC parcel are the same, the size, configuration and location of each will impact the ultimate character and mix of uses:</p> <ul style="list-style-type: none"> ■ Parcel F-31 is of a typical size and configuration to accommodate a conventional community/neighborhood retail center, and provides opportunities to connect to adjacent HDR uses. ■ Parcels F-32 and F-33 are two adjacent smaller CC parcels located next to HDR uses. These parcels provide the opportunity to create less intense local serving neighborhood centers. ■ Parcels F-34 is of medium size and provides the opportunity to include uses and designs that complement, orient toward and connect to the adjacent Regional Sports Park. ■ Parcel F-35 is a smaller-sized parcel adjacent to BP uses. ■ Parcel W-30 is a smaller-sized parcel and provides opportunity to connect to adjacent HDR and City Edge open space.
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As specified in the City of Roseville Zoning Ordinance. Within the Village Center, additional development standards apply to CC (see Section 10, Village Center Plan).



Community Commercial Center

Business Professional (BP)	
<i>Typical FAR:</i>	20 to 40%
<i>Applied Zoning District:</i>	BP - Business Professional
<i>Description:</i>	Two Business Professional (BP) parcels (W-83 and F-30) have been designated in the WRSP. Both parcels may accommodate a small professional, administrative and/or medical office complex intended to provide services to Plan Area residents. Parcel F-30 is adjacent to and may orient onto open space uses. Parcel W-83 is adjacent to residential uses and partially encumbered by a power line easement, which may be used for parking.
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As specified in the City of Roseville Zoning Ordinance.

Light Industrial (LI) & General Industrial (IND)	
<i>Typical FAR:</i>	20 to 50%
<i>Applied Zoning Districts:</i>	M1/SA-WR – Light Industrial/Special Area Overlay M2/SA-WR – General Industrial/Special Area Overlay
<i>Description:</i>	Light Industrial (LI) and General Industrial (IND) uses form an Employment District adjacent to the Pleasant Grove Wastewater Treatment Plant (PGWWTP). In addition to providing employment opportunities, these uses ensure compatibility with the PGWWTP 1000-foot non-residential buffer. Light Industrial land use accommodates manufacturing, processing, assembly, high technology, research and development, storage and other uses that are compatible operating in relatively close proximity to commercial and residential uses. General Industrial allows a broader and more intense variety of uses that typically need to be buffered from more sensitive uses.
<i>Permitted Uses:</i>	The Special Area (SA) overlay has been applied to all WRSP Light and General Industrial zones. The SA overlay modifies WRSP M1 and M2 permitted, conditionally permitted and administratively permitted uses to ensure compatibility with the PGWWTP and other area uses. Uses have been restricted to limit those use types with the highest potential for conflict with the daily operations of the WWTP. The revised use table for the M1/SA-WR and M2/SA-WR zones is included as Table 4-6.
<i>Development Standards:</i>	As Specified in the City of Roseville Zoning Ordinance.
<i>Air Quality:</i>	At the time industrial users are proposed in the WRSP, the individual users will be required to obtain a Permit to Operate from the Placer County Air Pollution District. If the District determines that the operation may exceed toxic air contaminant thresholds, air quality modeling and/or additional environmental review may be required to ensure that measures are incorporated into the project that reduce emissions to a less than significant level.

Table 4-6: M1/SA-WR and M2/SA-WR Permitted Uses

AGRICULTURAL AND OPEN SPACE USE TYPES	M1/SA-WR	M2/SA-WR
Agricultural	P	P
Resource Protection and Restoration	P	P

CIVIC USE TYPES	M1/SA-WR	M2/SA-WR
Community Assembly	CUP	CUP
Community Service	P	P
Essential Services	P	P
Intensive Public Facilities	-	CUP
Public Parking Services	P	P
Schools, Private Elementary and Secondary	-	-
Social Services Food Distribution	A/CUP (2)	A/CUP (2)
Food Service	A/CUP (3)	A/CUP (3)
Temporary Resident Shelter	-	-

RESIDENTIAL USE TYPES	M1/SA-WR	M2/SA-WR
Caretaker/Employee Housing	-	-
Congregate Residence	-	-

COMMERCIAL USE TYPES	M1/SA-WR	M2/SA-WR
Adult Business Establishment	CUP	CUP
Animal Sales and Services		
Kennels	P	P
Veterinary Clinics	P	P
Veterinary Hospital	P	P
Automotive and Equipment		
Automotive Rental	CUP	-
Automotive Repairs	P	CUP
Automotive Sales	-	-
Car Wash and Detailing	P	CUP
Commercial Parking	P	P
Heavy Equipment Rental, Repair and Sales	P	P
Gasoline Sales	P	P
Impound Yards	CUP	P
Broadcasting and Recording Studios	P	P
Building Materials Stores	P	P
Business Support Services	P	CUP
Commercial Recreation		
Indoor Entertainment	CUP	CUP
Indoor Sports and Recreation	P	P
Outdoor Entertainment	CUP	CUP
Outdoor Sports and Recreation	P	P
Large Amusement Complexes	CUP	CUP
Eating and Drinking Establishment, Convenience	CUP	CUP
Maintenance and Repair	CUP	CUP
Medical		
Substance Abuse Treatment Clinic	CUP	CUP

Commercial Use Types (continued)		M1/SA-WR	M2/SA-WR
Neighborhood Commercial		CUP	CUP
Nightclubs (1)		CUP	CUP
Nursery, Retail		P	P
Offices, Professional		CUP	CUP
Personal Services		CUP	CUP
Retail Sales and Services		-	-
Specialized Education and Training			
Vocational Schools		CUP	CUP
Specialty Schools		CUP	CUP
Storage, Personal Storage Facility		P	P

INDUSTRIAL USE TYPES		M1/SA-WR	M2/SA-WR
Day Care Center, Secondary (employees only)		CUP	-
Equipment and Materials Storage Yards		CUP	P
General Industrial		CUP	P
Hazardous Materials Handling		CUP	P
Laundries, Commercial		P	P
Light Manufacturing		P	P
Printing and Publishing		P	P
Recycling, Scrap and Dismantling			
Enclosed		P	P
Unenclosed		CUP	P
Research Services		CUP	CUP
Specialized Industrial		CUP	CUP
Wholesale and Distribution			
Light		P	P
Heavy		CUP	P

TRANSPORTATION AND COMMUNICATION USE TYPES		M1/SA-WR	M2/SA-WR
Antennas and Communication Facilities (5)			
Developed Lot		P	P
Undeveloped Lot		CUP	CUP
Heliport		CUP	CUP
Intermodal Facilities (6)		P	P

All highlighted uses have been modified by the WRSP SA Overlay.

- (P) Principally Permitted
- (CUP) Permitted through approval of a Conditional Use Permit
- (A) Administratively Permitted
- (-) Not Permitted

Notes:

- (1) Additional Requirements contained in Chapter 19.49 of the City of Roseville Zoning Ordinance
- (2) Additional Requirements contained in Chapter 19.40 of the City of Roseville Zoning Ordinance
- (3) Additional Requirements contained in Chapter 19.39 of the City of Roseville Zoning Ordinance
- (4) Additional Requirements contained in Chapter 19.38 of the City of Roseville Zoning Ordinance
- (5) Additional Requirements contained in Chapter 19.34 of the City of Roseville Zoning Ordinance
- (6) Additional Requirements contained in Chapter 19.36 of the City of Roseville Zoning Ordinance

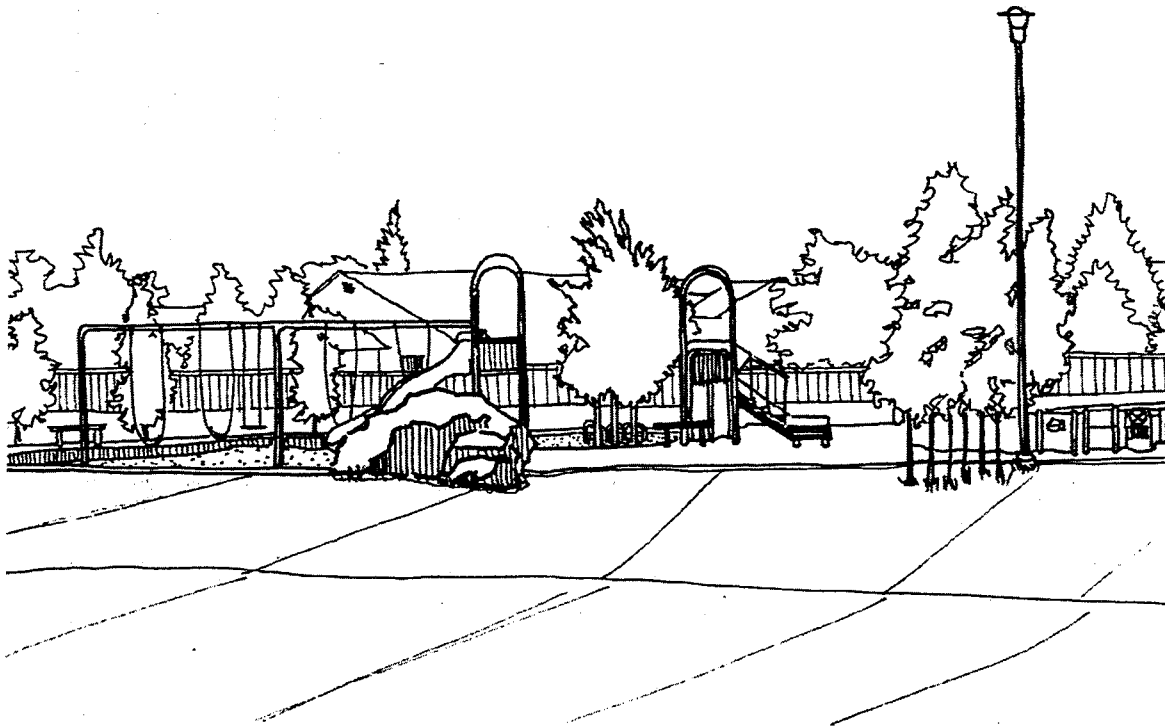
OPEN SPACE AND PUBLIC USES □

Over one-fifth of the West Roseville Specific Plan has been designated for open space use (open space preserve areas and paseos). In addition, approximately 251-acres of regional, community and neighborhood park sites are designated. The WRSP incorporates a variety of public/quasi-public uses including schools, fire station, electric substation, and expansion area for the PGWWTP site. Open space and public uses have been designated and sized consistent with General Plan policies and standards.

Park & Recreation and Public/Quasi-Public (church) uses are also incorporated within the Village Center. See Section 10, Village Center Plan, for further discussion.

Open Space (OS)	
<i>Applied Zoning District:</i>	OS - Open Space
<i>Description:</i>	<p>Open Space (OS) land use and zoning is generally applied to lands that are environmentally sensitive or otherwise significant due to habitat, hazards or natural features. Open space areas play a significant role in defining the visual character of the WRSP. They provide for passive recreation opportunities, preservation of significant resources, view sheds, flood water conveyance and retention, backbone sewer collection infrastructure, a community garden, resource mitigation, and can improve the interface between uses and define an edge to the City. In addition, open space areas may accommodate Class 1 bike path systems and are a key element in creating Plan Area interconnectivity.</p> <p>WRSP open space can be defined in two categories:</p> <ul style="list-style-type: none"> ▪ Open Space Preserve Areas generally correspond with those lands required to be preserved by accompanying Federal Clean Water Act Section 404 permits. These open space preserve areas incorporate creek corridors and floodplains, seasonal wetlands, and oak woodlands. Uses within open space preserve areas are limited to activities authorized under the accompanying 404 permit(s). The open space preserves may accommodate bike path, roadway, utility, drainage, other public facilities and maintenance for fire protection. Typical permitted uses may include trails, storm drainage facilities and passive recreation. A 2-acre community garden is planned within the open space on Parcel F-83. <p>Included in the open space preserve areas is the city edge open space. The city edge open space consists primarily of grassland and wetland areas located along the western edge of the WRSP. This open space preserve area visually defines the western limits of the City and creates a transition between urban uses in Roseville and planned rural and agricultural uses in unincorporated Placer County.</p>

	<ul style="list-style-type: none"> ▪ Paseos are “created” landscaped open space corridors established primarily to provide linkages through neighborhoods to parks, schools and commercial areas as an alternative to major roadway links. The paseos are typically 25 to 75 feet in width and may function as a wide landscape corridor along one side of a residential street, or as a greenway extending through neighborhoods. Paseos will include Class 1 bike paths and may accommodate other informal recreational activities. <p>Additional information relating to preserve areas is included in Resource Management (Section 6), and on paseos in Public Services Plan (Section 8).</p>
<p><i>Permitted Uses:</i></p>	<p>As specified in the City of Roseville Zoning Ordinance and the Preserve Area Operations and Maintenance (O&M) Plan.</p>
<p><i>Development Standards:</i></p>	<p>As Specified in the City of Roseville Zoning Ordinance.</p>



Park

Park & Recreation (P/R)	
<i>Applied Zoning District:</i>	PR – Parks & Recreation
<i>Description:</i>	Park and Recreation (P/R) land use and zoning identify public parks in the WRSP. These include City neighborhood, community and regional parks providing a combination of active and passive recreational facilities. Neighborhood parks are located internally within neighborhoods, in some cases combined with school sites, forming Neighborhood Nodes of local activity. The larger public park facilities, the Regional Sports Park and Fiddymment Park, are key components that, along with the planned high school site, make up the WRSP Activity Core. In addition to formal park sites, the WRSP includes pocket parks. Pocket parks are small public recreation amenities located centrally within specific subdivisions. Pocket parks are not subject to credit towards meeting the General Plan park dedication requirement. Parks and recreation facilities are further described under Public Services Plan (Section 8).
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As Specified in the City of Roseville Zoning Ordinance.

Public/Quasi-Public (P/QP)	
<i>Applied Zoning District:</i>	P/QP – Public/Quasi-Public
<i>Description:</i>	Public/Quasi-Public land use and zoning is applied to municipal facilities as well as quasi-public uses where the size or character of such uses warrant an individual designation. In the WRSP, P/QP uses include public schools (four elementary schools, a middle school and high school), a church, a fire station, an electric substation site, water tank/recycling center site, well sites and a site for expansion of the PGWWTP. The municipal and school facilities are discussed in more detail in Public Services Plan and Utilities Plan (Sections 8 and 9).
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As Specified in the City of Roseville Zoning Ordinance.

4.3

MINOR RESIDENTIAL DENSITY ADJUSTMENTS

The West Roseville Specific Plan has assigned residential densities based on a plan level assessment of the constraints and opportunities of each large-lot Specific Plan parcel and anticipated long-term demand for various housing types. As individual residential projects are designed and processed over time, a more detailed assessment of site, market and other conditions will occur. It is anticipated that this process may result in the desire or need to adjust (reduce or increase) the number of units assigned to some large-lot residential parcels.

It is the intent of the WRSP to permit flexibility in adjusting the number of residential units allocated to any residential large lot parcel in response to market demand, subdivision and/or design review considerations, including but not limited to transfers which do not result in additional impacts to oak trees or other natural resources. To further this intent, units may be transferred between large lot residential parcels provided:

- a. The transfer and receiving parcels are within the WRSP and covered by the same development agreement.
- b. The cumulative increase or decrease in units resulting from the minor density adjustment does not change by more than twenty-percent (20%) the number of pretransfer units allocated to either the transfer or receiving parcel as established at the time of original approval of the Specific Plan.
- c. The transfer and receiving parcels have existing General Plan land use designations of LDR, MDR or HDR and the density adjustments do not result in densities on either the transfer or receiving parcel above or below the existing land use designations.
- d. The transfer does not result in increased impacts on oak trees and does not preclude the ability of the parcels to conform to the WRSP Design Guidelines.
- e. The transfer does not result in increased impacts upon the transfer and receiving parcels beyond those identified in the WRSP EIR.
- f. The adjustments in density do not adversely impact planned infrastructure, roadways, schools, or other public facilities or Plan Area fee programs and assessment districts.

In addition to the above, the transfer of units from an HDR parcel to HDR, MDR, or LDR parcel(s) is permitted provided it also meets the following supplemental criteria:

- a. HDR units designated as affordable units and encumbered by Affordable Housing Development Agreements (or other form as approved by the City) are not eligible for density transfers.

- b. Units may only be transferred from an HDR parcel with a density of 18 units per acre or greater to another HDR parcel with a density of 18 units per acre or greater (Parcels F-20, F-25, W-25, W-28, W-29). In no case may such transfer result in the density of either parcel falling below 18 units per acre. These parcels may receive unit transfers from parcels with densities below 18 units per acre.

Minor density adjustments, if consistent with the above criteria, are contemplated by and within the intent of this Specific Plan and the WRSP EIR and will not require an amendment to the Specific Plan or the City General Plan.

To request a minor density adjustment, the owner or owners of both the transfer and receiving parcels shall submit an Administrative Permit to the Planning Director identifying the impacted parcels, designating the number of units being transferred and providing other documentation as required by the Planning Director to determine compliance with the above unit transfer criteria. Applicant shall also provide a revised Specific Plan Table 4-2 "Land Use, Zoning and Units by Parcel" reflecting the adjusted unit counts and densities. The revised table will be the official record tracking unit allocations to each large lot residential parcel.

If the Planning Director determines that the minor density adjustment is not consistent with the above criteria, the minor density adjustment shall be denied or may be referred or appealed to the Planning Commission for resolution. Any determination of consistency may, at the discretion of the Planning Director, be forwarded the Planning Commission for review. Applicant may request density adjustments that do not comply with the above criteria. Such requests shall require an amendment to the Specific Plan.

All unused units must be transferred prior to approval by the City of the last small lot final map or Design Review Permit for any residential large lot parcel. Any units designated by the Specific Plan to a large lot parcel that are not used by a tentative map/Design Review Permit or are not approved for transfer shall revert to the City unit pool and landowners shall have no subsequent claim to such units.

5 AFFORDABLE HOUSING PLAN

5.1 OVERVIEW

The City of Roseville General Plan Housing Element establishes a goal to provide decent, safe, adequate and affordable housing for all economic segments of the community. Given the nature of the housing market in Roseville and the South Placer area, it is a particular challenge to create housing opportunities affordable to middle- and low-income residents. Typically, such opportunities require market restrictions and/or subsidies. The City's Housing Element specifies that ten percent (10%) of all new housing units in the City be affordable to middle-, low-, and very low-income households.

The West Roseville Specific Plan proposes a mix of housing types that consist primarily of low density single-family residential neighborhoods. It is anticipated that, similar to other locations in Roseville, these neighborhoods will provide market rate housing affordable predominantly to moderate- and above moderate-income residents. The WRSP also includes medium density residential (MDR) and high density residential (HDR) housing dispersed in various locations within the Plan Area. These middle and high density parcels generally have lower per unit land and development costs and, therefore, provide the greatest opportunity to create affordable housing.

The WRSP affordable housing program focuses on the MDR and HDR parcels and has been structured to be consistent with and implement the General Plan affordable housing goal.

5.2 DEFINITION OF HOUSING AFFORDABILITY

Housing affordability is based on household income categories defined by the U.S. Department of Housing and Urban Development (HUD). The standard measure of affordability is the median household income calculated for the Sacramento Primary Metropolitan Statistical Area (SPMSA). All jurisdictions within the SPMSA, including Roseville, utilize the same basic income calculations irrespective of actual income level distribution in the community. Income categories are summarized in Table 5-1.

Numerous assumptions are required to translate household income to affordable rental rates and purchase prices. Lenders ultimately determine the actual purchasing power of household income at a given point in time. For planning

purposes, the City of Roseville assumes that low- and very low-income households should not spend more than thirty percent (30%) of their monthly gross income on housing costs, including utilities. For middle-income households, thirty five percent (35%) of monthly gross income is the threshold. Purchase housing costs include principal, interest, taxes, insurance, and any homeowner's association dues.

Table 5-1: Definition of Household Income Categories

Income Category	Percent of Median Income
Very Low-Income	Less than 50% of Median
Low-Income	51% to 80% of Median
Middle-Income	81% to 100% of Median
Moderate-Income	101% to 120% of Median
Above Moderate-Income	121% + of Median

It is recognized that the various factors that determine affordability continually change, and that project specific affordability standards need to be established and adjusted as development occurs.

5.3

AFFORDABLE HOUSING PROGRAM

Consistent with the General Plan affordable housing goal, ten percent (10%) of the units in the WRSP have been designated for middle-, low- and very low-income households. This includes a mix of purchase housing affordable to middle-income households, and rental housing affordable to low- and very low-income households. Approximately twenty percent (20%) of the affordable housing goal will be available to middle-income residents, forty percent (40%) to low-income residents and forty percent (40%) to very low-income residents. The WRSP affordable housing goal is summarized in Table 5-2.

Table 5-2: Affordable Housing Goal

Total Dwelling Units	8,430 du
10% Affordable Housing Goal	843 du
Middle-Income (purchase)	168 du (20%)
Low-Income (rental)	338 du (40%)
Very Low-Income (rental)	337 du (40%)

The WRSP affordable housing goal has been allocated to specific MDR and HDR parcels as identified on Table 5-3, with the designated parcels reflected on Figure 5-1. The intent is to distribute affordable units throughout the WRSP area.

Table 5-3: Affordable Housing Allocation

Parcel	Land Use	Total Units in Parcel	Total Affordable Allocation	Very Low-Income Rental	Low-Income Rental	Middle-Income Purchase
W-16	MDR	160	42			42
W-19	MDR	165	43			43
W-25	HDR	240	144	72	72	
W-28	HDR	175	47	23	24	
W-29 (Senior)	HDR	150	150	75	75	
<i>Subtotal</i>			426	170	171	85
F-16	MDR	185	46			46
F-17	MDR	174	37			37
F-20	HDR	136	68	27	41	
F-22	HDR	110	44	22	22	
F-23	HDR	160	96	32	64	
F-24	HDR	200	86	46	40	
F-25	HDR	100	20	20		
F-26	HDR	98	20	20		
<i>Subtotal</i>			417	167	167	83
TOTAL			843	337	338	168

Note: The WRSP (Section 5.4) allows for the transfer of affordable units. Check with the City of Roseville Economic and Community Services Department to confirm current affordable housing allocations.

A *Supplemental Affordable Housing Development Agreement* (or substitute form as specified by the City) is required for each parcel with an affordable housing allocation to detail and secure specific requirements and obligations.

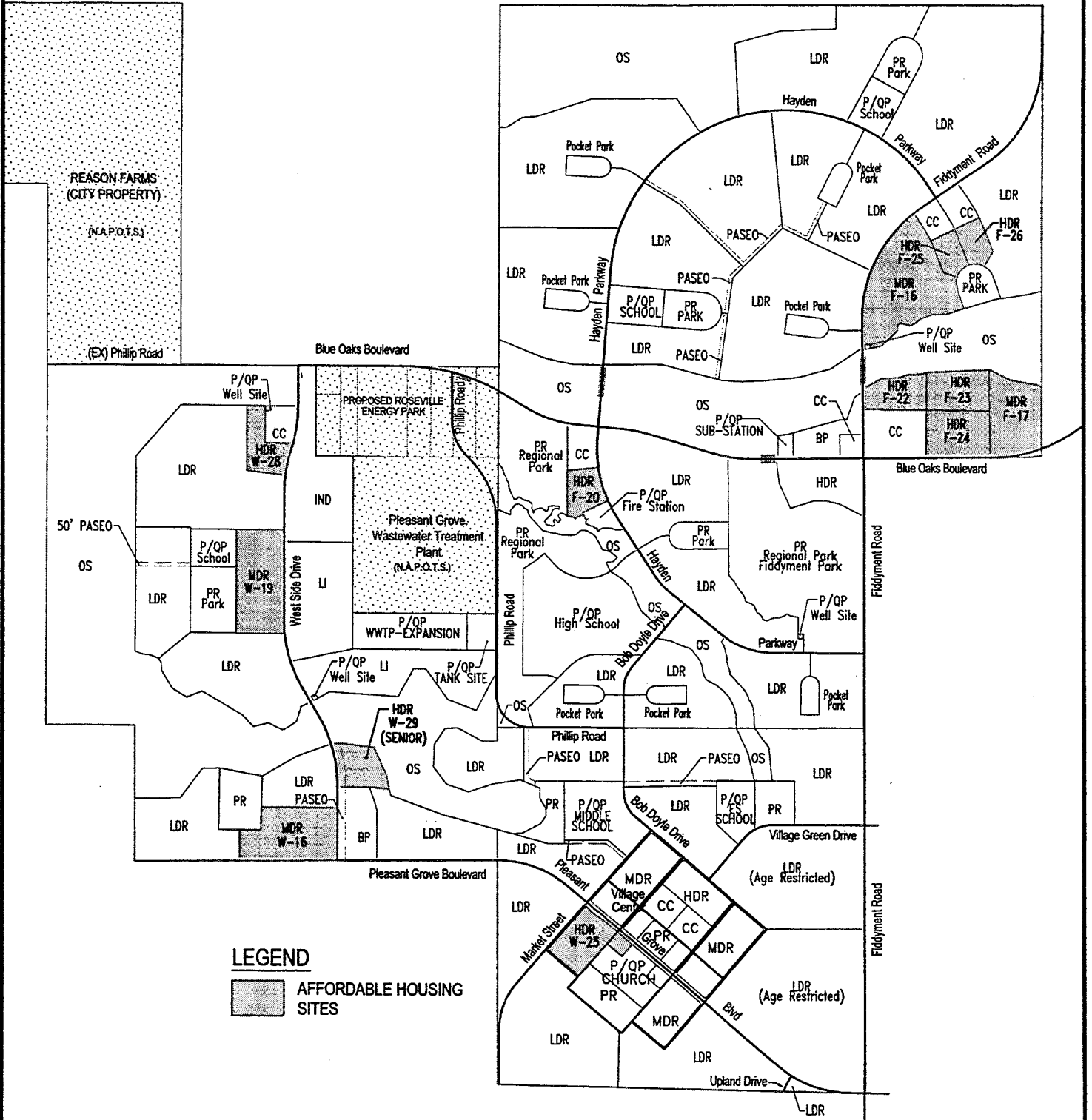
Among other provisions, the Supplemental Affordable Housing Development Agreement will:

- Specify the number of affordable units to be reserved at each income level.
- Specify the term of the affordability obligation.
- Set initial rent or purchase prices for the designated affordable units.
- Establish criteria and a basis for annual rent or purchase price increases.
- Provide the City with a mechanism to monitor actual rents and purchase prices paid.
- Identify any City or other subsidies required to assist in meeting the affordability requirement and, if applicable, the basis and terms for refunding such subsidies.

Supplemental Affordable Housing Development Agreements require City approval prior to the issuance of building permits, or recordation of a final map where a subdivision map is required, for any large-lot WRSP parcel with an affordable housing allocation.

Figure 5-1

Affordable Housing Distribution



LEGEND
 AFFORDABLE HOUSING SITES

5.4 FLEXIBILITY IN IMPLEMENTATION

WRSP residential builders are encouraged to explore creative approaches in providing a range of housing opportunities to meet the needs of middle-, low- and very low-income households. Housing markets, income categories, funding programs and other factors change over time. As a result, it is important to retain some level of flexibility to ensure that affordable housing goals are achieved. The following may be considered to assist in achieving the WRSP affordable housing goal:

Transfers/Credits – Subject to approval of the Economic and Community Services Director, the affordable housing allocations identified on Table 5-3 may be transferred among WRSP parcels. In addition, to the extent that the number of affordable units produced on a parcel exceeds the number of affordable units allocated to that parcel, the excess units may be credited towards meeting the WRSP affordable housing goal assigned to other parcels. The Economic and Community Services Director may approve such transfers and/or credits without the need for a Specific Plan Amendment if it is determined that:

1. The transfers/credits are applied to parcels within the WRSP and covered by the same development agreement.
2. The transfers/credits improve the ability to produce affordable units and achieve the WRSP affordable housing goal.

Requests for transfers and/or credits shall include information as deemed necessary by the Economic and Community Services Director to ensure consistency with the above determinations. In addition, a revised Affordable Housing Allocation (Table 5-3) shall be provided reflecting adjusted affordable unit allocations. The Economic and Community Services Department shall maintain all revisions to Table 5-3 as the official WRSP affordable housing allocation record.

Density Bonus – The City may, in accordance with its Density Bonus Ordinance (Zoning Ordinance, Chapter 19.28), assign additional residential units to projects for the purpose of achieving the affordable housing goal. The increase in units provided by a density bonus is intended to reduce average per unit development costs. In the WRSP, a density bonus is assigned by City approval of a Supplemental Affordable Housing Development Agreement (or substitute form as specified by the City) to individual projects on a case-by-case basis, and may constitute a portion of the subsidy (if required) for the provision of affordable units.

In-Lieu Fee - To the extent an in-lieu affordable housing fee is adopted on a citywide basis, the City may allow the WRSP to satisfy a portion of its affordable housing obligation through this means.

The City reserves the right to consider alternatives to achieving affordable housing within the WRSP should the cost of producing the affordable housing preclude the City from accessing federal and state financing programs, or legislation mandate the City alter its approach to affordable housing.

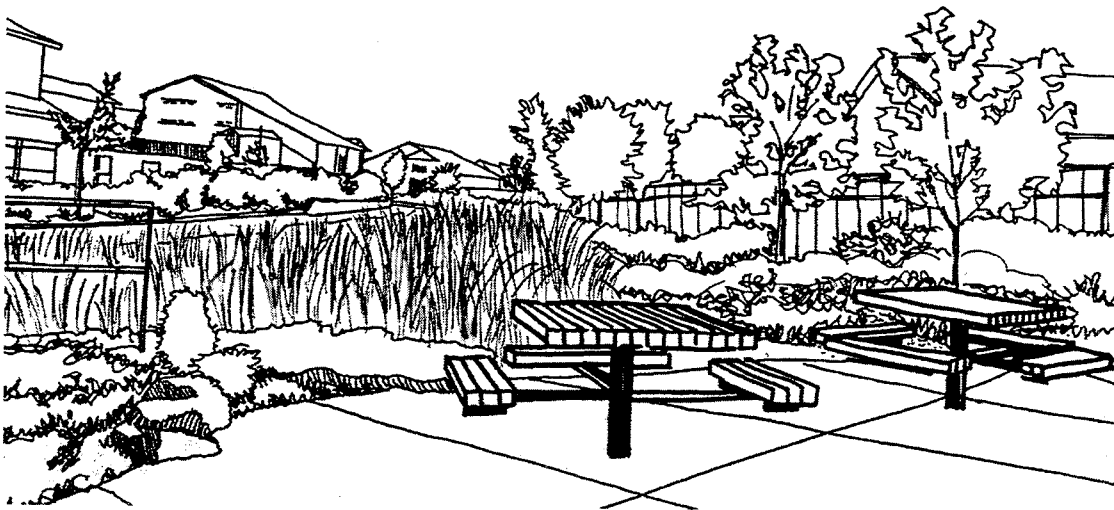
6 RESOURCE MANAGEMENT

6.1 OVERVIEW

The natural undeveloped character of the West Roseville Specific Plan consists primarily of non-native grassland with relatively flat topography on the southern portion of the Plan Area, and gently rolling topography on the northern portion of the Plan Area. Intermittent creek corridors, Kaseberg, Curry and Pleasant Grove Creeks, traverse the site. Riparian and oak woodlands line portions of Pleasant Grove and Kaseberg Creeks, with seasonal wetlands, including vernal pools, clustered throughout the Plan Area.

Coordination with other agencies and conservation efforts is a fundamental principle and key objective of the WRSP resource management approach.

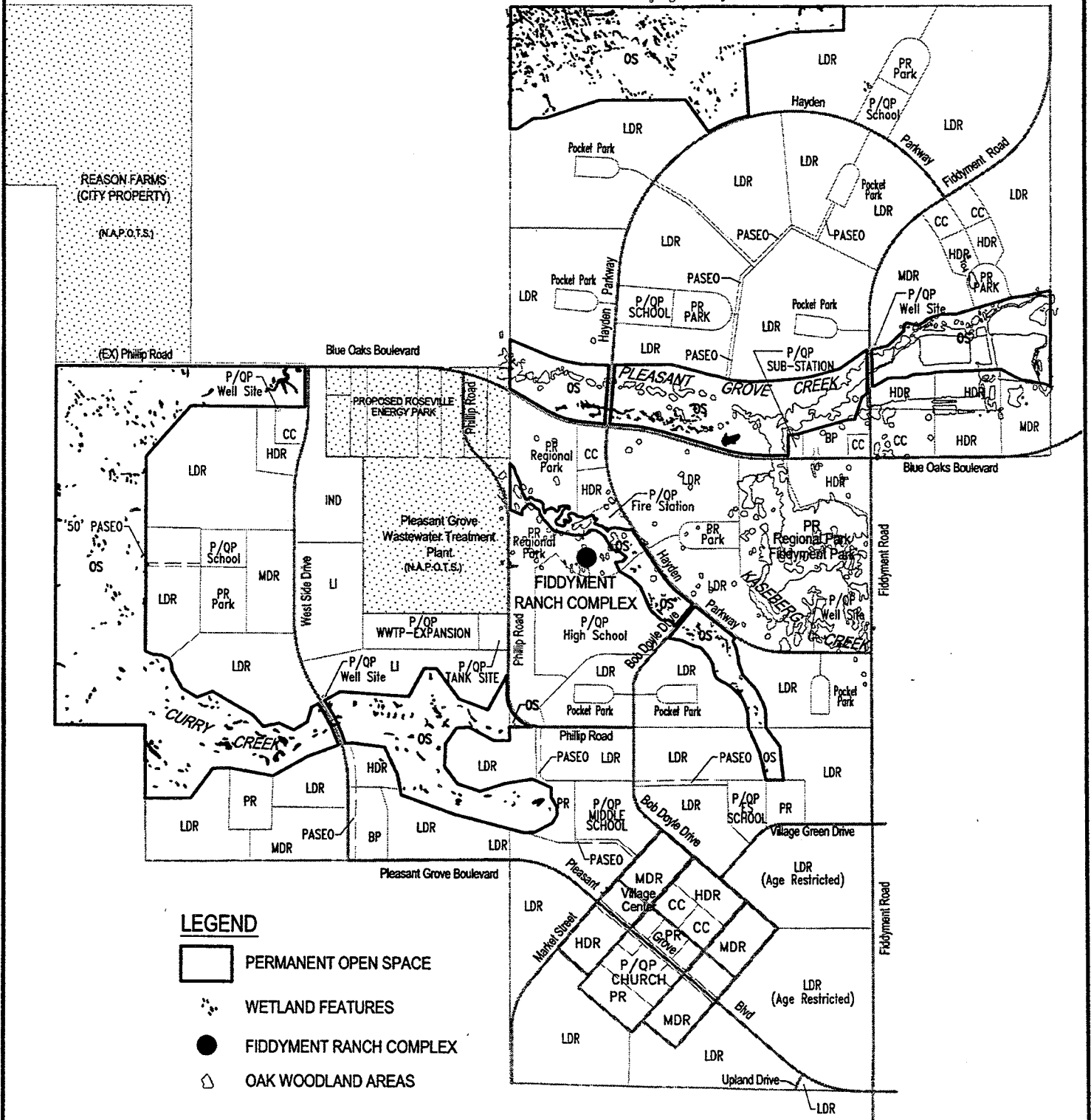
The WRSP establishes contiguous open space areas that are driven by the protection of significant natural resources and allow for potential connectivity with larger-scale conservation efforts. Over twenty percent (20%) of the WRSP is designated as permanent open space (see Figure 6-1). In addition to resource protection, the designated open space areas play a significant role in defining the visual character of the WRSP and provide for passive recreation opportunities, pedestrian and bike access, storm drainage, flood water conveyance, backbone sewer infrastructure and land use buffering.



Open Space

Figure 6-1

Open Space and Resource Areas



LEGEND

- PERMANENT OPEN SPACE
- WETLAND FEATURES
- FIDDYMENT RANCH COMPLEX
- OAK WOODLAND AREAS

The WRSP is planned to comply with the City of Roseville General Plan Open Space and Conservation Element. The form of the WRSP resource management approach has also been appreciably influenced by early consultation with appropriate federal and state resource agencies. The resource management approach is designed to be consistent with the Memorandum of Understanding between the City and U.S. Fish and Wildlife Service relating to the Pleasant Grove Regional Wastewater Treatment Plant. In addition, the WRSP and related off-site preservation and restoration efforts being undertaken consistent with State and federal permit requirements are intended to complement larger-scale regional conservation strategies for Placer County. These include the County proposed Habitat Conservation Plan (HCP)/Natural Communities Conservation Plan (NCCP) and Placer Legacy. Coordination with other agencies and conservation efforts is a fundamental principle and key objective of the WRSP resource management approach.

Typically, open space areas are dedicated to the City. It may be appropriate, however, to dedicate portions of the open space to another public, private or institutional entity charged with maintaining and enhancing natural resources. Such dedication could occur in concert with complementary regional conservation efforts.

Based on the characteristics of the WRSP area, the resource management approach in the Specific Plan focuses on wetlands, riparian and oak woodlands, and the site's historic resources. Additional resources, including grassland foraging habitat and protected species, are further addressed in the project EIR.

6.2 WETLAND RESOURCES

The WRSP is situated within the Pleasant Grove Creek and Curry Creek watersheds. As previously noted, Kaseberg Creek, Curry Creek and the main and southern branches of Pleasant Grove Creek traverse the WRSP site (see Figure 6-1). In addition, another small creek, Coyote Creek, crosses a portion of the WRSP. These ephemeral (intermittent) creeks have historically flowed with winter rainfall and run dry (or below the surface) during the summer months, although small surface pools may remain into the dry season. Increased irrigation runoff from urban development is expected to result in the creeks becoming more perennial (year round) in character.



Vernal Pool

In addition to the creeks, there are small swales and drainages throughout the WRSP that carry water briefly during winter rainfall. Outside of the creeks and swales, vernal pools and seasonal wetlands are found primarily within grassland areas. The vernal pools and seasonal wetlands fill with rainwater, and can remain inundated until spring or early summer. The wetland areas include habitat potentially suitable for certain federal and/or state special-status plant and wildlife species.

A total of 63.89 acres of verified wetlands or “other waters” of the U.S. occur within the WRSP in its undeveloped state. It is estimated that implementation of the WRSP will preserve 40.19 acres, or 63%, of Plan Area jurisdictional wetlands. The remaining 23.21 acres, or 37%, will be directly impacted, with additional acreage indirectly impacted. Table 6-1 summarizes the estimated acreage of preserved and potentially impacted WRSP wetlands by type.

Table 6-1: Jurisdictional Wetlands

	Vernal Pools	Vernal Swale	Wet Swales	Seasonal Wetland	Emergent Marsh	TOTAL
Pre-Development	33.91 ac	8.05	17.39 ac	3.92 ac	0.62 ac	63.89 ac
Preserved	19.62 ac (58%)	4.76 (59%)	13.22 ac (76%)	2.59 ac (66%)	0 ac (0%)	40.19 ac (63%)
Impacted (Direct)	13.80 ac ¹ (42%)	3.29 (41%)	4.17 ac (24%)	1.33 ac (34%)	0.62 ac (100%)	23.21 ac ¹ (37%)

1. 0.49 additional acres of vernal pool was impacted and permitted by City of Roseville Wastewater Treatment Plant Pipeline (USFWS 1-1-01-F-0034).

The overall goal of the WRSP mitigation program is no net loss of wetland functions, habitat and values. To achieve this goal, it is anticipated that the WRSP mitigation program may incorporate a combination of: 1) on-site avoidance and preservation of wetlands; 2) on-site enhancement and creation of wetlands; 3) off-site acquisition of existing wetlands; 4) off-site restoration of degraded wetlands; and/or, 5) purchase of habitat credits at an agency-approved mitigation bank.

To promote coordination and ensure compliance with the Clean Water and Endangered Species Acts, design of the WRSP mitigation program has involved early consultation with the resource agencies. Prior to submittal of an application for the WRSP, the City and WRSP landowners initiated regular meetings with representatives from the U.S. Fish and Wildlife Service (USFWS), the U.S Army Corps of Engineers (USCOE), the California Department of Fish and Game (CDFG), the Environmental Protection Agency (EPA) and the National Marine Fisheries Service (NMFS).

Development of the mitigation program has followed the process outlined in the City and USFWS Memorandum of Understanding (MOU) from May 2000. The purpose of the MOU is to minimize the indirect impact of incidental take of vernal pool species resulting from future City growth served by the then under construction Pleasant Grove Regional Wastewater Treatment Plant. To accomplish this, the MOU commits the City to development of an “interim conservation strategy” to address City development that would be served by Phase I treatment plant operation, and a Habitat Conservation Plan (HCP) or equivalent for future City development served by Phase II operation. At the time the MOU was signed, the planning area was restricted to existing City boundaries and did not include the WRSP. The MOU, however, includes provisions to incorporate annexed lands, such as the WRSP, into the “planning area” covered by the MOU.

The City is committed to developing a “vernal pool strategy” which is generally consistent with the MOU and subsequent consultation with the USFWS. This includes, in the context of an annexation project such as the WRSP, development of an overarching management strategy to tie the existing City preserve system together under a broader more unified framework. It also includes the commitment to develop an annexation area conservation plan in a manner compatible with the larger-scale conservation efforts sponsored by Placer County, such as the HCP/NCCP and Placer Legacy. This compatibility will extend to opportunities for efficiency and consistency in operations and maintenance approaches between these efforts, existing City preserves and annexation conservation plans.

Based on consultation with the resource agencies, an appropriate strategy for the WRSP will include a combination of avoidance and a substantial on- and off-site preservation component. To preserve the vernal pools, site design, grading and drainage plans will be designed to maximize maintaining the existing hydrology in the preserve areas. Vernal pool inventory mapping prepared to formulate the County’s HCP/NCCP vernal pool conservation strategy will influence the location of the off-site mitigation and preservation component. The ultimate mitigation and subsequent monitoring program will be subject to approval through the 404 permit process. A 404 permit must be secured prior to any development activity within the WRSP.

Both on-site and off-site wetlands associated with the WRSP mitigation program will be preserved and managed in perpetuity. Maintenance and management of the preserve areas will be in accordance with the Operations and Maintenance (O&M) Plan, and deed restrictions, both are requirements of the Clean Water Act 404 permit, and will comprise the methodology for preservation and management of the preserve areas. The O&M Plan will include standards and methods for wetland preservation, monitoring and adaptive management (including fire protection measures). Funding for the management of on-site preserve areas will be provided by a long-term landowner endowment (or equivalent) managed either by the City or a third party such as a local non-profit or land conservancy.

6.3

RIPARIAN & OAK WOODLAND RESOURCES

Riparian and oak woodlands are found in the central portion of the WRSP generally along Pleasant Grove Creek, Kaseberg Creek and some of their unnamed tributaries (see Figure 6-1). Limited woodlands exist outside of these corridors. The riparian woodland occurs immediately adjacent to the creeks and includes valley oak, cottonwood, alder, willow, ash and other species. The oak woodland occurs upland and adjacent to the riparian woodlands and creek channels, and consists primarily of blue oak, with small amounts of interior live oak and valley oak. Both the riparian and oak woodlands are an important habitat for a variety of wildlife species, and a significant visual amenity. There are approximately 110.3 acres of woodland in the WRSP in its undeveloped state.

The WRSP land use plan designates creek corridors, their associated floodplains and a majority of the related riparian and oak woodland areas for preservation as open space. In addition, the densest concentration of oak woodland, along Kaseberg Creek near its confluence with Pleasant Grove Creek, has been included in Fiddymt Park. Uses within open space are generally limited to trails, stormwater drainage, floodwater conveyance, roadway/utility crossings, and passive recreation. It is estimated that approximately 91% of the WRSP woodlands are within open space or park areas. Outside of open space and park areas, preservation of native oaks will be addressed through review of individual projects and improvements.



Oak Woodland along Pleasant Grove Creek

Development in the WRSP, including public and private improvements, is subject to the City of Roseville Tree Preservation Ordinance. Regulated activities within the protected zones of native oaks, and proposed tree removals, are subject to approval of a tree permit by the City. The Tree Ordinance requires that trees approved for removal be compensated on inch-for-inch bases either through regeneration/replanting or payment of mitigation fees.

Oak Woodland compensation in the WRSP focuses on habitat value by requiring regeneration/replanting receiving zones adjacent and connected to preserved woodland areas, as well as providing for compensation through replanting in landscape corridors or other formalized landscape settings. In addition to designating receiving zones, required infrastructure (irrigation) and monitoring/maintenance programs are required. Beyond accommodating

compensation from Plan Area projects, the WRSP regeneration/replanting receiving zones may also accept compensation from outside the Plan Area, either directly or through the City's mitigation fees, and contribute to larger-scale regional conservation efforts. See Figure 6-2 for the location of WRSP Oak Woodland regeneration/replanting receiving zones within the Plan Area. In addition, off-site oak woodland compensation may occur outside of the WRSP.

6.4 FIDDYMENT RANCH HISTORIC RESOURCES

Beside natural resources, the WRSP contains historic resources associated with past ranching activities. The *Fiddymment Ranch Main Complex* is located in the central portion of the Plan Area (see Figure 6-1). The Ranch complex was the headquarters of the Fiddymment ranching and living operations dating to the 1870's. It is the oldest, continually operating family ranch in Roseville. The complex consists of a main house, barn, cooler, smokehouse and reservoir all from the 1880's. Numerous other outbuildings and subsequently constructed structures also exist within the complex.

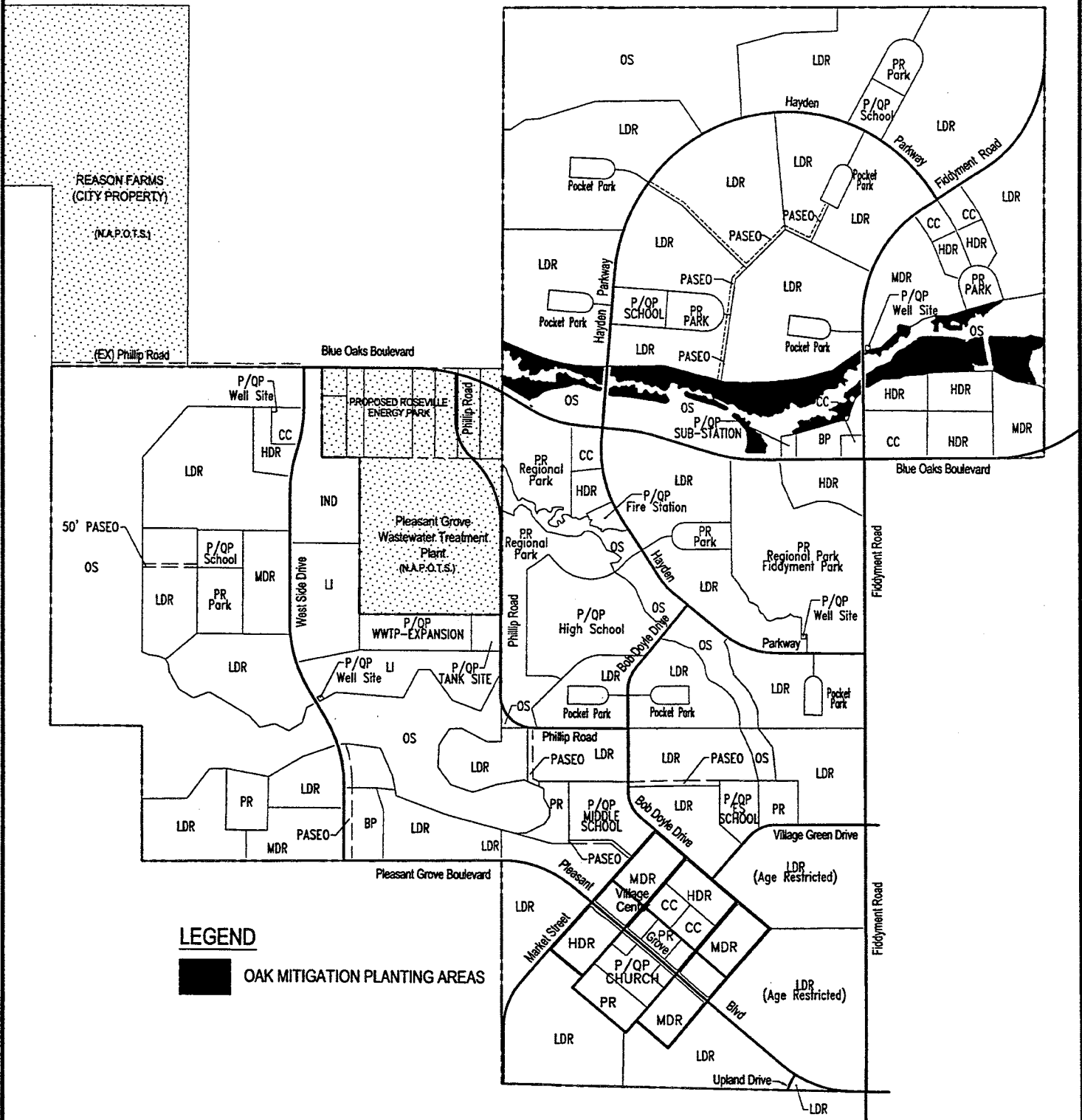


Fiddymment Ranch Home

A cultural reconnaissance survey of the WRSP identified the ranch complex as meeting the California and National Registers criteria for the role it played in the social and economic development of Roseville. The Fiddymment Ranch Main Complex is located within the Regional Sports Park (Parcel F-56). It is anticipated that the Fiddymment Ranch Complex will be preserved within the Sports Park as a community facility available for future use by the City. The complex would be owned by the City and potentially operated by a private entity and could be used for meetings, weddings, receptions and other events. The precise use and disposition of the buildings and associated grounds will be determined as part of the facilities planning for the Regional Sports Park.

Figure 6-2

Oak Mitigation Planting Areas



LEGEND

 OAK MITIGATION PLANTING AREAS

7 CIRCULATION PLAN

7.1 OVERVIEW

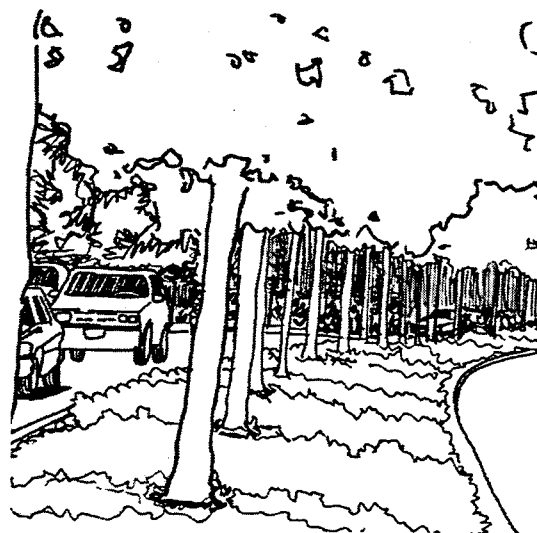
The West Roseville Specific Plan circulation system includes a hierarchy of roadways, a pedestrian and bikeway network, and public transit. Emphasis is placed on ensuring connectivity between uses and on creating a safe and efficient circulation system that complies with City policies and allows for transportation options. The circulation system has been designed to link with existing City and regional systems, with effort made to ensure that the WRSP does not preclude a feasible alignment for the proposed Placer Parkway. In addition, specific provisions and standards have been included to reflect the unique nature of the Village Center (see Section 10).

7.2 ROADWAYS

The WRSP roadway system includes arterial, collector and local roadways. The WRSP Major Roadway Plan is reflected on Figure 7-1 with lane capacity, right-of-way and landscape requirements summarized on Table 7-1. Landscaping standards, enhanced bridge designs and other design details are included within the WRSP Design Guidelines (Section 12). The construction of arterial and collector roadways will be phased as described in the Specific Plan development agreements. All roads will be constructed to City of Roseville standards.

□ Arterial Roadways

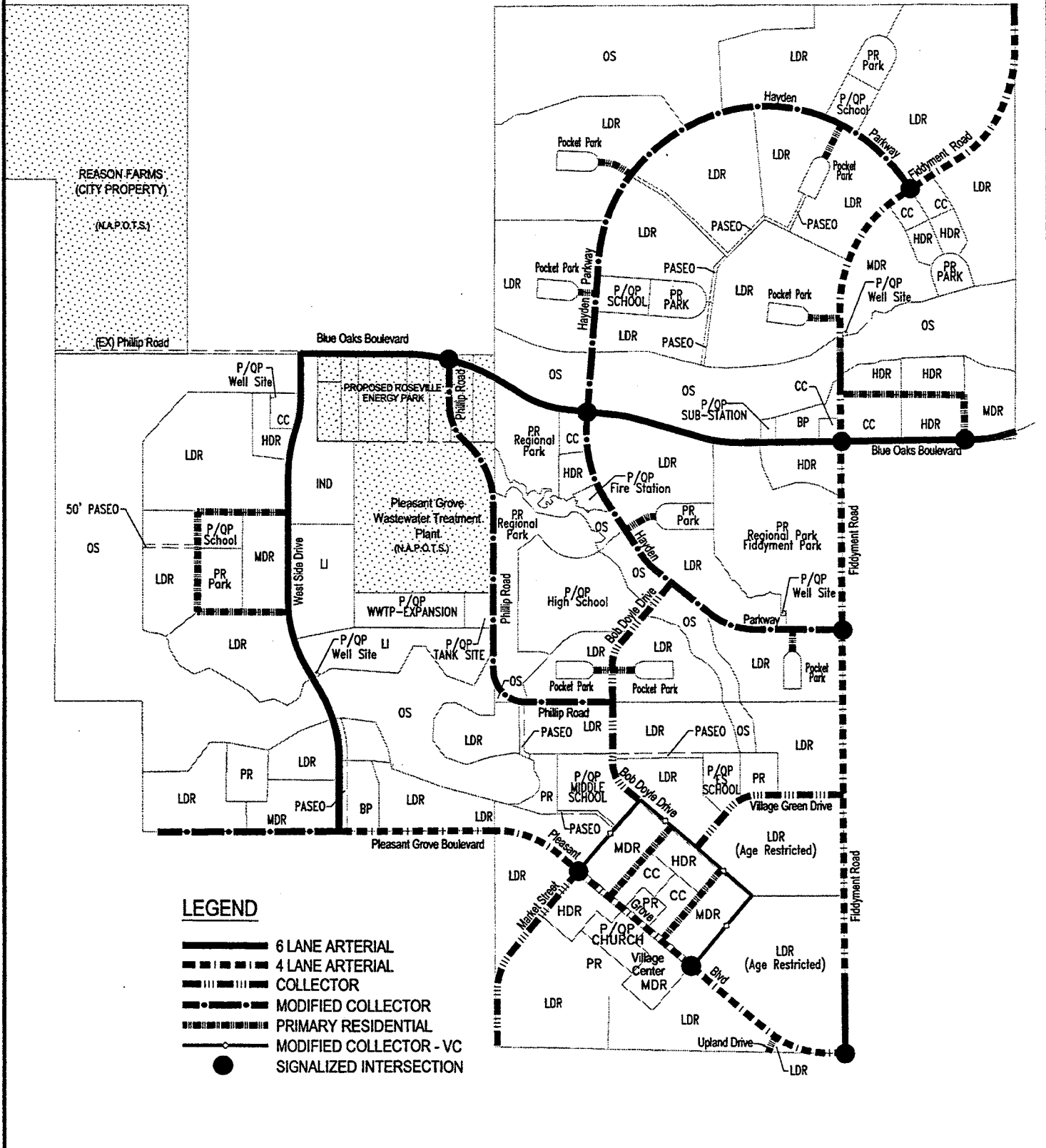
Arterial streets are primary circulation routes that provide linkages between sections of the City and to the regional circulation system. These roadways generally carry large volumes of traffic within and through the City. On street parking on arterials is prohibited, with access to adjacent land uses limited to minimize cross traffic turning movements and landscape median breaks, and to improve traffic safety and flow. In the WRSP, arterials range from 4 to 6-lanes with landscape medians, Class II bike lanes, and adjacent landscape corridors incorporating 8-foot wide detached sidewalks.










Typical Arterial Street

Figure 7-1

Major Roadway Plan



LEGEND

-  6 LANE ARTERIAL
-  4 LANE ARTERIAL
-  COLLECTOR
-  MODIFIED COLLECTOR
-  PRIMARY RESIDENTIAL
-  MODIFIED COLLECTOR - VC
-  SIGNALIZED INTERSECTION

Y:\2001\010003\DWG\PLAN\NINGSPECIFIC PLAN\01003-LU12A-E33-KS.DWG 09-10-03 11:28 am - pbarber

Table 7-1: Roadway Summary

Type	Roadway		Landscape Corridor		Landscape Median	Figure
	Reserved Lane Capacity	Right-of-Way	Adjacent to LDR & MDR ²	Adjacent to Other Uses ¹		
ARTERIAL ROADWAYS³						
Blue Oaks Boulevard	6	100'	50'	50' ⁴	14'	7-2
West Side Drive	6	100'	35' ⁸	50' ⁷	14'	7-4
Pleasant Grove Boulevard ⁵	4	76'	25'	35'	14'	7-5
Fiddymment Road	4 ⁹	76' ⁹	35'	35'	14'	7-5, 7-6
COLLECTOR ROADWAYS³						
Typical Collector (Without Median)	2	48'	25'	25'	-	7-7
Hayden Parkway (With Median)	2	50'	25'	25'	12'	7-8
Phillip Road	2	40', 42' and 48'	25'	25' ⁶	-	7-9
LOCAL ROADWAYS						
Primary Residential (Detached Sidewalk)	2	56'	-	-	-	7-11
Primary Residential (Attached Sidewalk)	2	46'	-	-	-	7-11
Minor Residential (Detached Sidewalk)	2	52'	-	-	-	7-12
Minor Residential (Attached Sidewalk)	2	42'	-	-	-	7-12
VILLAGE CENTER ROADWAYS	See Village Center Plan, Section 10					

- Landscape corridors will not be constructed adjacent to parks. Sidewalks with modified transitional landscaping will be constructed along open space areas (see Section 12, Design Guidelines).
- Landscape corridors adjacent to LDR and MDR along arterial and collector roadways will be incorporated within the street right of way.
- Ancillary lanes and tapers may not come out of the landscape setback. Bus turnouts may come out of the landscape setback, but in no case shall the landscape setback be less than 20 feet (after the bus shelter has been taken out) except for Hayden Parkway. Here the landscape setback may be reduced to 10 feet behind the bus shelter only, provided there is direct access to the bus shelter from Parcel F-34, the bus shelter is adjacent to on-site landscaping, and either a walkway or plaza design element is included.
- A landscape corridor 50 feet in width will be provided on the north side of Blue Oaks Blvd. and is designed to accommodate a powerline easement (see Figure 7-2).
- Pleasant Grove Boulevard street sections and landscaping vary within the Village Center. See Village Center Plan, Section 10.
- 20' adjacent to PGWWTP (See Design Guidelines).
- Landscaped paseo is located adjacent to a 35' powerline easement along Parcel W-63 (see Figure 7-4).
- LDR and MDR parcels directly abutting West Side Drive (within large lot parcels W-13, W-16, W-17, W-18 and W-19) shall include a 15-foot wide supplemental setback in addition to the required setback.
- A section of Fiddymment Road just north of its intersection with Pleasant Grove Boulevard expands to 6-lanes with 120-feet of right of way (see Figure 7-6).

Note: See Design Guideline (Section 12), for landscape requirements, entry treatments, requirements for single loaded streets, relationship to paseos, enhanced bridge design, street lighting, exceptions and other details.

- Blue Oaks Boulevard (Figure 7-2) is planned for 6-lanes (100-foot right-of-way) extending west through the WRSP terminating at West Side Drive. A 60kV overhead power line is planned within a 35- to 50-foot wide easement coterminous with the landscape corridor on the north side of Blue Oaks Boulevard. The landscape treatment within this easement differs dependent upon whether the adjacent use is open space or another use (see Design Guidelines, Section 12). Early phases of the WRSP will construct 4-lanes of Blue Oaks Boulevard with a 38-foot wide median that may accommodate expansion to 6-lanes in the future. A striping and turning movement plan for the Fiddymment Road/Blue Oaks Boulevard intersection area is included on Figure 7-3, with a median opening plan for all WRSP roadways with landscaped medians included on Figure 7-10. The intent of the turning movement and median opening plans are to identify opportunities to consolidate left turn movements to facilitate traffic flow and minimize interruption to the median.
- West Side Drive (Figure 7-4) is planned as 6-lanes (100-foot right-of-way) extending between Blue Oaks and Pleasant Grove Boulevards. The landscape treatment for West Side Drive differs when adjacent to a Paseo versus other uses (see Design Guidelines, Section 12). Early phases of the WRSP will construct 4-lanes of West Side Drive with a wide median that may accommodate expansion to 6-lanes in the future.
- Pleasant Grove Boulevard (Figure 7-5) is planned for 4-lanes (76-foot right-of-way) with a landscaped median extending from Fiddymment Road to West Side Drive. Pleasant Grove Boulevard will continue west beyond West Side Drive as a 2-lane collector to serve adjacent residential neighborhoods.
- Fiddymment Road (Figure 7-5 and 7-6) is planned to be expanded to 4-lanes (76-foot right-of-way) with a landscaped median and will be realigned to improve flow and safety north of the Blue Oaks Blvd. intersection. Portions of the old Fiddymment Road will be utilized as local roadways. Other portions, including the bridge over Pleasant Grove Creek, will be utilized as bike and pedestrian trails. A section of Fiddymment Road just north of its intersection with Pleasant Grove Boulevard expands to 6-lanes with 120-feet of right of way (see Figures 7-1 and 7-6)

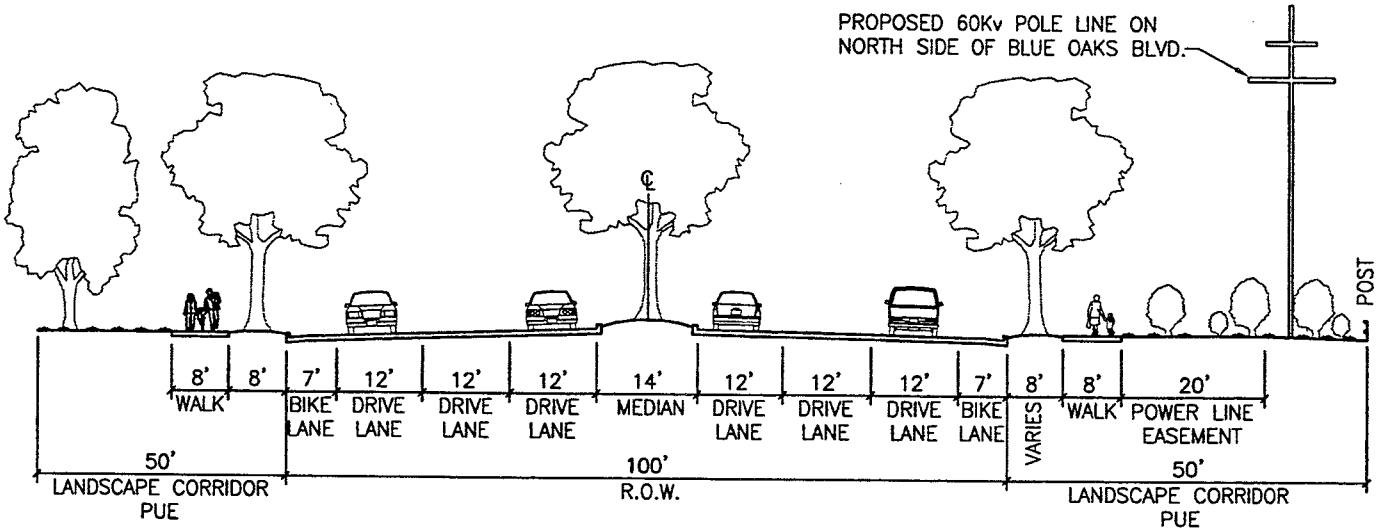
The Placer County General Plan includes a general alignment for a northern extension of Watt Avenue near the western edge of the WRSP. Given that the WRSP represents the western limit of development in the City of Roseville, and that an extension of Watt Avenue is not needed to serve the Plan Area, this roadway has not been included in the WRSP. West Side Drive could be utilized as the alignment of Watt Avenue if necessary in the future.

Details relating to the construction and phasing of WRSP arterial roadways is included in the project development agreements.

Figure 7-2

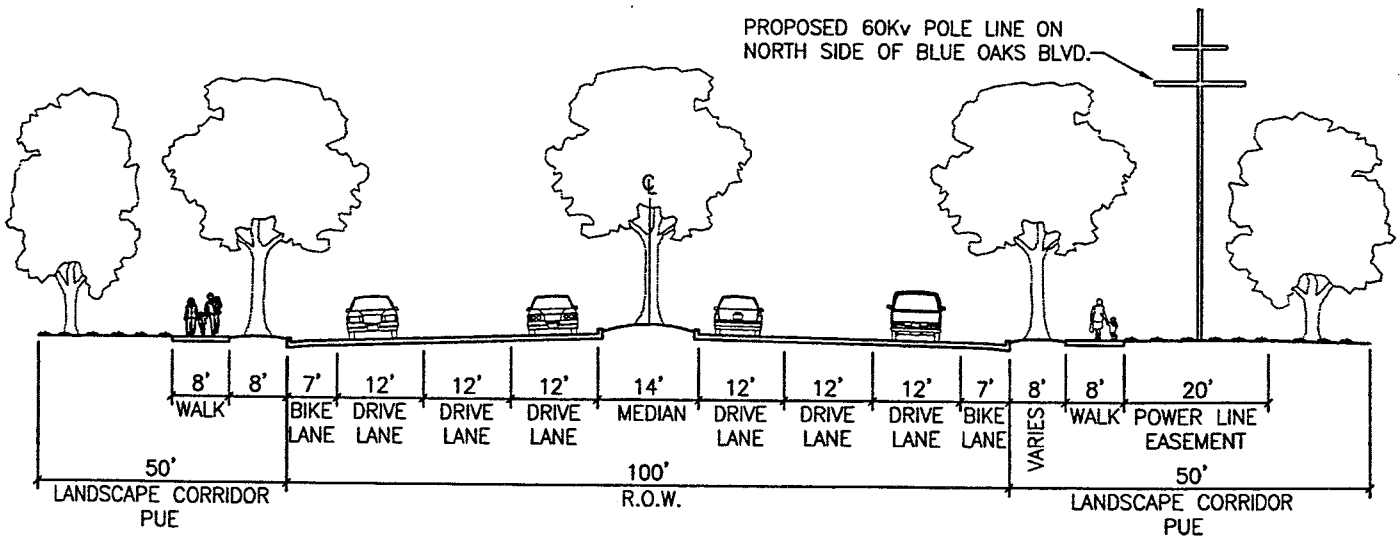
Blue Oaks Boulevard

ADJACENT TO OPEN SPACE



100' R.O.W.

ADJACENT TO OTHER LAND USE



100' R.O.W.

Figure 7-3

Fiddymment Road/Blue Oaks Boulevard Intersection Turning Movements

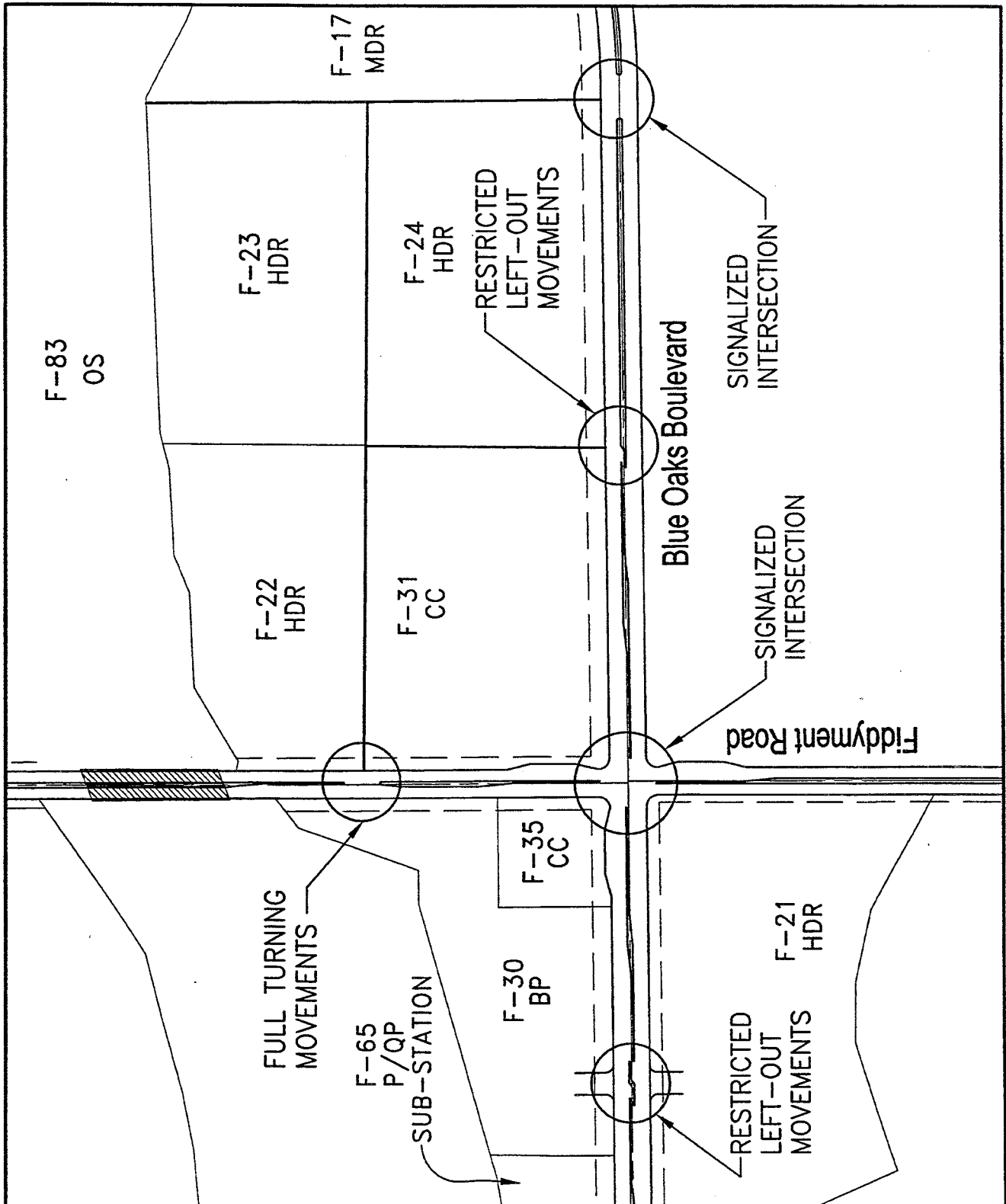
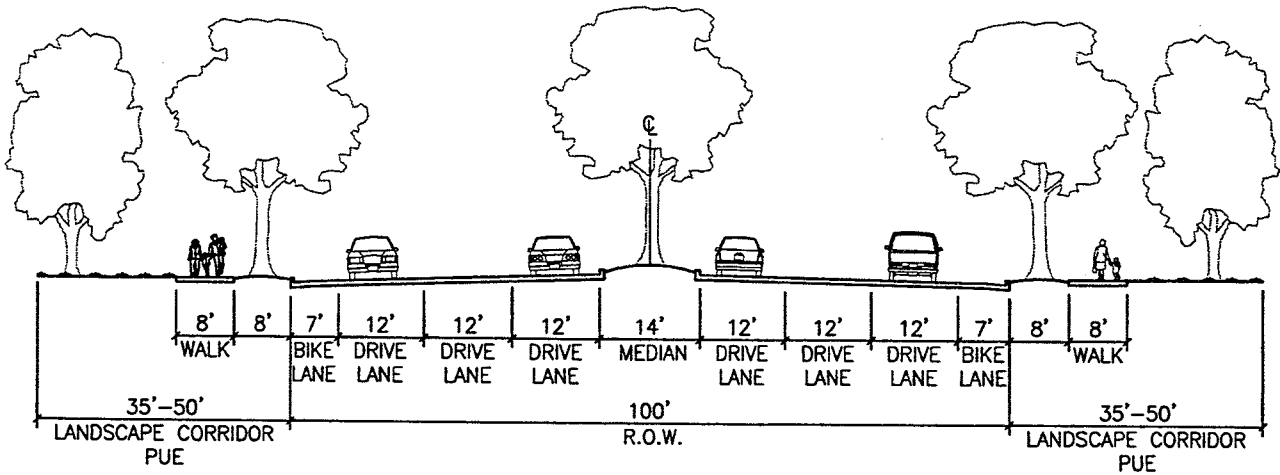
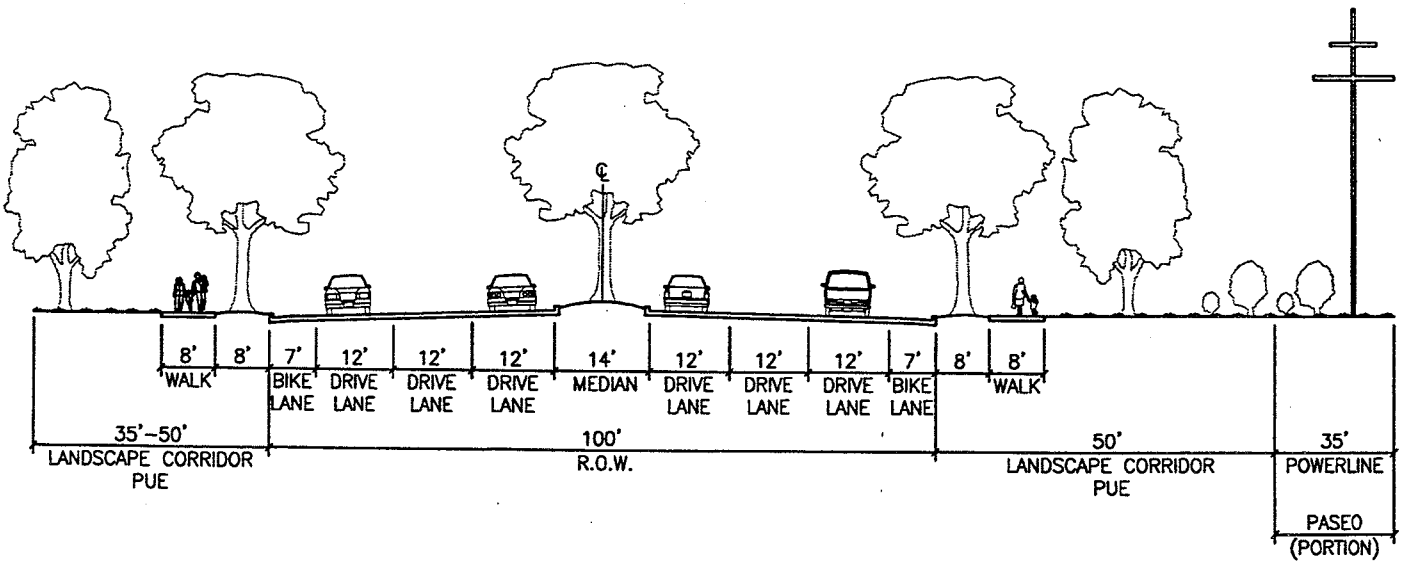


Figure 7-4

West Side Drive



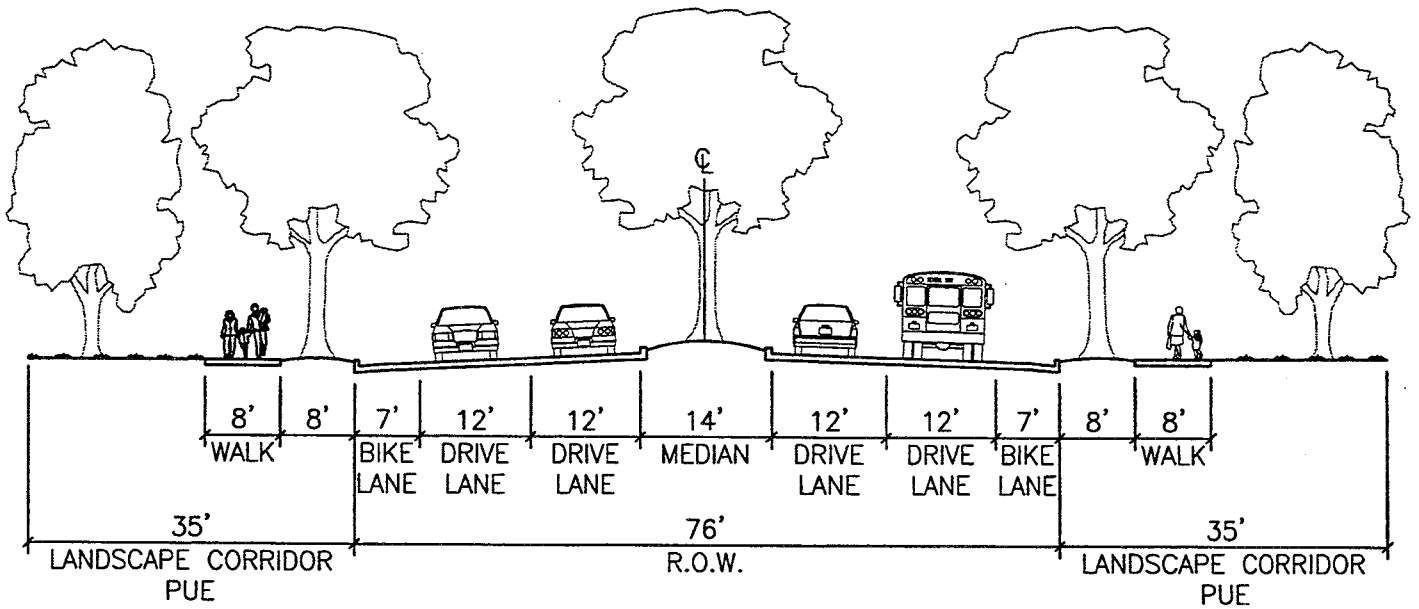
100' R.O.W. Typical Section



100' R.O.W. Section at Paseo Near Powerline Easement

Figure 7-5

Pleasant Grove Boulevard and Fiddymment Road Four Lane Arterial



76' R.O.W.

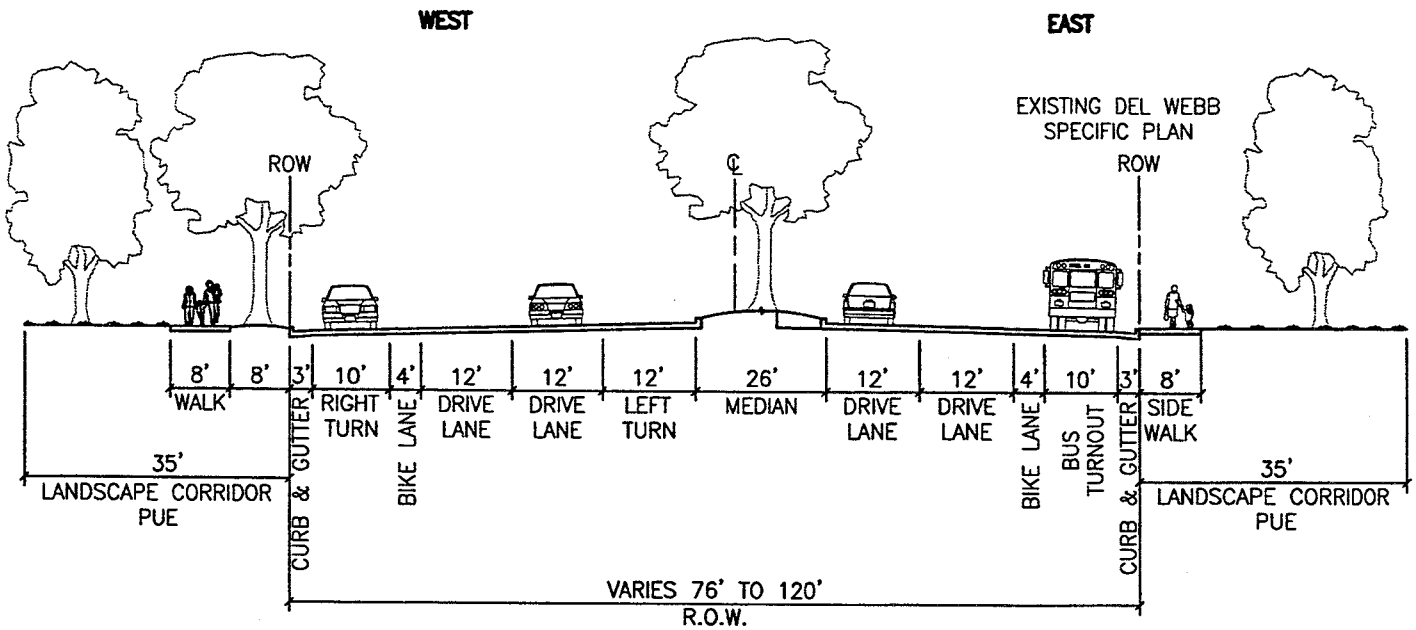
NOTE:

The north side of Pleasant Grove Boulevard is identified as an alternative alignment for a 60kV electric line. The 60kV electric line would be located in the landscape corridor on the north side of the roadway.

The west side of Fiddymment Road is identified as an alternative alignment for a 60kV electric line. The 60kV electric line would be located in the landscape corridor on the west side of the roadway.

Figure 7-6

Fiddymment Road Taper North of Pleasant Grove Boulevard Six Lane Section



□ Collector Roadways

Collector streets are secondary circulation routes that generally distribute trips from the arterial street system to the local street system. On street parking on WRSP collectors is prohibited, and access to adjacent uses may be restricted dependent upon projected traffic volumes or to minimize breaks in landscape medians where present. Collector streets contain 2-lanes, Class II bike lanes, and adjacent 25-foot wide landscape corridors incorporating 5-foot detached sidewalks. Right-of-way for a typical collector is 48-feet (see Figure 7-7).

Within the WRSP, there are two unique collector configurations:

- Hayden Parkway (Figure 7-8) provides 50-feet of right-of-way to accommodate a 12-foot wide landscape median with restricted median breaks (see Figure 7-10). A detailed median opening plan for Hayden Parkway adjacent to the fire station site is included as Figure 8-1, Public Services Plan (Section 8).
- Phillip Road is a typical collector street with a 20- to 25-foot wide landscape corridor on the west and south side of the street with landscape screening and barrier requirements adjacent to the PGWWTP (see Design Guidelines, Section 12). There are three variations to the landscape treatments and road section for Phillip Road as reflected on Figure 7-9.

As previously noted, a median opening plan for all WRSP roadways with landscaped medians is included on Figure 7-10. The intent of this figure is to show in advance all left turn movements approved within the project. The goal is to minimize interruption to the landscape median in an effort to maximize landscaping and to facilitate efficient traffic flow. Additional detail is included in Section 12.2.1.7 of the Design Guidelines.

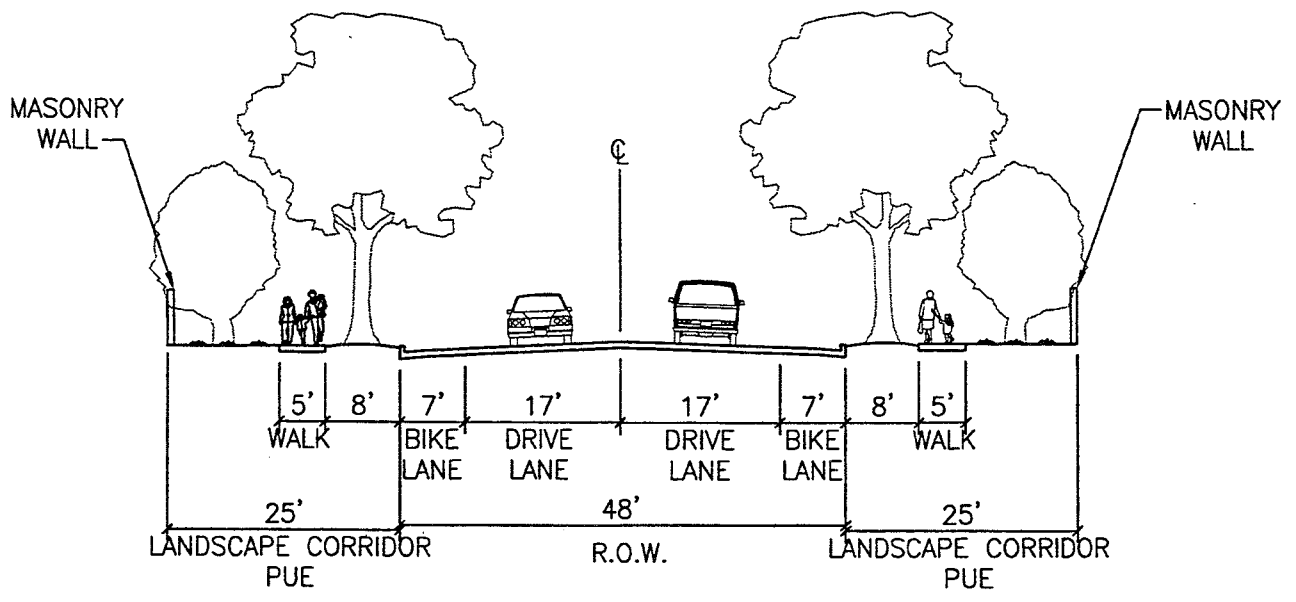
□ Local Roadways

Local streets provide direct access to abutting land uses and connections to collector streets. Local streets are 2-lanes, include on-street parking and adjacent 4-foot wide sidewalks. Provisions are included for both detached and attached sidewalks. The WRSP encourages the use of detached sidewalks and includes modified residential development standards to promote their use (see Land Use Plan, Section 4). In addition, the WRSP requires the use of single loaded roadways adjacent to paseo and open space areas, and the inclusion of entry elements at intersections with collector or arterial roadways. See Design Guidelines, for additional details.

- Primary Residential Streets (Figure 7-11) are used to accommodate higher traffic volumes and where Class III bike lanes are desired. Primary Residential Streets include two travel lanes with 46 to 56-feet of right-of-way.

Figure 7-7

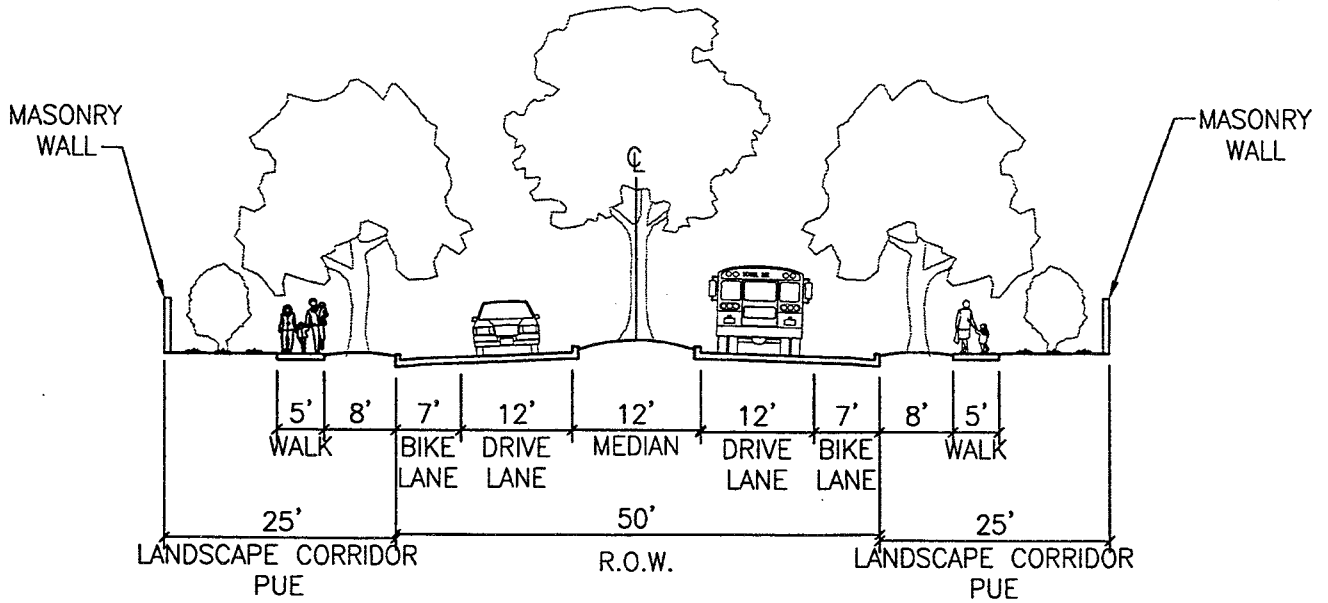
Typical Collector Street



48' R.O.W.

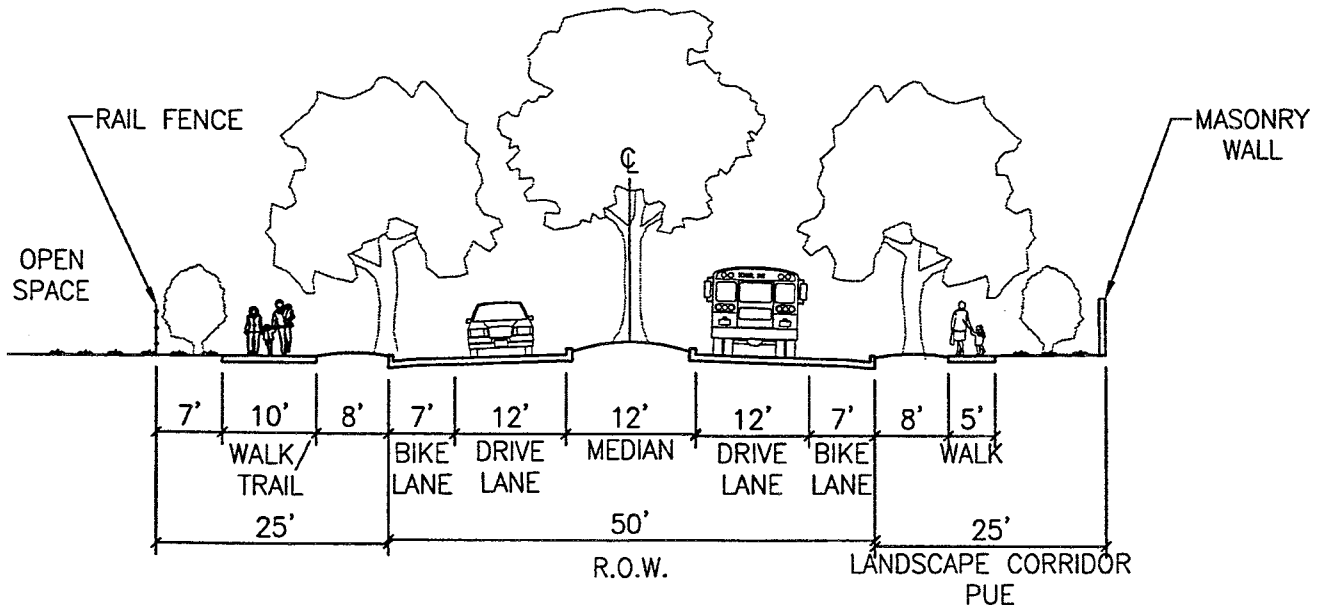
Figure 7-8

Hayden Parkway Adjacent to Land Use



50' R.O.W.

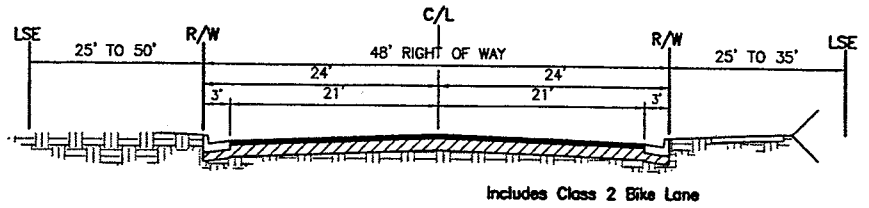
Hayden Parkway Adjacent to Open Space



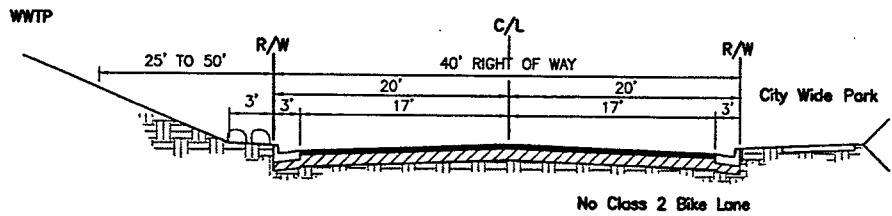
50' R.O.W.

Figure 7-9

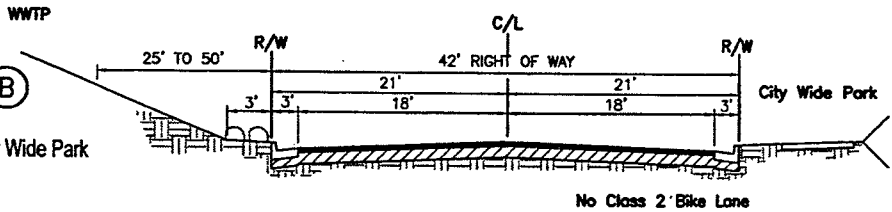
Phillip Road



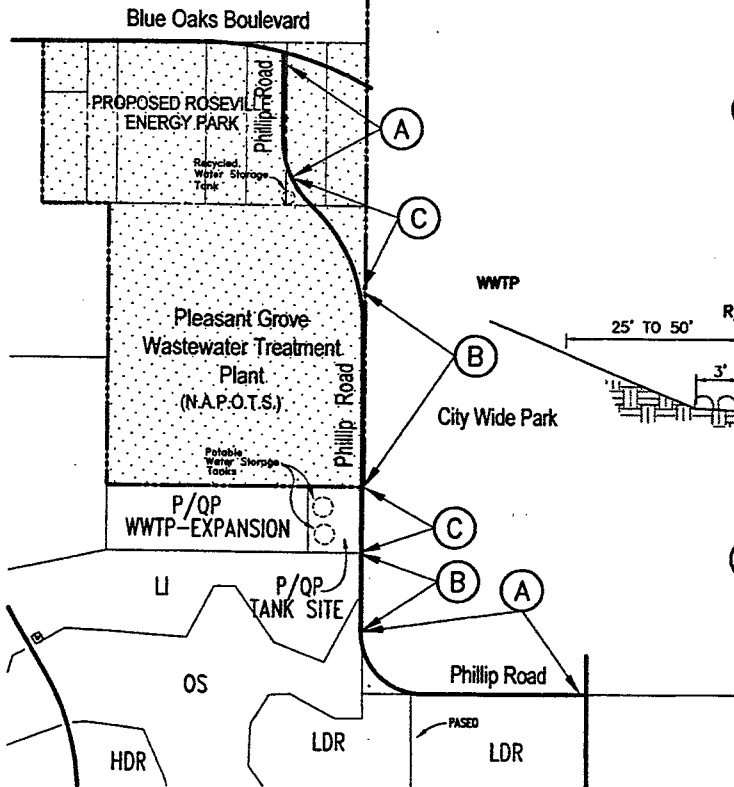
**(A) TYPICAL COLLECTOR STREET
48' RIGHT OF WAY**
NORTH OF WWTP &
ADJACENT TO RESIDENTIAL NTS



**(B) COLLECTOR STREET
40' RIGHT OF WAY**
ADJACENT TO WWTP NTS



**(C) COLLECTOR STREET
42' RIGHT OF WAY**
NE CORNER OF WWTP @
INTX OF EXIST. PHILLIP ROAD NTS



NOTE:

25' LSE ADJACENT TO RESIDENTIAL, 35' LSE ADJACENT TO NON RESIDENTIAL, 20' LSE REQUIRED ADJACENT TO PLEASANT GROVE WASTEWATER TREATMENT PLANT AND CITY PARK. (SEE FOOTNOTE #6 ON TABLE 7-1)

Figure 7-10

Median Openings

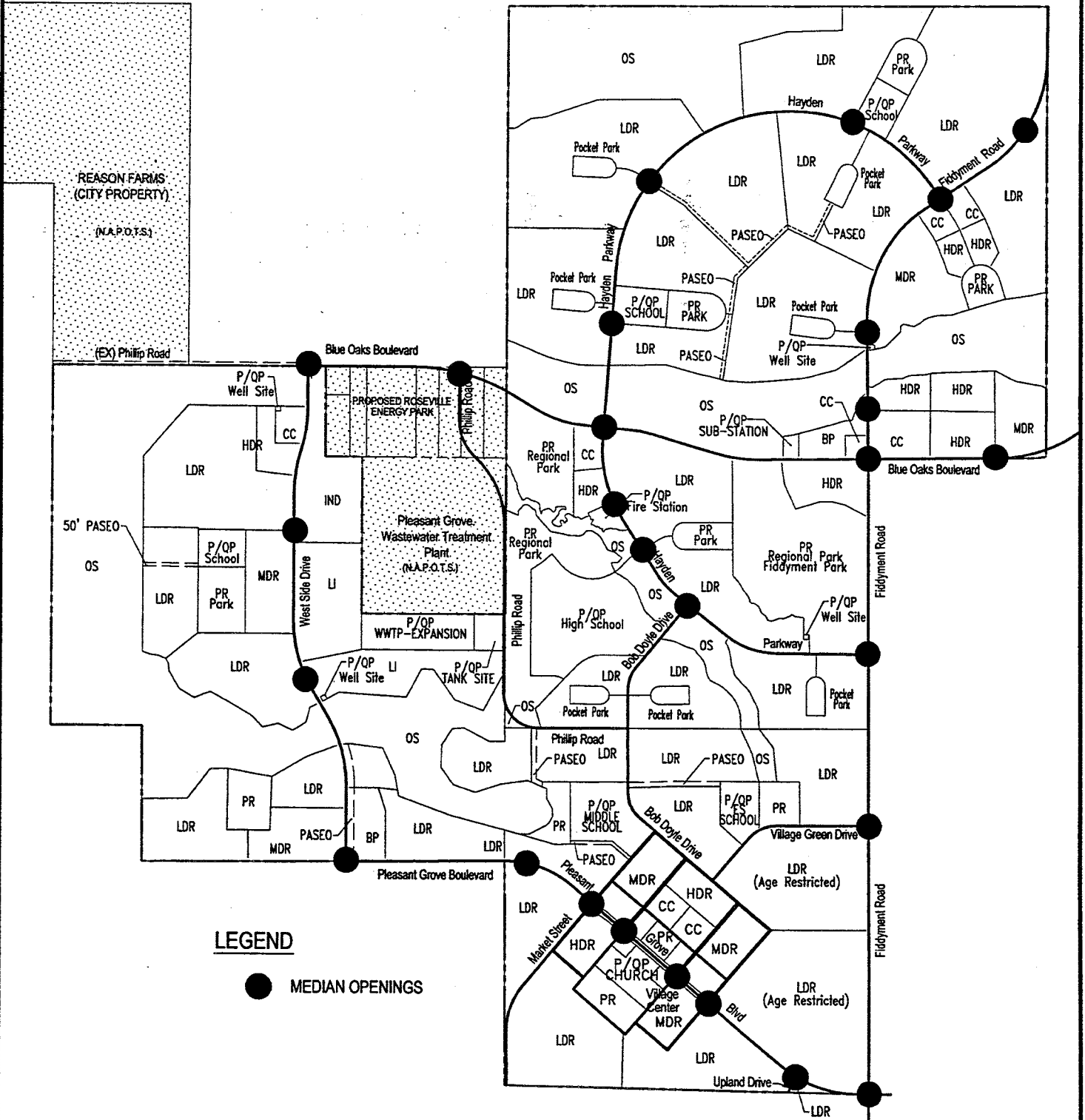
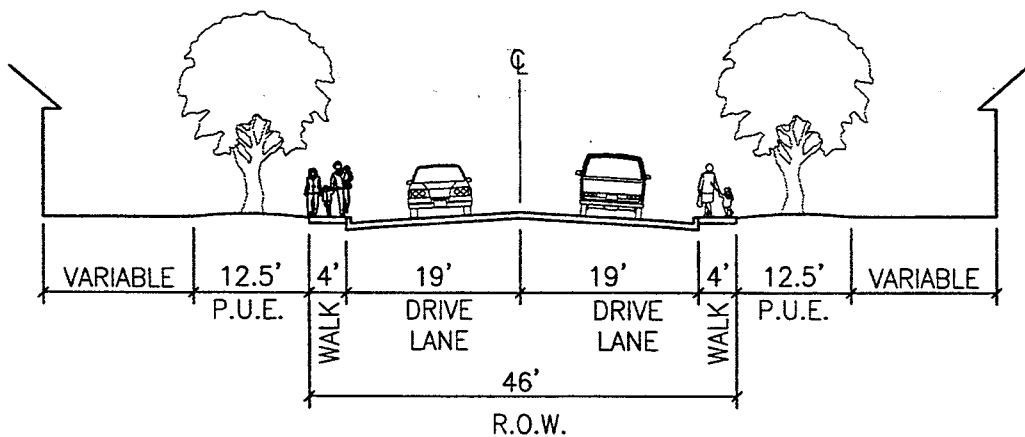


Figure 7-11

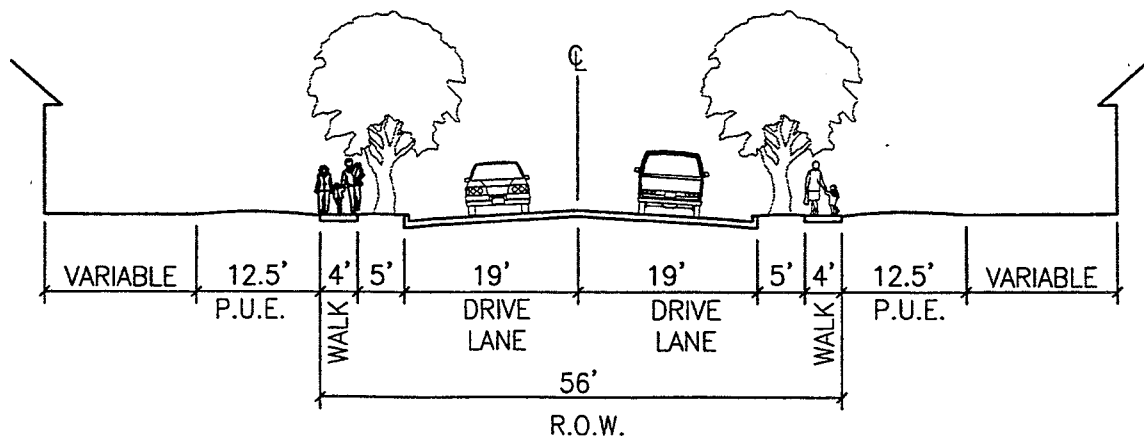
Primary Residential Streets

Residential Attached Sidewalk



46' R.O.W.

Residential Separated Sidewalk



56' R.O.W.

- Minor Residential Streets (Figure 7-12) carry lower traffic volumes with two travel lanes and 42' to 52' feet of right-of-way.

The pattern of local streets and use of detached or attached sidewalks will be determined through the subdivision map for individual projects. The local street pattern should maximize connectivity and, to the extent feasible, eliminate barriers among residential uses and parks, schools, open space and service uses. Additional street sections (alleys, roundabouts, etc.) may be considered through the subdivision map approval process.

Village Center

The Village Center is envisioned as the gathering place of the community modeled after a traditional small town center with preference given to the pedestrian. The Village Center is designated as a Pedestrian District in accordance with the City's General Plan. A Pedestrian District is intended to encourage increased pedestrian activity and improve walkability through enhanced safety, security, and convenience. Establishment of a Pedestrian District allows for construction and/or implementation of a number of design features, including pedestrian enhancements and traffic calming. In these districts, it is recognized that pedestrian travel takes a higher priority than automobile travel, which could reduce the vehicular level of service.

To further its pedestrian focus, the Village Center is anticipated to include a traditional grid pattern consisting of short blocks, straight streets and intersections at regular intervals. Besides a grid pattern, the Village Center will include enhanced pedestrian crossings, angled on-street parking, wide sidewalks, bus stops, buildings set forward immediately behind the sidewalks, pedestrian plazas and outdoor seating areas, pedestrian scale lighting and a dense tree canopy. Due to the specific design concepts, unique street cross sections have been developed exclusively for the Village Center. See Village Center Plan, Section 10, for specific details, cross sections, standards and guidelines.

Signalization

The locations of traffic signals within the WRSP are reflected in Figure 7-1. Traffic signals will be installed as specified in the WRSP development agreements and as warranted by the City of Roseville.

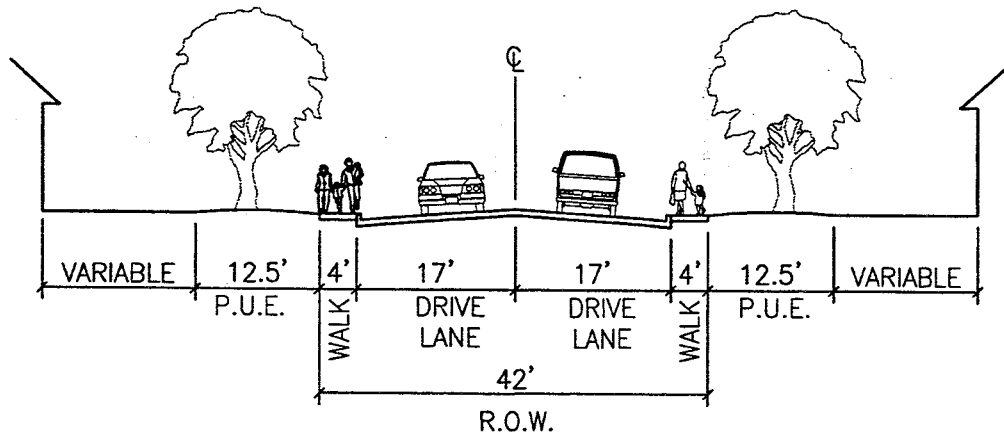
Placer Parkway

Placer Parkway is a proposed transportation facility that would connect SR 65 in the Roseville/Rocklin/Lincoln area to SR 70/99 in south Sutter County and the Sacramento International Airport. The intent of this facility is to create an additional east-west connector to accommodate inter-regional traffic and improve the movement of people and goods. Placer Parkway is anticipated to benefit the regional transportation system by providing an alternative to SR 65, I-80 and Baseline/Reigo Roads.

Figure 7-12

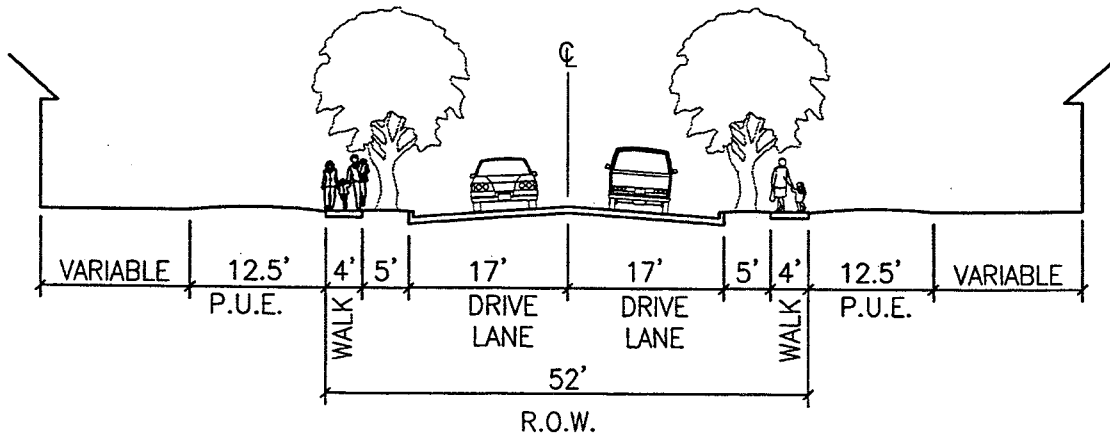
Minor Residential Streets

Residential Attached Sidewalk



42' R.O.W.

Residential Separated Sidewalk



52' R.O.W.

At the time of Specific Plan approval, the Placer County Transportation Planning Agency (PCTPA) had initiated a contract for environmental analysis of Placer Parkway. No alignment had been adopted, with Placer Parkway being studied for programming purposes along multiple alignments within 1000-foot-wide bands. Potential alignments within the WRSP include an alignment through the northern corner of the WRSP, and another conceptual alignment paralleling Baseline Road through the middle of the WRSP (see Figure 7-13). The alignments are conceptual and a final adopted alignment is subject to subsequent engineering and environmental studies.

It is the intent of the WRSP to not preclude an alignment for Placer Parkway. A substantial amount of non-urban land exists north of the WRSP that could accommodate feasible alignments. The northern corner of the WRSP has been designated as open space and could be used as a buffer adjacent to such alignment(s). Subsequent studies to determine an adopted alignment for Placer Parkway will need to consider WRSP land uses.

7.3

PEDESTRIAN AND BIKEWAY NETWORK






Bike Path

The pedestrian and bikeway network is an important component in ensuring connectivity and promoting non-vehicular travel in the WRSP. The network has been designed to allow movement throughout the Plan Area and provide linkages to the City's existing and planned network. The WRSP system includes sidewalks, on-street Class II and Class III bike lanes, and off-street Class I and Class IA bike paths. The Class I and Class IA system is reflected on Figure 7-14. As previously noted, the Village Center has been designated a Pedestrian District and includes enhanced pedestrian improvements (See Village Center Plan, Section 10).

Sidewalks

Sidewalks are required along all WRSP roadways. These include 4-foot wide attached or detached sidewalks along local streets, 5-foot wide detached sidewalks within landscape corridors along collector streets, and 8-foot wide detached sidewalks within landscape corridors along arterial streets. All 8-foot wide sidewalks along arterial streets are also considered Class IA bike paths.

Figure 7-13
West Roseville Specific Plan
Placer Parkway

-  Project Study Area For Placer Parkway
-  West Roseville Specific Plan
-  Sphere Amendment Remainder Area

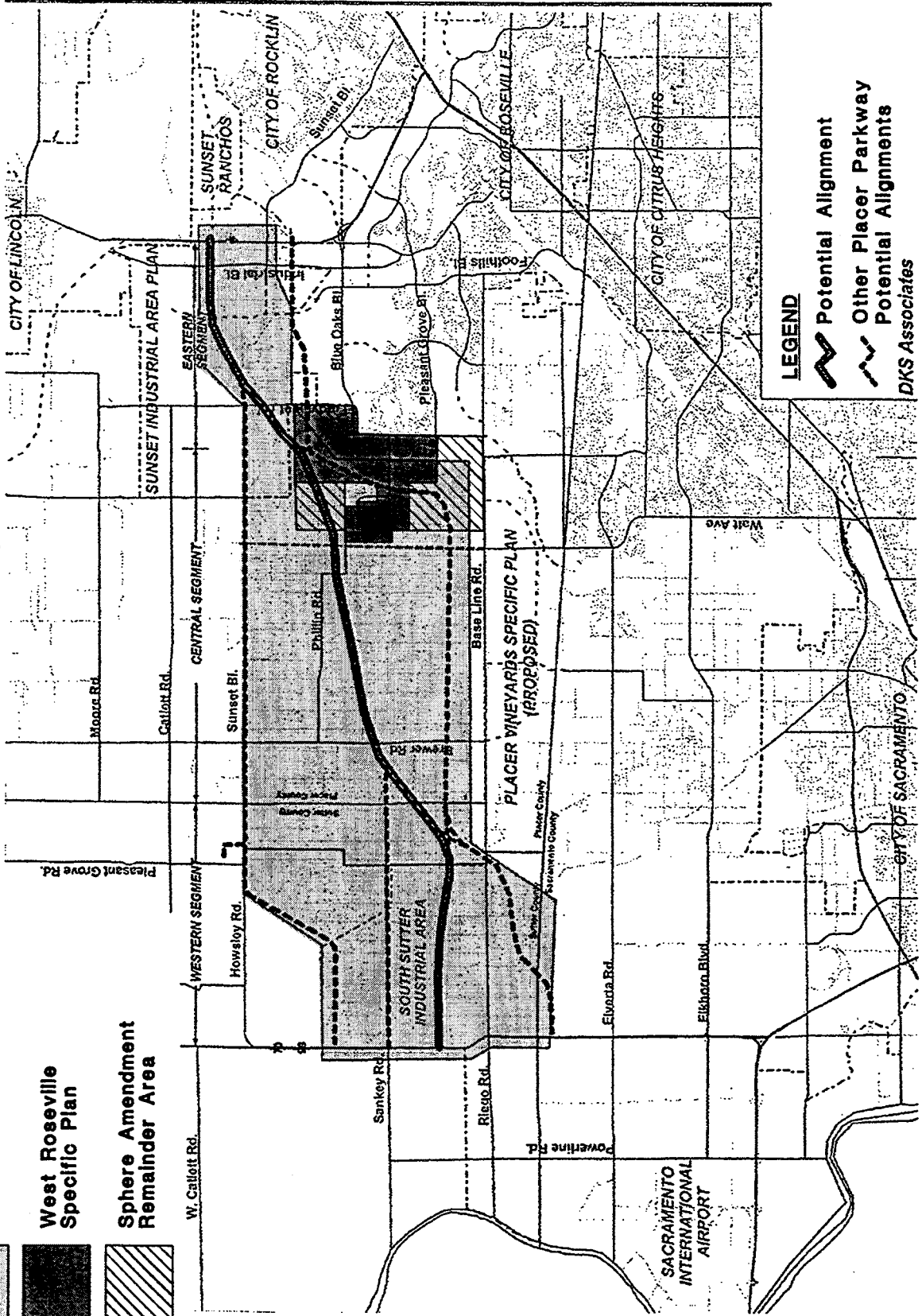
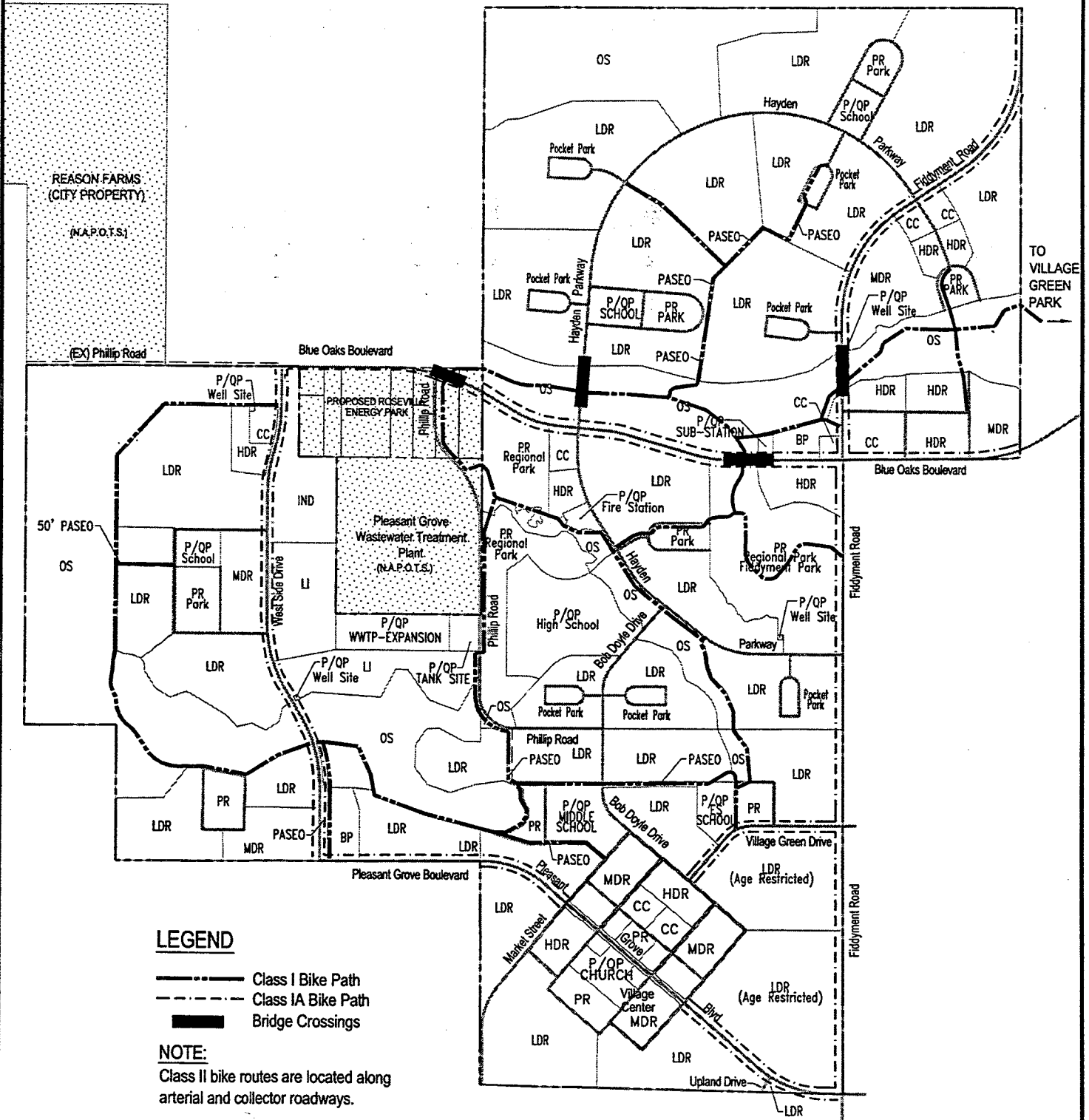


Figure 7-14

Bikeway System



LEGEND

- Class I Bike Path
- - - Class IA Bike Path
- █ Bridge Crossings

NOTE:

Class II bike routes are located along arterial and collector roadways.

Class III bike routes are located along primary residential streets.

Class II Bike Lanes and Class III Bike Routes

Approximately 28 miles of Class II bike lanes will be provided along all arterial and collector roadways within the WRSP. Class II bike lanes are designated on-street bike routes, 7-feet wide (consisting of four feet of pavement and three feet of curb and gutter) delineated with signage and painted stripes. Bicycle traffic is allowed along all local streets, with some potentially designated in accordance with Class III standards (signed/no striping).

Class I Bike Paths

When complete, the WRSP will provide approximately 12 miles of Class I, paved off-street bike paths along Plan Area open space, park and paseo areas. The WRSP Class I system provides connectivity within the Plan Area for both cyclist and pedestrians. In addition, the paths accommodate emergency and maintenance vehicle access to open space areas.

The Class I path along Pleasant Grove Creek provides for connection east to the existing Class I path in Veteran's Park within the North Roseville Specific Plan. To the west, this path may also provide the opportunity to link to the proposed multi-use stormwater retention facility/open space area downstream of the Plan Area. In addition, the Class I trail system will connect to Class II bike lanes at various points along major WRSP roadways. Connections are included along Blue Oaks Boulevard, Pleasant Grove Boulevard, Village Green Drive, Fiddymont Road, Hayden Parkway, and an off-site connector to Veteran's Park east of the Plan Area.

The Class I system has been designed to minimize barriers and reduce potential travel disruptions. Bike path bridges have been included over Kaseberg Creek in Fiddymont Park, as well as over a tributary adjacent to Pleasant Grove Creek. The path will pass under Hayden Parkway and Fiddymont Road where these roads cross Pleasant Grove Creek. The Class I bike path will also pass under Blue Oaks Boulevard at two locations near the Phillip Road intersection with Blue Oaks, and in the vicinity of Fiddymont Park. At grade crossings of streets will occur where the Class I path intersects at or near signalized intersections (see Figure 7-14 for bridge crossings).

The WRSP promotes frequent connections between the Class I system and adjacent uses. Where a single loaded street abuts open space, park or paseo, the Class I path (separated from street) may replace the standard sidewalk on the open space side of the street. Where a cul-de-sac or loop street, multi-family or non-residential project abuts the Class I path, a paved link shall be provided to the path to the extent feasible. The Class I system within an open space area may meander to minimize environmental impacts and create visual interest, but in no instance shall the path be closer than 15-feet from the property line of a single-family residential parcel. In accordance with City

standards, all Class I paths require 10-feet of pavement with a 2-foot decomposed granite/gravel shoulder on each side (see Design Guidelines, Section 12, for additional detail).

Barriers (bollards, rail fence, post and cable, posts, etc.) shall be provided along bike paths within open space preserve areas regulated by a Section 404 permit issued under the federal Clean Water Act. Such barriers shall comply with the 404 permit regarding use of the preserve area, and with City design, maintenance and public safety requirements.

7.4

PUBLIC TRANSIT

Roseville Transit provides fixed route and Dial-A-Ride services within the City, as well as fixed route commuter services between Roseville and downtown Sacramento. The fixed route local and commuter systems operate on regularly scheduled routes, with the Dial-A-Ride system providing demand responsive curb-to-curb service. Roseville Transit users can connect to both Placer County Transit (PCT) and to Sacramento Regional Transit (RT) at designated transfer points. The transfer points are at the Galleria Mall and Orlando Avenue at Lois Street.

To facilitate the expansion and use of transit, the highest intensity land uses in the WRSP have been located within close proximity to major transportation corridors and potential transit stops. These uses include high density residential, employment, regional park and the Village Center.

Bus turnouts and shelters shall be located and constructed in accordance with City Improvements Standards and as otherwise required by the Public Works Director for specific projects.

7.5

PARK & RIDE

Park and ride lots provide parking for commuters to leave their vehicles to meet carpools, vanpools or access transit. In the WRSP, a total of five park and ride lots are dispersed throughout the Plan Area in commercial, office, industrial and park locations. Each designated location requires the inclusion of 20 park and ride spaces. Park and Ride spaces are in addition to the minimum required parking spaces for each project. These spaces will be installed with project development and maintained by the project developer, with all designated spaces signed in accordance with City standards. Park and Ride lots shall be available from sun-up to sundown on a daily basis. Specific park and ride locations are reflected on Figure 7-15.

7.6

TRANSPORTATION SYSTEM MANAGEMENT

Transportation System Management (TSM) measures are designed to reduce the number and length of home-to-work commute trips through actions such as ridesharing, flexible work hours and support of public transportation. Any project site, common work location or employer with 50 or more employees is required to comply with the City of Roseville TSM ordinance and shall incorporate TSM measures to the degree required by the ordinance.

8 PUBLIC SERVICES PLAN

The West Roseville Specific Plan (WRSP) will provide public services necessary to meet the needs of Plan Area residents, in accordance with the policies of the City's General Plan. Phasing and financing obligations relating to public services are outlined in the Specific Plan development agreements and in Implementation and Administration, Section 11, of the Specific Plan. Table 8-1 summarizes the public service providers to the WRSP.

Table 8-1: WRSP Service Providers

Service	Provider
Fire Protection	City of Roseville
Police Protection	City of Roseville
Schools	Roseville City School District Roseville Joint Union High School District
Parks and Recreation	City of Roseville
Library	City of Roseville

8.1 FIRE PROTECTION

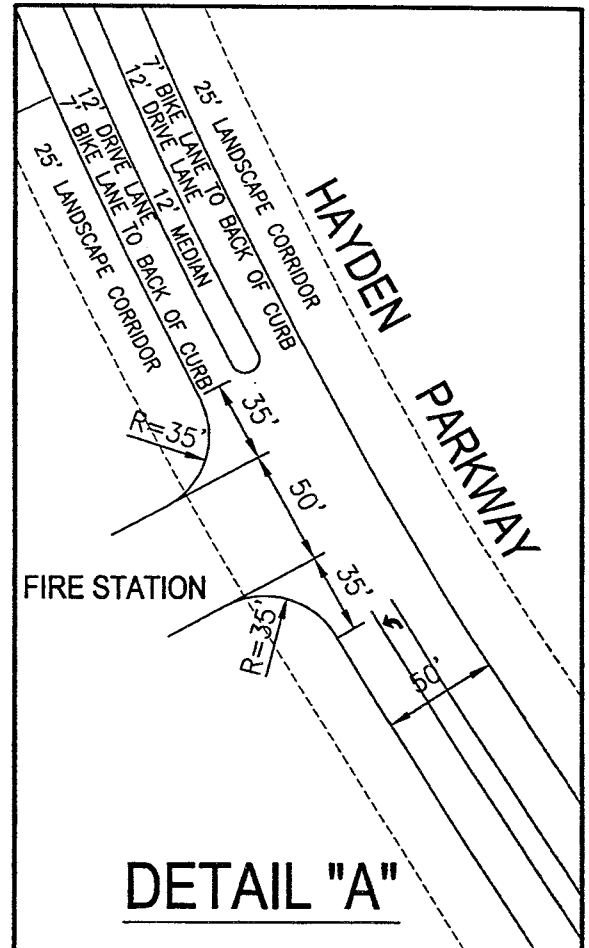
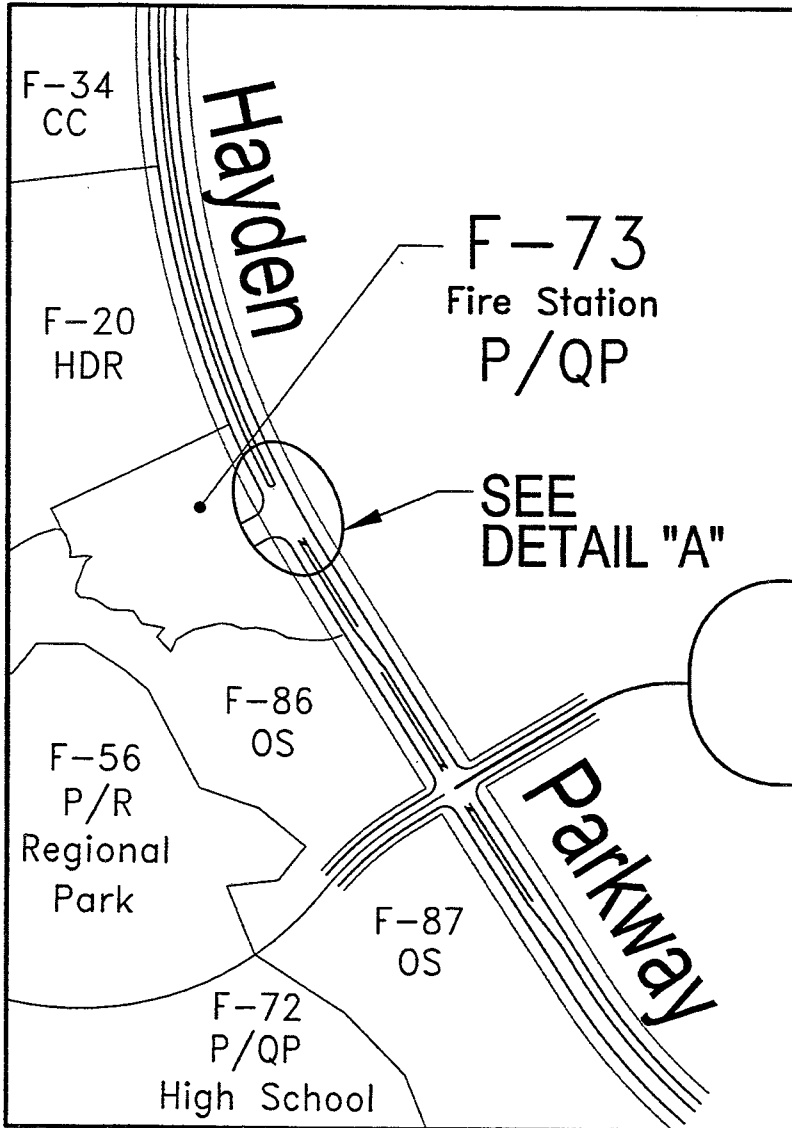
The Roseville Fire Department will provide fire protection, suppression, emergency medical services and hazardous materials management to the Plan Area. A 3.1-acre fire station site (Parcel F-73) is designated within Phase 1 of the WRSP west of Hayden Parkway and south of Blue Oaks Boulevard, east of the Regional Sports Park (see Figure 8-1 for station location and required Hayden Parkway median break detail). This station will provide first response within the Plan Area. Timing of construction and staffing of the fire station will be consistent with the Fire Department Standards of Response Coverage Study. Stations located in adjacent portions of the City will provide interim and secondary response.

8.2 POLICE PROTECTION

The Roseville Police Department will serve the WRSP. The Police Department provides all operations and patrols out of its central station located on Junction Boulevard, approximately four miles from the Plan Area. The WRSP will comply with City of Roseville Police Department recommendations regarding safety and security.

Figure 8-1

Fire Station Site and Median Break



8.3 SCHOOLS

The WRSP is within the boundaries of the Roseville City School District (grades K-8) and Roseville Joint Union High School District (grades 9 to 12). The Plan Area will generate an estimated 2,288 elementary school (K-5) students, 984 middle school (6-8) students and 1,463 high school (9-12) students, as shown on Table 8-2.

Table 8-2: Student Generation

	LDR/ MDR ¹	HDR ²	Students Generated	School Capacity	Schools Required
Roseville City School District					
Grades K-5	0.365	0.073	2,288	600	3.8
Grades 6-8	0.158	0.028	984	1,000	1.0
Roseville Joint Union High School District					
Grades 9-12	0.227	0.067	1,463	1,800	0.8

1. 5,906 LDR/MDR units and 1,814 HDR units assumed. Age-restricted units not included in total.
2. Includes 40 units assigned to the Village Center Commercial use.

Within the WRSP, four elementary school sites, a minimum of eight and up to ten acres each, are planned (Parcels W-70, W-75, F-70, F-71). One elementary school is located in each phase of the project. In addition, a 20-acre middle school (Parcel W-73) is planned northeast of the Village Center south of Pheasant Run. All elementary and middle schools are located adjacent to neighborhood park sites to promote joint use opportunities and create a local activity amenity.

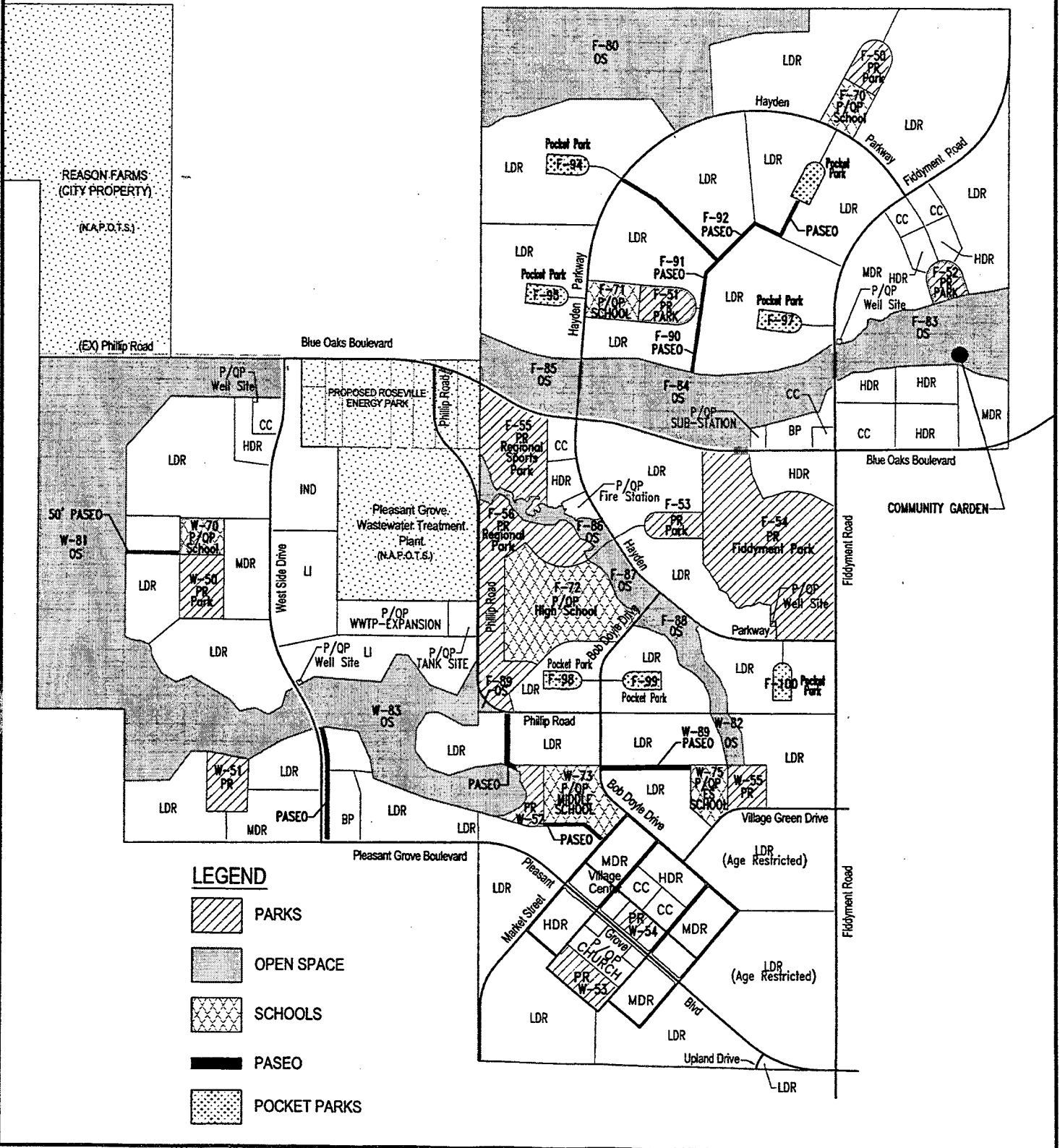
A 53-acre high school site (Parcel F-72) is located in the central portion of the WRSP, west of Hayden Parkway adjacent to the Regional Sports Park. The high school and sports park are part of the WRSP Activity Core, providing for education, active and passive recreation, joint use opportunities and community activity. The high school is anticipated to serve students generated both within and outside the Plan Area.

School sites within the WRSP are reserved for the school districts. Facility planning and the sequencing of development of these sites is to be determined by the districts. The WRSP is required to fully mitigate school impacts in accordance with the Specific Plan development agreements and funding agreements with the respective school districts.

WRSP School sites are reflected on Figure 8-2.

Figure 8-2

Schools, Parks and Open Space



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8.4 PARKS AND RECREATION

Approximately thirty percent (30%) of the WRSP is planned for park and open space use. The parks and open space program provides for a range of active and passive recreational opportunities that exceed the City's General Plan requirement of 9 acres of parkland per 1,000 residents. The WRSP includes: active neighborhood, community and regional park sites; and credited open space and paseo areas. Construction of park and recreation facilities as described in this document is dependant upon adequate construction and long-term maintenance funding.

The placement and sizing of parks is reflective of community need, General Plan policy, proximity to users, ability to promote joint use activities, and existence of natural resources such as floodplain, oak woodland and vernal pool preservation. Parks are generally located within neighborhoods creating a local focal point and activity amenity. In addition, when feasible, park sites have been located adjacent to or connected with open space areas. The resulting interconnected open space and park network provides for pedestrian and bicycle access throughout the WRSP with connections outside the Plan Area.

The general location of WRSP parks and open space is included on Figure 8-2. Details relating to dedication, operation, funding, phasing and construction is included in the Specific Plan development agreements.

□ City of Roseville Park and Recreation Requirements

The WRSP provides recreation facilities, parkland and open space to comply with the policies and requirements of the City's General Plan Parks and Recreation Element. The General Plan requires the provision of nine (9) acres of parkland per 1,000 residents. The General Plan requirement is comprised of three (3) acres of city-wide park, three (3) acres of neighborhood and community park, and three (3) acres of credited open space.

The 8,430 dwelling units in the WRSP will generate an estimated population of 20,810 residents based on an average household size of 2.54 residents for conventional (non age-restricted) development and 1.8 residents for age-restricted development. The City's requirement of nine (9) acres per 1,000 residents requires a total of 187.2 credited acres of parkland in the Plan Area. WRSP park dedication requirements are summarized in Table 8-3.

Table 8-3: Park Dedication Requirements

	General Plan Standard	Acreage Required
City-wide Park	3 acres per 1,000 residents	62.4 acres
Neighborhood/Community Park	3 acres per 1,000 residents	62.4 acres
Open Space	3 acres per 1,000 residents	62.4 acres
Total Parkland	9 acres per 1,000 residents	187.2 acres

□ Credited Parkland and Open Space

The WRSP designates a total of 949.2 acres in park and open space uses. This total includes 250.1 acres of active neighborhood and city-wide parkland, and 684.6 acres of open space and paseo. Of the 250.1 acres of active parkland, 166.6 acres are designated for city-wide park and 83.5 acres for neighborhood parks. Each acre set aside for active park use is credited as a full acre towards meeting the General Plan park dedication requirement.

In addition to meeting the City's standards for active parkland, the WRSP provides additional passive recreation opportunities in the form of open space areas. Partial credit may be granted for open space areas that include non-traditional informal recreational facilities and open space amenities. Each acre set aside for open space and paseo is credited at 0.10 acre (one acre of credit for each 10 acres dedicated). In total, the 949.2 acres of parks and open space provided in the WRSP are granted a credit of 320.01 acres towards meeting the General Plan park dedication requirement. This credit exceeds the dedication requirement for the WRSP by 132.81 acres. A summary of WRSP park and open space credits is included on Table 8-4.

□ Credited Park Descriptions

The following provides a brief overview of the various components that constitute the WRSP credited park and open space program. Park sites within the WRSP have been designed to include facilities required by the Parks Vision 2010 Master Plan. Additional detail relating to those parks within the Village Center is included in Section 10, Village Center Plan.

Table 8-4: Summary of Park and Open Space Lands and Credits

Parcel	Type	Acreage	Credit Ratio	Credited Acreage
City-wide Parks				
F-54	City-wide Park	91.0	1:1	91.0
F-55	City-wide Park	29.8	1:1	29.8
F-56	City-wide Park	45.8	1:1	45.8
Subtotal		166.6		166.6
Neighborhood/Community Parks				
F-50	Neighborhood Park	7.8	1:1	7.8
F-51	Neighborhood Park	8.9	1:1	8.9
F-52	Neighborhood Park	5.9	1:1	5.9
F-53	Neighborhood Park	6.0	1:1	6.0
W-50	Neighborhood Park	14.1	1:1	14.1
W-51	Neighborhood Park	12.1	1:1	12.1
W-52	Neighborhood Park	7.9	1:1	7.9
W-53	Neighborhood Park (VC)	9.1	1:1	9.1
W-54	Neighborhood Park (VC)	3.5	1:1	3.5
W-55	Neighborhood Park	8.2	1:1	8.2
Subtotal		83.5		83.5
Open Space				
F-80	Open Space	132.7	0.1:1	13.27
F-83	Open Space	54.9	0.1:1	5.49
F-84	Open Space	81.2	0.1:1	8.12
F-85	Open Space	26.4	0.1:1	2.64
F-86	Open Space	12.2	0.1:1	1.22
F-87	Open Space	10.3	0.1:1	1.03
F-88	Open Space	16.7	0.1:1	1.67
F-89	Open Space	0.8	0.1:1	0.08
W-81	Open Space	267.0	0.1:1	26.70
W-82	Open Space	5.2	0.1:1	0.52
W-83	Open Space	77.2	0.1:1	7.72
F-90	Paseo	1.1	0.1:1	0.11
F-91	Paseo	0.9	0.1:1	0.09
F-92	Paseo	3.0	0.1:1	0.3
F-93	Paseo	1.1	0.1:1	0.11
W-84	Paseo	0.9	0.1:1	0.09
W-85	Paseo	1.1	0.1:1	0.11
W-87	Paseo	2.6	0.1:1	0.26
W-88	Paseo	1.5	0.1:1	0.15
W-89	Paseo	2.3	0.1:1	0.23
Subtotal		699.1		69.91
TOTAL		949.2		320.01

City-wide Parks

The WRSP includes two city-wide parks, Fiddymment Park and the Regional Sports Park. These parks, combined with the adjacent high school site, form the WRSP Activity Core as described in Community Form and Planning Principles, Section 3. Strong interconnectivity between these facilities is important to support joint use and community activity. A continuous greenway comprised of park and open space will connect Fiddymment Park, the Regional Sports Park and the high school site. The greenway includes a Class I bike path connection from the Regional Sports Park east through open space (Parcel F-86), to Park F-53 and to Fiddymment Park.

Fiddymment Park is a 91-acre city-wide park (Parcel F-54) located south of Blue Oaks Boulevard and west of Fiddymment Road. Fiddymment Park may potentially contain a variety of active and passive recreation opportunities at buildout including bike and pedestrian paths, an outdoor bandstand area, activity greens, a disc golf course and a small multi-purpose center. A City groundwater well site will also be included. The park encumbers the highest concentration of oak woodlands within the WRSP, and will be highly visible as an entry to the Plan Area. Figure 8-3 provides a conceptual plan for Fiddymment Park. Included is a listing of WRSP fee improvements and potential future amenities not associated with development of the WRSP.

Regional Sports Park is a 75.6-acre city-wide park (Parcels F-55 and F-56) located adjacent to the high school site, south of Blue Oaks Boulevard between Phillip Road and Hayden Parkway. Included on the property is the historic Fiddymment Ranch Main Complex (see Resource Management, Section 6.4). The Sports Park is envisioned as a regional facility available for tournaments and local league play. The combined Regional Sports Park and adjacent high school site will include various recreational joint use facilities such as soccer fields, baseball fields, a lighted soccer/football stadium, tennis courts, softball fields, an outdoor swimming pool, basketball courts and a 400 meter track. The combined Regional Sports Park/high school site includes a concession area with restrooms, outdoor seating and parking lots. Figure 8-4 provides a conceptual plan and listing of potential improvements for the Regional Sports Park.

Neighborhood Parks

The WRSP includes ten (10) neighborhood park sites distributed throughout the Plan Area. These sites range in size from 3.5 to 14.1 acres. Neighborhood parks include a variety of facilities to accommodate local recreation needs including soccer and baseball fields, hard courts, playgrounds, tot lots and picnic areas. Five of the neighborhood parks are located adjacent to schools to support joint use activities. The neighborhood parks create nodes of activity within the neighborhoods.

Conceptual plans for the neighborhood parks, along with a listing of potential amenities, are included as Figures 8-5 through 8-12. Conceptual plans for the parks within the Village Center (Parcels W-53 and W-54) are included in Section 10, Village Center Plan.

Open Space Areas

Open space areas are a significant visual amenity and help to define the form of the WRSP. Open space provides for a variety of functions including passive recreation, view corridors, resource preservation, and stormwater drainage and flood water conveyance. In addition, open space areas can improve the interface between uses and define the City edge. The WRSP open space system encompasses a majority of the significant Plan Area resources including the trees and wetlands that follow the Pleasant Grove Creek, Curry Creek and Kaseberg Creek corridors. In most locations, open space corridors may accommodate Class 1 bike path systems providing interconnectivity and allowing residents to walk, jog or bike through the WRSP. Other facilities may be incorporated including rest stops, benches, overlooks and habitat interpretation areas. In addition, open space areas will accommodate public utilities, roadway crossings and fire protection activities including fire breaks. Open space use will be restricted in accordance with the Preserve O&M Plan to be developed as a condition of the project's 404 permit.

A 2-acre community garden is planned within open space parcel F-83 (see Figure 8-2). A small parking area (approximately 35 spaces) will be provided for the community garden and for use by trail users.

Paseos

Paseos are "created" landscaped open space corridors generally ranging from 25 to 75 feet in width. They include bike paths and are intended to provide linkages through neighborhoods to parks, schools, and commercial areas. In most cases, paseos will be located adjacent to a residential street, or as a greenway extending through neighborhoods. Through the inclusion of paseos, the WRSP is able to provide an uninterrupted Class 1 bike path through the Plan Area. In addition to trails, paseos may accommodate other informal recreation activities such as picnic tables. Paseos are to remain visible, adjacent to a single loaded street or other open area on a minimum of one side (see Design Guidelines, for additional detail). Paseos will be owned and maintained by the City.



Typical Paseo

Figure 8-3: Fiddymment Park Conceptual Plan

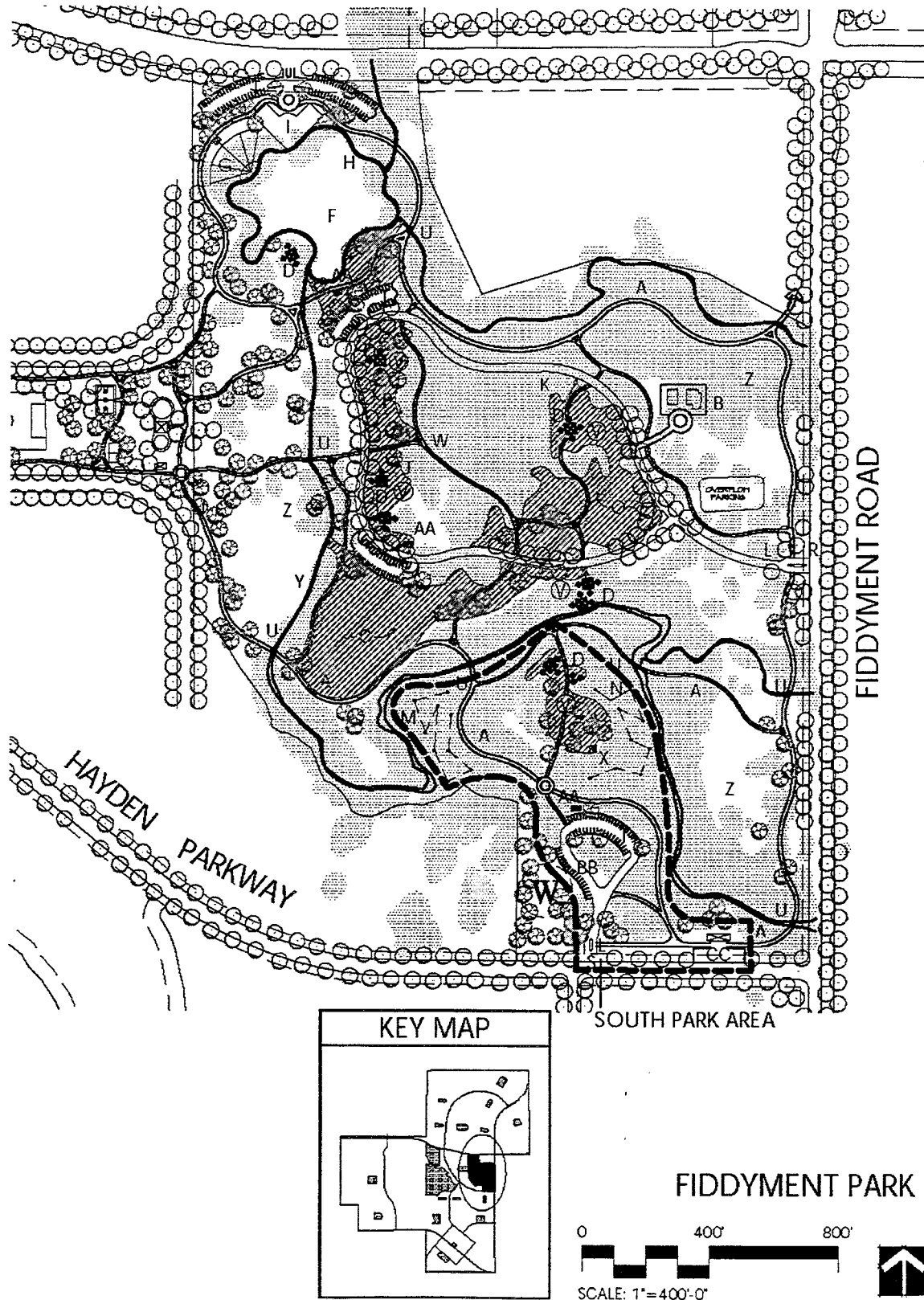



Figure 8-3: Fiddymment Park Conceptual Plan Legend

FIDDYMENT PARK F-54 AMENITIES
FIDDYMENT PARK (90.9 ACRES)
A. MULTI-USE TRAIL
B. CARETAKER HOME
C. CENTRAL FESTIVAL GROUNDS
D. GROUP PICNIC AREAS (LARGE)
E. GROUP PICNIC AREAS (MEDIUM)
F. STORM WATER DETENTION MEADOW
G. VILLAGE GREEN
H. NATURE TRAIL
I. 10,000 SQUARE FT. MULTIPURPOSE CENTER
J. MONUMENT HILL
K. 24' ACCESS ROAD
L. PARK SIGNAGE
M. DISC GOLF FRONT NINE
N. DISC GOLF BACK NINE
O. EAST ACTIVITY GREEN
P. WEST ACTIVITY GREEN
Q. SOUTH ACTIVITY GREEN
R. GATED FIDDYMENT ROAD ENTRY
S. GATED HAYDEN PARKWAY ENTRY
T. PARKING
U. FOOT BRIDGE
V. ADVENTURE PLAYGROUND
W. PEDESTRIAN TRAIL
X. HORSESHOE PITS
Y. EXISTING CREEKWAY
Z. EXISTING OAK GROVES/ GRASSLANDS
AA. RESTROOM/CONCESSION BUILDING
BB. PARK AND RIDE PARKING
CC. BUS STOP
 WELL SITE

WRSP FEE IMPROVEMENT AMENITIES
SOUTH PARK AREA (16.2 ACERS)
A. MULTI-USE TRAIL
D. GROUP PICNIC AREAS (LARGE)
L. PARK SIGNAGE
M. DISC GOLF FRONT NINE
N. DISC GOLF BACK NINE
S. GATED HAYDEN PARKWAY ENTRY
T. PARKING
V. ADVENTURE PLAYGROUND
W. PEDESTRIAN TRAIL
X. HORSESHOE PITS
Y. EXISTING CREEKWAY
Z. EXISTING OAK GROVES/ GRASSLANDS
AA. RESTROOM/CONCESSION BUILDING
BB. PARK AND RIDE PARKING
CC. BUS STOP

NOTE:
WRSP FEE IMPROVEMENTS WILL BE
CONSTRUCTED THROUGH PARK FEES
COLLECTED WITHIN THE WRSP. OTHER LISTED
IMPROVEMENTS ARE POTENTIAL FUTURE
AMENITIES, THE CONSTRUCTION OF WHICH ARE
NOT ASSOCIATED WITH THE DEVELOPMENT OF
THE WRSP.

Figure 8-4: Regional Sports Park Conceptual Plan

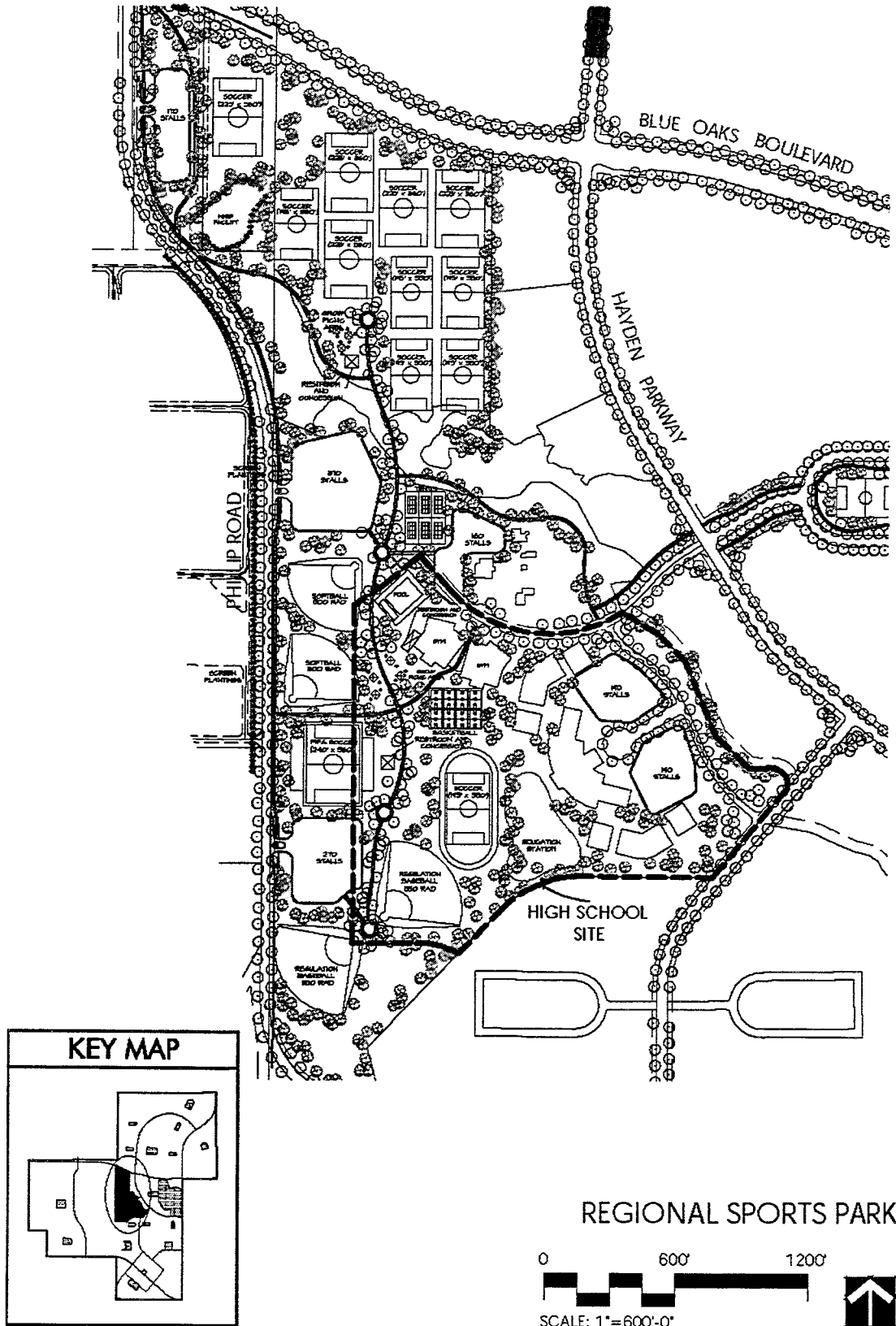


Figure 8-4: Regional Sports Park Conceptual Plan Legend

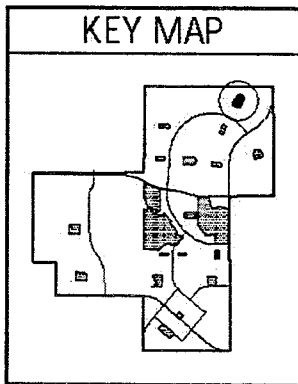
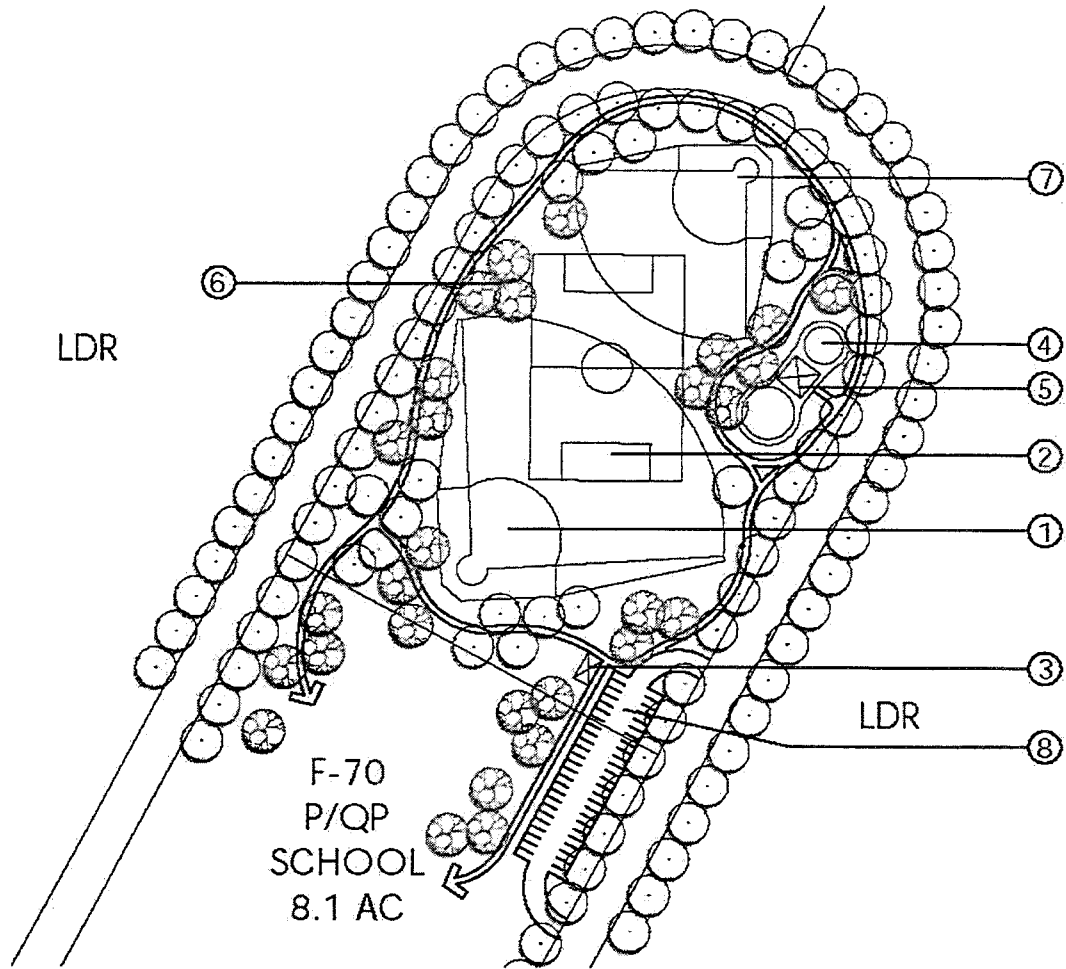
REGIONAL SPORTS PARK AMENITIES
<p>REGIONAL SPORTS PARK PROPERTY</p> <ul style="list-style-type: none"> • LIGHTED SOCCER FIELDS (10) • LIGHTED SWIMMING POOL • LIGHTED PARKING - 970 CARS • SPECIAL EVENTS FACILITY/FIDDYMENT HOUSE • TENNIS COURTS (6) • LIGHTED SOFTBALL DIAMONDS (2) • LIGHTED SYNTHETIC TURF FOOTBALL/SOCCER STADIUM • BASEBALL DIAMONDS (2) • SITE AMENITIES <ul style="list-style-type: none"> - CONCESSION/RESTROOM (2) - GROUP PICNIC AREA (3) - DRINKING FOUNTAINS - PATHWAYS - INFORMAL TURF AREAS - BENCHES - BICYCLE PARKING - TRASH/RECYCLE CANS - SHADE TREES - TELEPHONES - PICNIC/BARBEQUE FACILITIES

HIGH SCHOOL AMENITIES
<p>HIGH SCHOOL PROPERTY</p> <ul style="list-style-type: none"> • BASKETBALL COURT (8) • LIGHTED ALL WEATHER 400M TRACK/ TURF SOCCER FIELD • LIGHTED PARKING - 380 CARS • CONCESSION/RESTROOM (1) • SCHOOL GYMNASIUM

REGIONAL SPORTS PARK AMENITIES
<p>CONSTRUCTION AMENITIES FUNDED BY DISTRICT</p> <ul style="list-style-type: none"> • BASEBALL DIAMONDS (2) • LIGHTED SYNTHETIC TURF FOOTBALL/SOCCER STADIUM • TENNIS COURTS (6) • SWIMMING POOL • LIGHTED PARKING-270 CARS

REGIONAL SPORTS PARK AMENITIES
<p>CONSTRUCTION AMENITIES FUNDED JOINTLY BY DISTRICT AND CITY</p> <ul style="list-style-type: none"> • LIGHTED SOFTBALL DIAMONDS (2) • SWIMMING POOL SHOWER FACILITIES • SWIMMING POOL PARKING - 160 CARS

Figure 8-5: Park F-50 Conceptual Plan



PARK F-50 AMENITIES	
7.8 ACRE/JOINT USE SITE	
1.	SOFTBALL DIAMOND (300' RAD)
2.	YOUTH SOCCER (180'x270')
3.	RESTROOM
4.	CHILDREN'S PLAY AREA
5.	GROUP PICNIC AREA
6.	INFORMAL PICNIC AREAS
7.	YOUTH BASEBALL (200' RAD)
8.	JOINT USE PARKING (16)

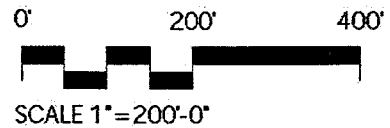


Figure 8-6: Park F-51 Conceptual Plan

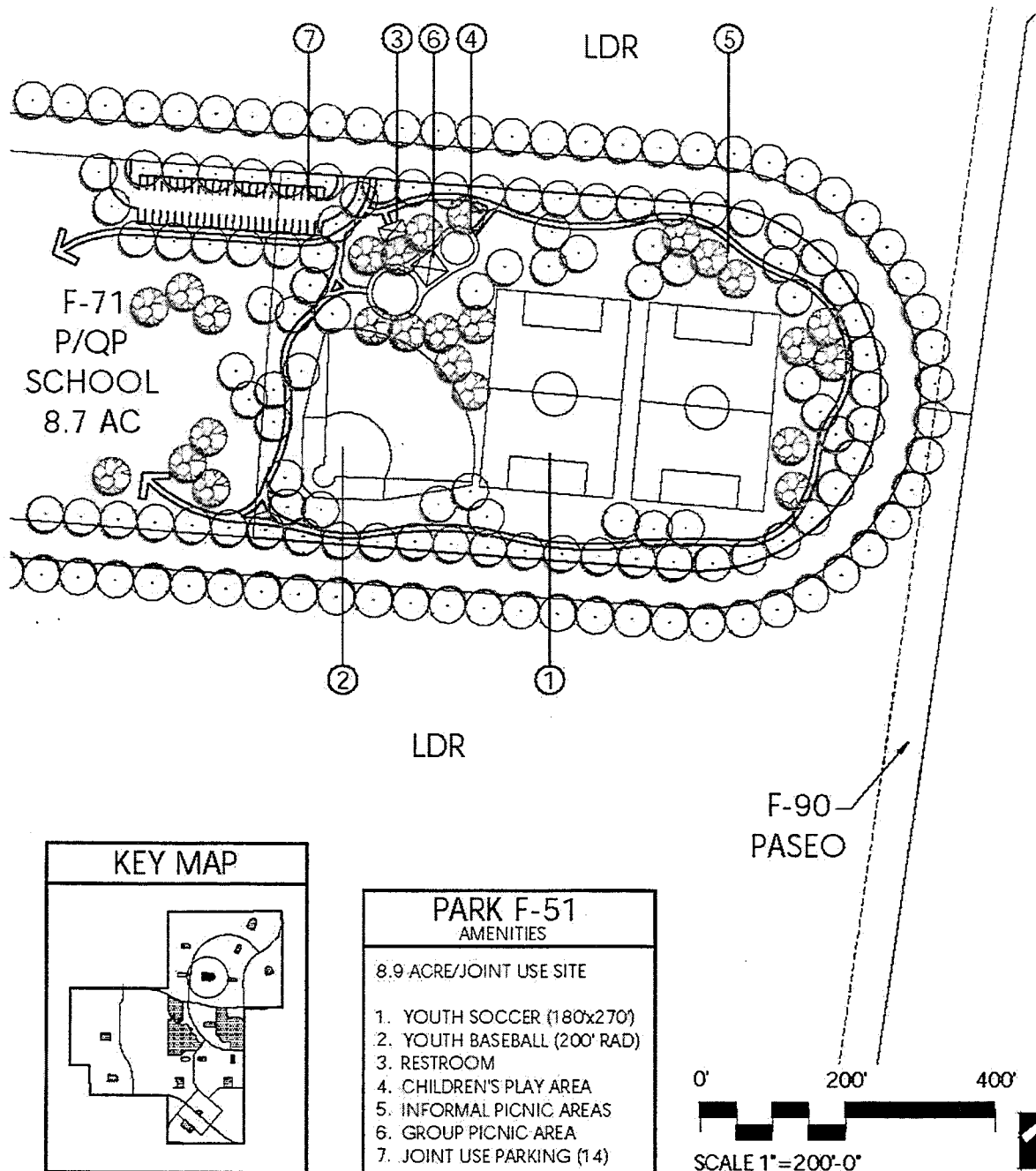
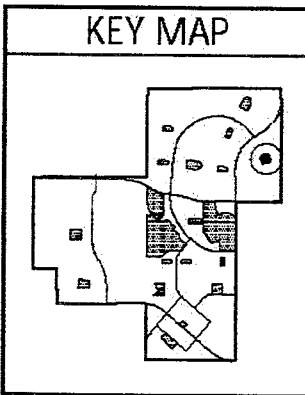
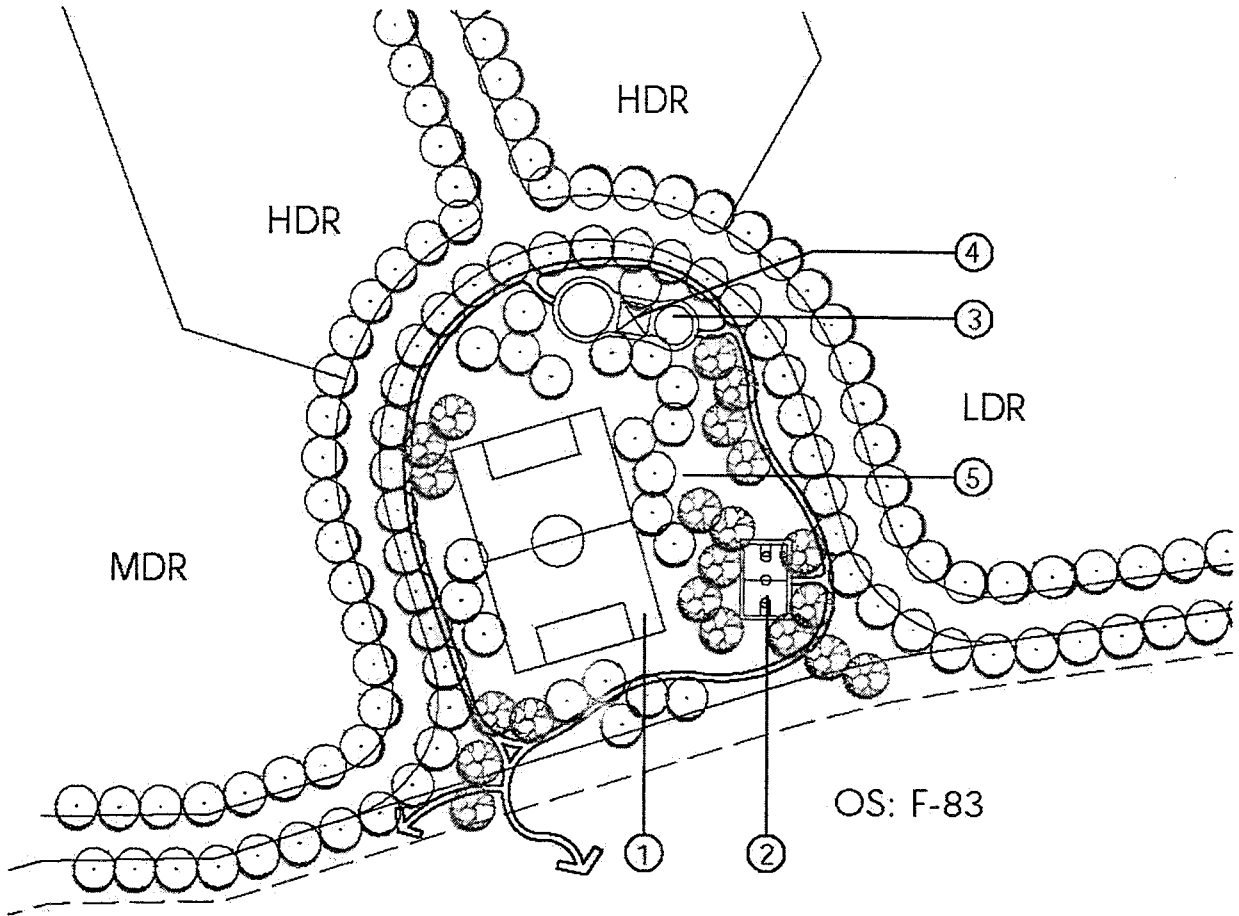


Figure 8-7: Park F-52 Conceptual Plan



PARK F-52 AMENITIES	
5.9 ACRE PARK	
1.	YOUTH SOCCER (180'x270')
2.	BASKETBALL COURT
3.	CHILDREN'S PLAY AREA
4.	GROUP PICNIC AREA
5.	INFORMAL PICNIC AREAS

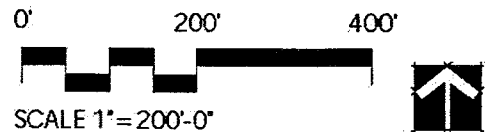
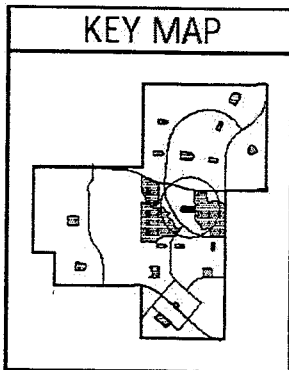
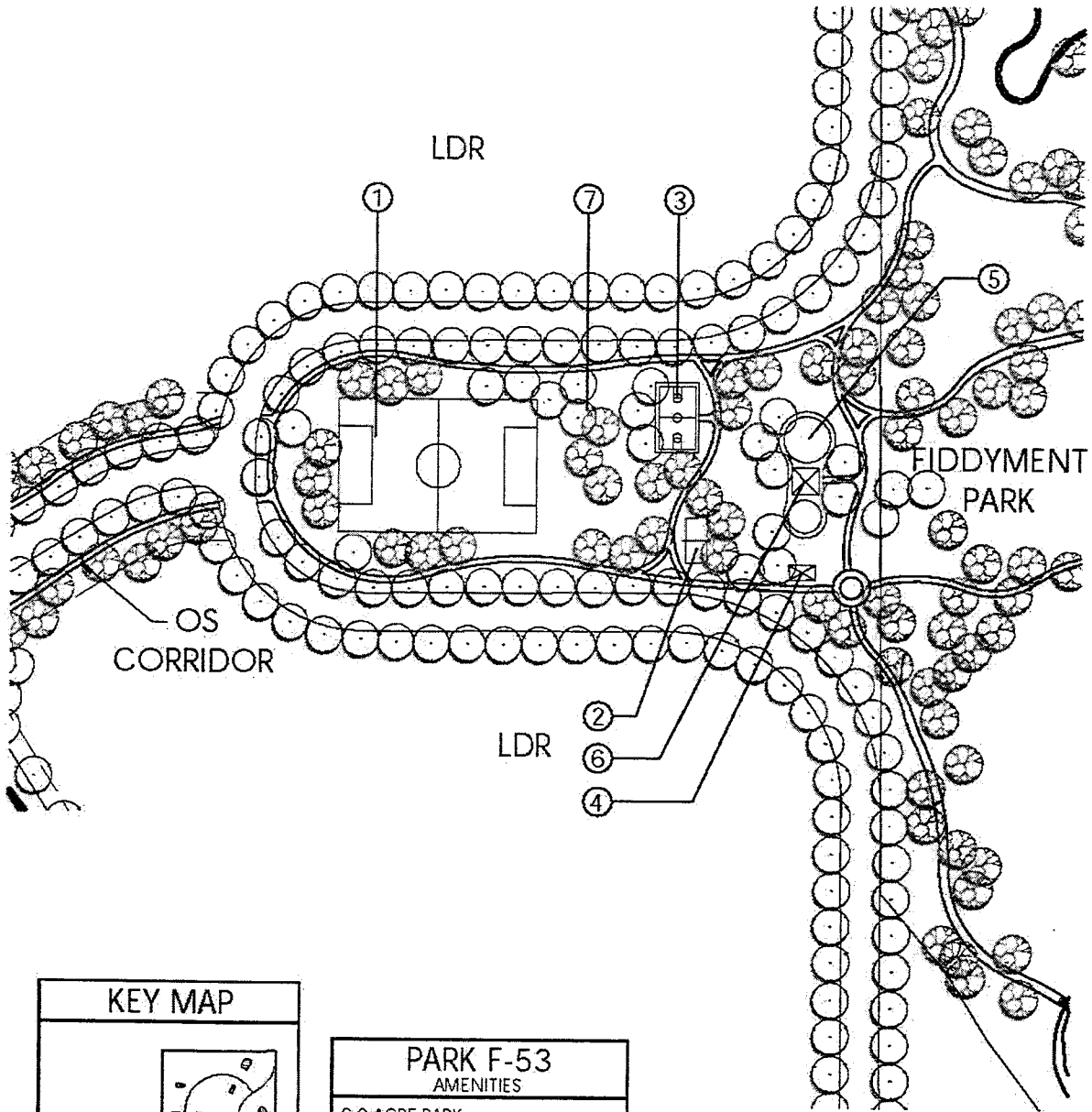


Figure 8-8: Park F-53 Conceptual Plan



PARK F-53 AMENITIES	
6.0 ACRE PARK	
1.	YOUTH SOCCER (180x270)
2.	VOLLEYBALL COURT
3.	BASKETBALL COURT
4.	RESTROOM
5.	CHILDREN'S PLAY AREA
6.	GROUP PICNIC AREA
7.	INFORMAL PICNIC AREA

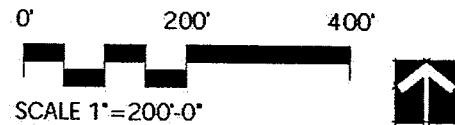
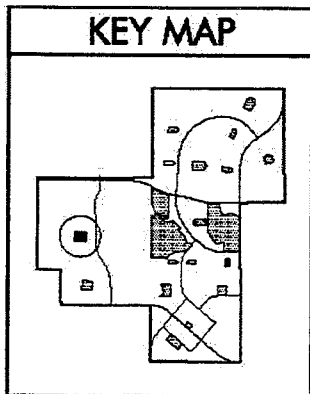
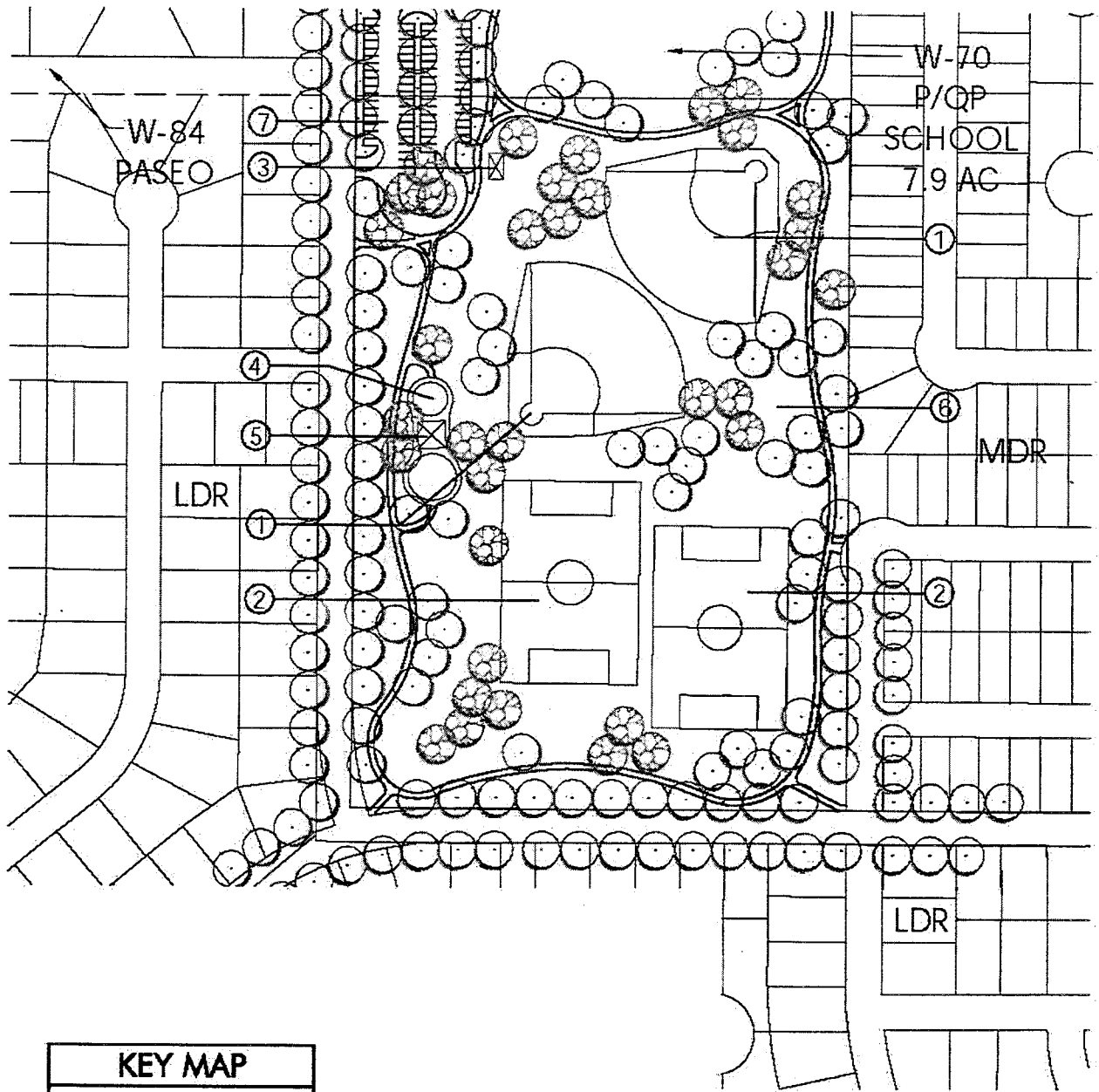


Figure 8-9: Park W-50 Conceptual Plan



PARK W-50 AMENITIES	
14.1 ACRE PARK/ JOINT USE SITE	
1.	YOUTH BASEBALL (200' RAD)
2.	YOUTH SOCCER (180'X270')
3.	RESTROOM
4.	CHILDREN'S PLAY AREA
5.	GROUP PICNIC AREA
6.	INFORMAL PICNIC AREAS
7.	JOINT USE PARKING (30)

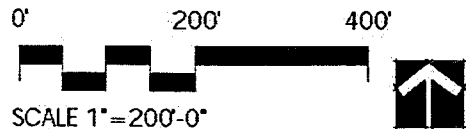


Figure 8-10: Park W-51 Conceptual Plan

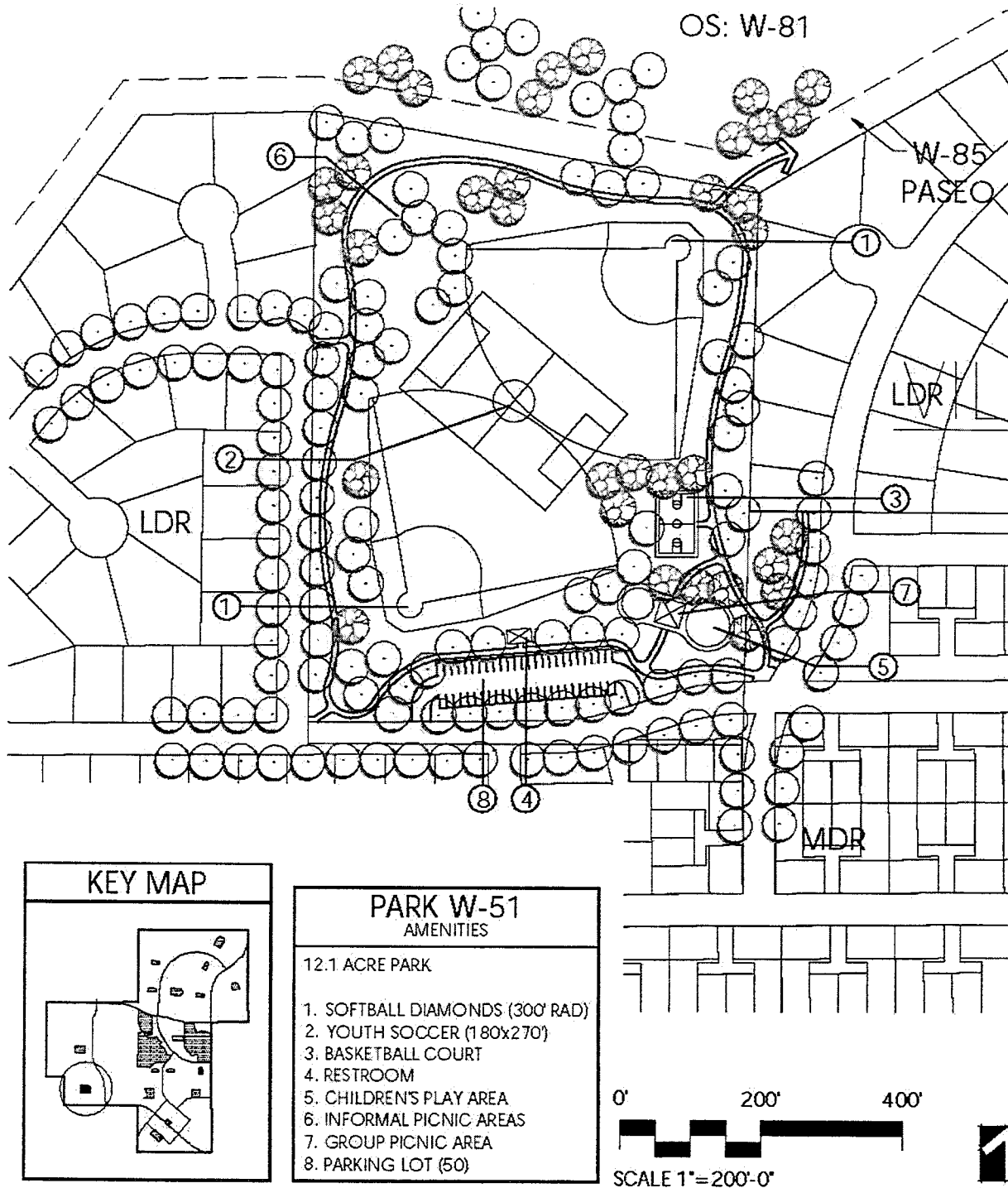


Figure 8-11: Park W-52 Conceptual Plan

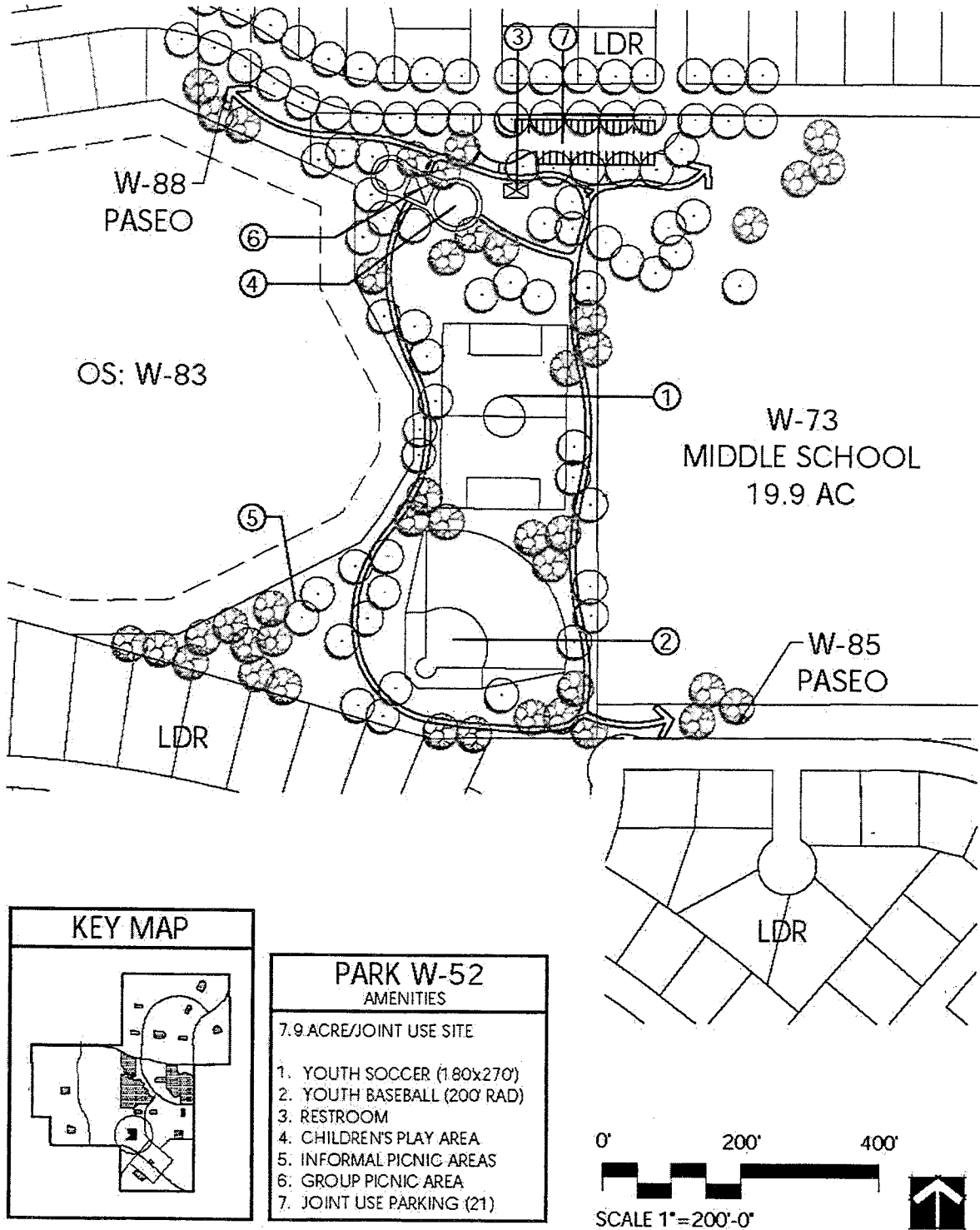
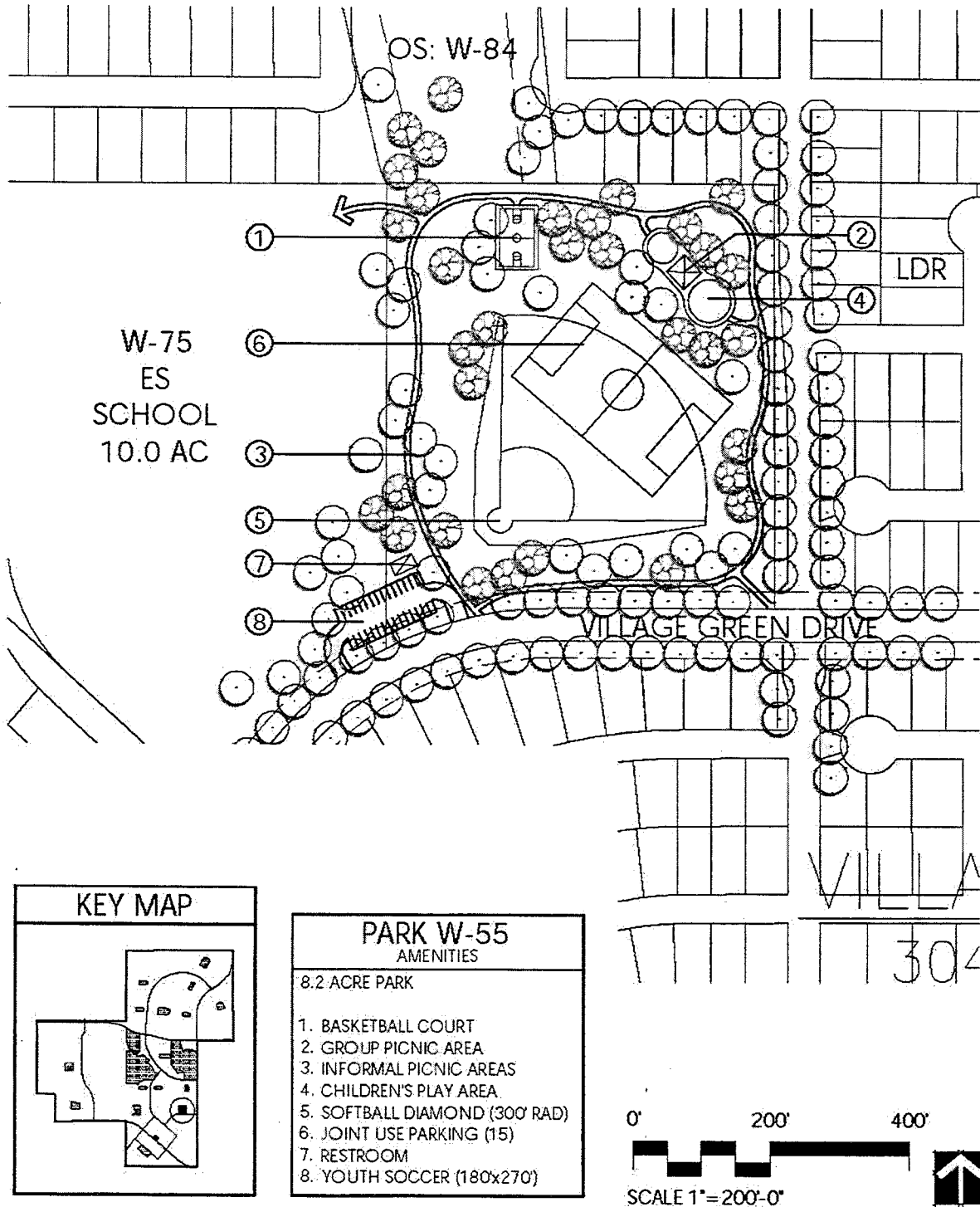


Figure 8-12: Park W-55 Conceptual Plan



Non-Credited Pocket Parks

In addition to credited acreage, the WRSP includes non-credited pocket parks which provide additional passive recreational opportunities for WRSP residents.

Pocket parks are small 2.4 to 3.2 acre facilities located centrally within specific subdivisions north and west of Phillip Road (Fiddymont portion of WRSP). Pocket parks are sized and positioned to provide for passive recreation and a social gathering spot for the immediate neighborhood. They differ from neighborhood parks in that they are less formal with a strong emphasis on passive recreation uses rather than active play areas. Typical facilities and uses may include tot lots, picnic tables, benches, informal turf areas, landscaping, shade structures, and under age 8 soccer fields. Pocket parks are to be situated along the primary entry axis of the proposed subdivision and will be ringed by local streets with residential units fronting the adjacent road and park to provide a strong entry statement and increase visibility.

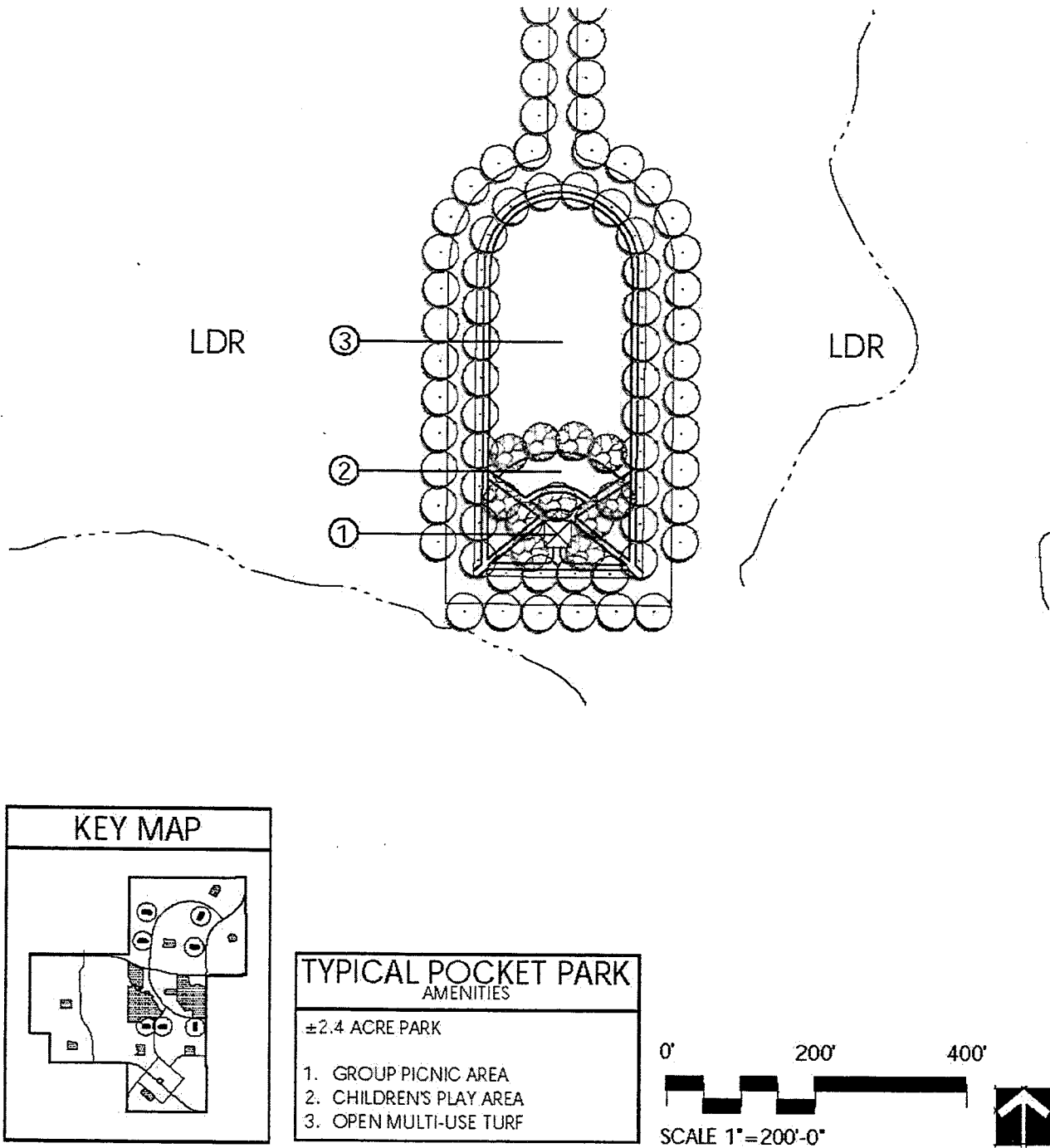
A total of seven (7) pocket parks are proposed in the WRSP (see Figure 8-2 for locations). Pocket parks do not receive credit towards meeting the General Plan park dedication requirement, but do provide an additional recreational amenity within the WRSP. Pocket parks will be designed and constructed as part of the adjacent subdivisions. Figure 8-13 provides a conceptual plan for a typical pocket park.

8.5 LIBRARY

The City of Roseville operates a public library system that includes the main library at 225 Taylor Street and existing and planned branch library locations. These include two 10,300-square foot branch libraries planned within Mahany Park and within the North Central Roseville Specific Plan Area. The Mahany Park branch library, planned approximately one mile east of the WRSP on Pleasant Grove Boulevard, will be the closest library facility and provide the most convenient service to the Plan Area.

At the time of Specific Plan approval there were no plans to include a new library facility within the WRSP. The proposed multi-purpose center at Fiddymont Park could be expanded to include a facility in the event the City identifies the need and resources for a new library within the WRSP in the future. Details relating to the WRSP's contribution toward library facilities are included in the Specific Plan development agreements.

Figure 8-13: Typical Pocket Park Conceptual Plan



9 UTILITIES PLAN

The West Roseville Specific Plan (WRSP) includes utility infrastructure required to serve the Plan Area. Each component of the utility infrastructure system is designed to accommodate build out of the WRSP area. Phasing of infrastructure improvements and funding obligations are detailed in the Specific Plan development agreements. Table 9-1 summarizes the utility providers to the WRSP.

Table 9-1: WRSP Utility Providers

Utility	Provider
Water	City of Roseville
Wastewater	City of Roseville
Recycled Water	City of Roseville
Drainage and Flood Control	City of Roseville
Electric Service	City of Roseville
Natural Gas	Pacific Gas and Electric
Communications	Various
Solid Waste Disposal	City of Roseville

Utility infrastructure will be constructed, dedicated and easements provided consistent with this Specific Plan, the project development agreements and all applicable standards and requirements.

9.1 WATER

The City of Roseville will provide water service (supply, treatment and conveyance) to the WRSP. Water will be delivered to the Plan Area via the City's backbone system to four connection points along Fiddyment Road at Blue Oaks Boulevard, Hayden Parkway, Veterans Boulevard and Pleasant Grove Boulevard. The City will serve the plan during normal operations demand. Peak demand volumes will be stored in two on-site storage tanks located south of the PGWWTP on Parcel W-74. Reservoirs will be used to augment the City supply during peak and emergency system demands. Groundwater wells will be developed as a backup water supply.

The WRSP water demand is approximately 7,042 acre feet per year. The water supply for the WRSP will consist of a combination of sources for wet and dry year scenarios. Non-potable (recycled) water will be utilized for irrigation purposes in both wet and dry years (see Section 9-3). Potable water for the WRSP will be obtained through the

City's existing contracts with Placer County Water Agency (PCWA) and the U.S. Bureau of Reclamation. In addition, a transfer of 3,200 acre feet per year from the San Juan Water District will supplement the City's water supply.

Water will be distributed within the WRSP via a looping system that parallels collector and arterial roadways on a transmission main grid. The transmission and distribution system consists of 12-inch to 24-inch diameter mains. Four water supply wells are planned to ensure service in emergency situations. Two potable water storage tanks will be constructed on Parcel W-74. The water distribution system, storage tanks and well locations are reflected on Figure 9-1.

9.2

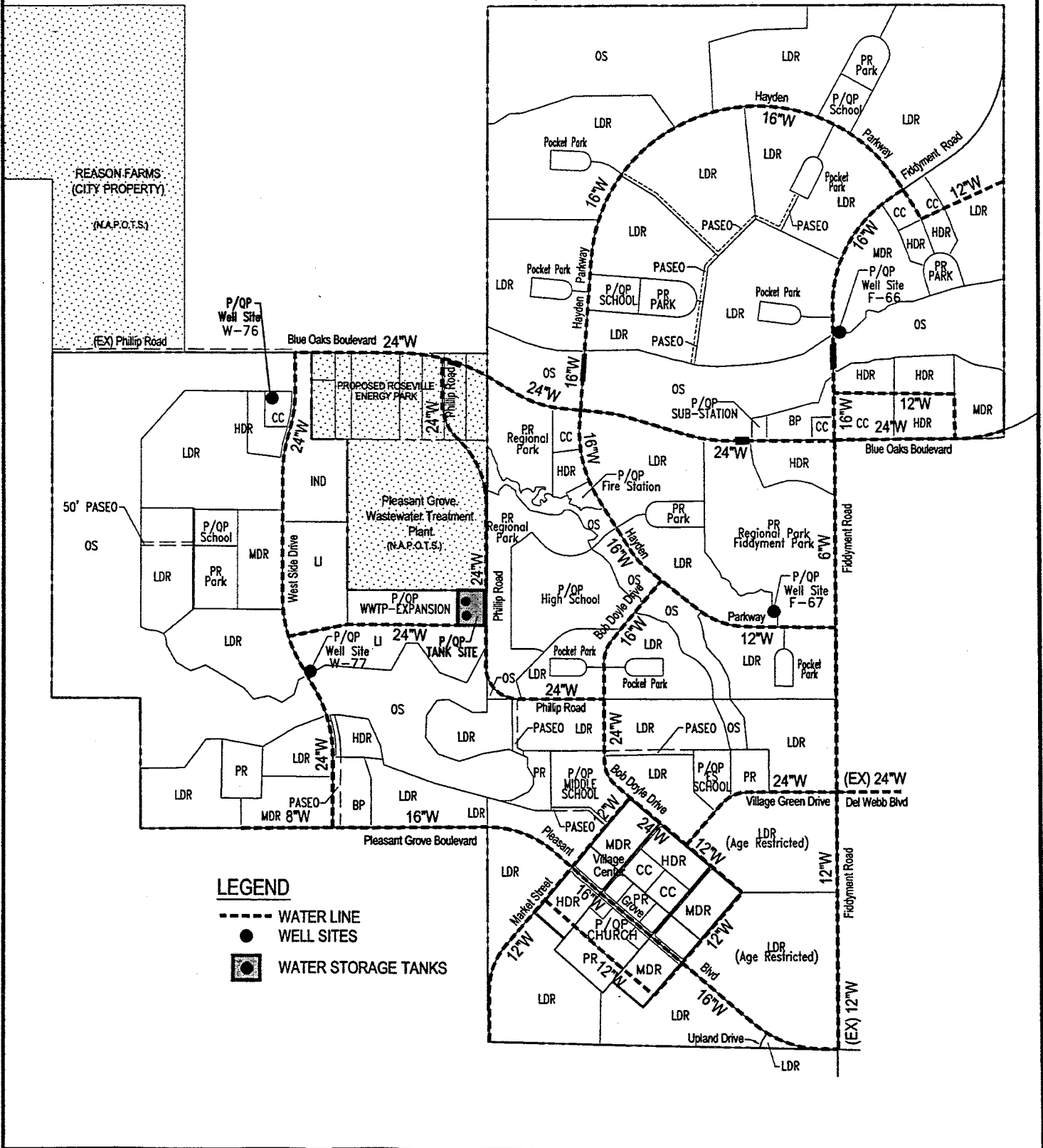
WASTEWATER

Sanitary sewer service is provided to the WRSP by the City of Roseville. The City's wastewater treatment system utilizes the Pleasant Grove Wastewater Treatment Plant (PGWWTP) and Dry Creek Wastewater Treatment Plant (DCWWTP). The WRSP will generate approximately 2.6 mgd average daily wastewater flow. The WRSP will be served by the PGWWTP. The PGWWTP is located adjacent to the central portion of the Plan Area, west of Phillip Road and south of Blue Oaks Boulevard. The WRSP includes a 20-acre area (Parcel W-71), which could be used for a future expansion of the PGWWTP, if and when an expansion becomes necessary. A 1000-foot non-residential buffer encumbers the WRSP to the west, south and east of the PGWWTP. The City owns the parcels within the 1,000-foot non-residential buffer north of the PGWWTP and is committed to maintaining non-residential land uses for these parcels. This buffer is intended to ensure compatibility between the PGWWTP and adjacent uses. The 1000-foot non-residential buffer will be extended past the 20-acre future expansion area to insure compatibility south of the expansion area. WRSP land uses have been planned accordingly.

The location of the PGWWTP and the backbone wastewater collection system are reflected on Figure 9-2. Wastewater flows from the WRSP will be directed to PGWWTP by a network of pipes installed within street rights of way or easements. Sewer collection pipes will range in size from 8-inches to 33-inches. A small portion of the WRSP will connect to an existing City-owned 78-inch diameter pipe just north of the intersection of Blue Oaks Boulevard at Fiddymont Road south of Pleasant Grove Creek. As a result of site topography, the sanitary sewer system will not require lift stations. A siphon will be required near the Pleasant Grove Creek crossing by Phillip Road in order to allow sewer lines to gain the necessary depth to cross the creek. The siphon will consist of a deeper pipe connected by underground structures with surface manholes. All sewer improvements will be consistent with the Regional Wastewater Master Plan.

Figure 9-1

Water Distribution System, Storage Tanks and Well Locations



9.3 RECYCLED WATER

The City of Roseville will provide the WRSP with recycled water from the Pleasant Grove Wastewater Treatment Plant (PGWWTP). The City currently provides recycled water to several locations on the west side of the City of Roseville. The WRSP will utilize a semi-aggressive strategy of recycled water use by irrigating landscaping at parks, schools, commercial, business professional, industrial and multi-family projects as well as publicly landscaped areas (including roadway landscape corridors and medians). The use of recycled water offsets potable water demand and is an important component of the overall WRSP water supply. The estimated annual recycled water demand in the WRSP is 1.56 mgd (1,750 AF/yr).

A recycled water storage facility is planned off-site, south of Blue Oaks Boulevard and west of Phillip Road. This facility will store recycled water for use in the WRSP and other areas of western Roseville. The storage facility consists of two storage tanks with a capacity of 850,000 gallons each for a total of 1.7 mgd, and will include a backup potable water supply. A backbone of dedicated non-potable water lines ranging from 6-inches to 24-inches in diameter will be constructed within street rights of way to serve the WRSP. Figure 9-3 identifies the location of the recycled water storage facility and recycled water distribution system.

9.4 DRAINAGE AND FLOOD CONTROL

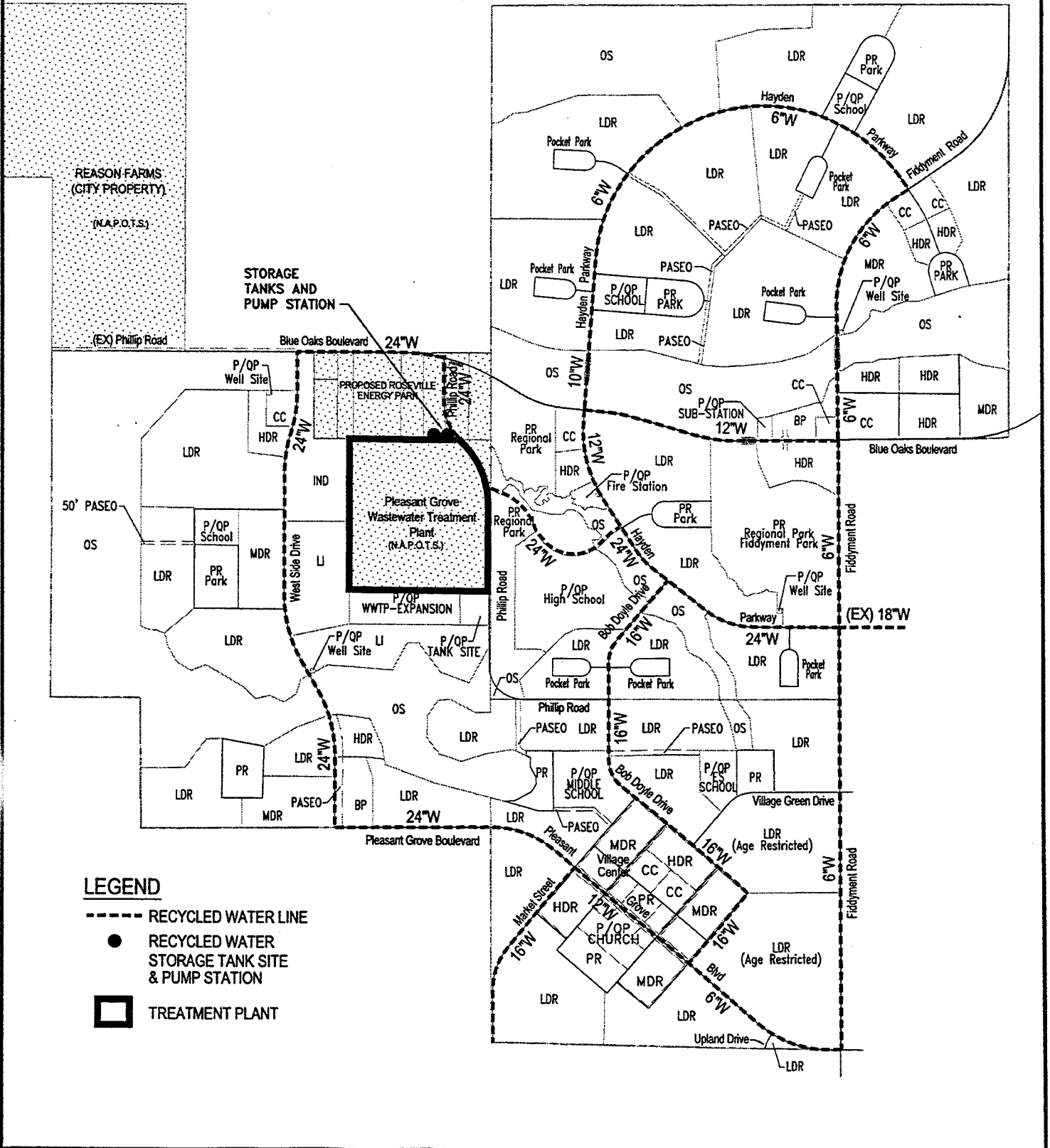
The WRSP is located within the Pleasant Grove Creek and Curry Creek watersheds, both of which are located within the larger Natomas Cross Canal watershed of northwestern Placer County and southeastern Sutter County. Approximately 64% of the WRSP, the northern and eastern portions, are within the Pleasant Grove Creek watershed. The remainder is within the Curry Creek watershed.

The main branch of Pleasant Grove Creek drains east to west for approximately 1.5 miles through the WRSP within an open space corridor (Parcels F-83, F-94, F-95) north of Blue Oaks Boulevard. The floodplain for Pleasant Grove Creek varies in width from 1,200 feet at the east Plan Area boundary to 300 feet at the west boundary. Other WRSP creeks that eventually drain into Pleasant Grove Creek include Kaseberg Creek, Coyote Creek and the South Branch of Pleasant Grove Creek.

A tributary of Curry Creek drains from east to west through the WRSP open space (Parcels W-81 and W-83) to the main branch of Curry Creek located a half-mile south of the Plan Area. Curry Creek has a floodplain varying in width from 150 to 700 feet.

Figure 9-3

Recycled Water Distribution System and Storage Facility



LEGEND

- RECYCLED WATER LINE
- RECYCLED WATER STORAGE TANK SITE & PUMP STATION
- ▣ TREATMENT PLANT

Due to the location of that portion of the WRSP within the Pleasant Grove Watershed, hydrologic modeling indicates that onsite detention of runoff would likely contribute to peak flows downstream. This is because the peak WRSP discharge would occur before the most significant watershed peak, reducing the potential for un-detained peak flows to coincide downstream. As a result, no detention is proposed for the portion of the project within the Pleasant Grove Watershed. Conversely, within the Curry Creek watershed, hydrologic modeling indicates that onsite detention of runoff would contribute to a reduction of peak flows downstream. As a result, five detention basins are proposed on the Westpark property. Two of the basins are planned within open space parcels W-81 and W-83 and three of the basins are planned within LDR parcels W-8, W-11 and W-18. The final location of these detention basins will be determined with small lot tentative subdivision maps. Detail relating to the sizing, timing, construction, funding and maintenance of the basins is included in the Specific Plan development agreements. The detention basins will be included in the WRSP 404 permit and Preserve O&M Plan.

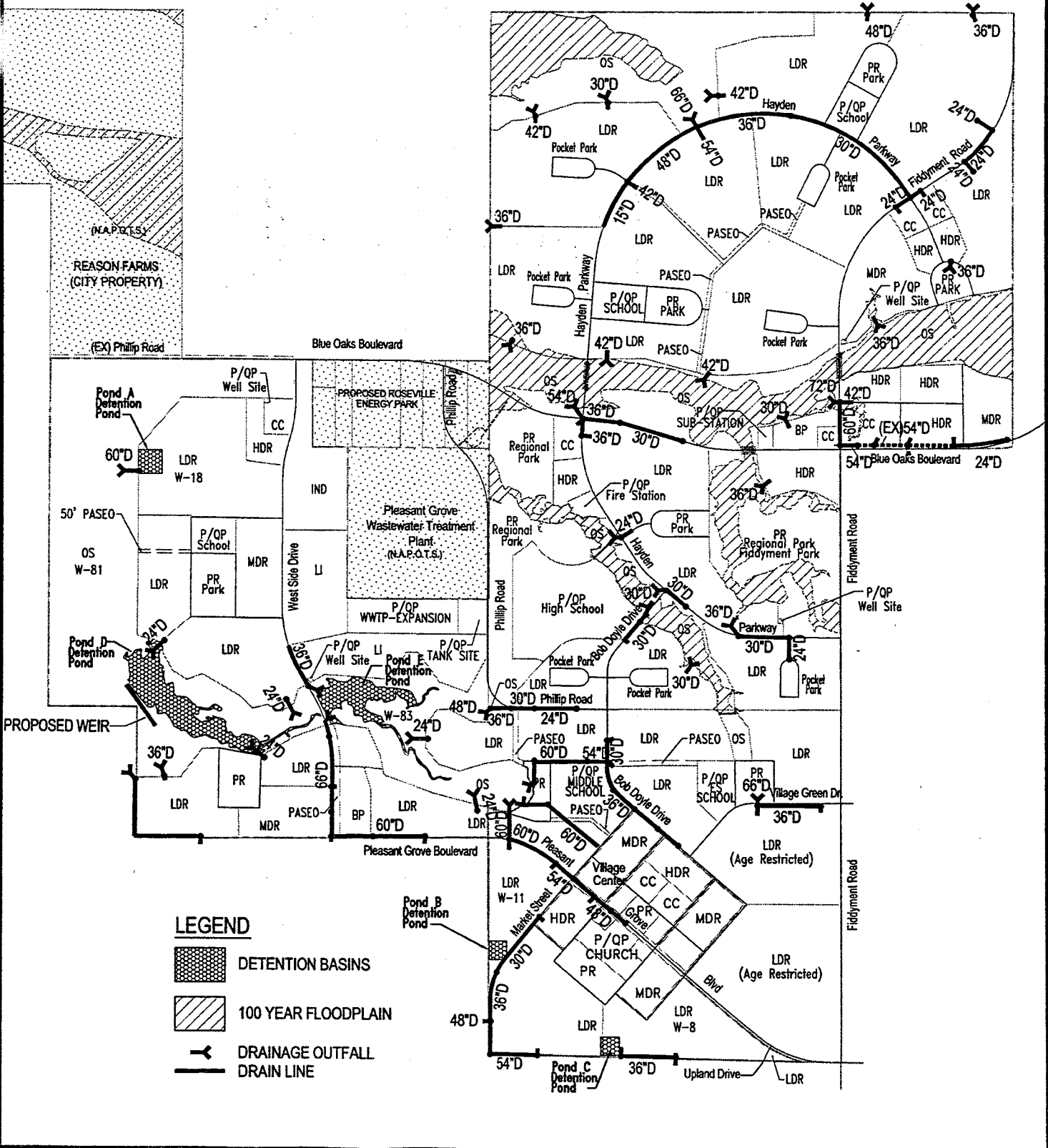
In addition to detention, the WRSP landowners will participate with the City of Roseville in constructing a regional retention basin to mitigate stormwater volume runoff. The WRSP's contribution to this basin is approximately 430 acre-feet. The regional retention basin will be located within the Pleasant Grove watershed west of the WRSP.

WRSP drainage improvements and floodplains are shown on Figure 9-4. On-site drainage improvements consist of a combination of conventional subsurface and surface drainage systems including rechannelization of minor tributaries, construction of pipe conveyance systems and construction of culverts and bridges over creek and tributary crossings. Stormwater will be discharged through outfalls into open space corridors. Settling basins, sand filters, grassy swales and other best management practices will be utilized at pipe outfalls or other appropriate locations for water quality management, and to convey stormwater runoff to receiving waters while minimizing impacts to open space resources.

Drainage facilities will be designed and constructed in conformance with City of Roseville Improvement Standards, the Placer County Flood Control Agency's Stormwater Management Manual, and the open space preserve Operations and Maintenance (O&M) Plan, the preparation of which is a requirement of the Clean Water Act 404 permit. The number and location of outfalls shown in Figure 9-4 is based on the best available information and is subject to refinement during the subdivision map approval process. The Preserve O&M Plan will include requirements to minimize erosion and direct drainage away from vernal pool habitat by employing conceptual drainage improvements to be outlined in the plan (swales, outfalls, energy dissipation and erosion control).

Figure 9-4

On-Site Drainage Improvements and 100 Year Floodplain



9.5 ELECTRIC SERVICE

The Roseville Electric Department will supply electricity to the WRSP. Demand for electrical service is estimated to average 40.79 MW per year within the WRSP. An electric substation is planned on a 1.6-acre site (Parcel F-65) near the northwest corner of Blue Oaks Boulevard and Fiddymment Road. 60kV overhead transmission lines will loop through the Plan Area extending westerly on the north side of Blue Oaks Boulevard and south through a 35-foot power line easement west of the PGWTP. The power line easements extend south and east outside of the WRSP to connect to the existing Fiddymment Substation near Pleasant Grove Boulevard and Fiddymment Road. Underground electrical distribution will be extended to individual projects in conjunction with roadway improvements. In addition, street lighting will be provided along all public streets as part of the roadway frontage improvements. All electric and street light facilities will be constructed to the City's then current standards.

The location of the proposed electric substation and the alignments of the 60kV power line easements are shown on Figure 9-5. The proposed alignment requires the purchase of off-site easements. Included on Figure 9-5 is an alternative 60kV power line alignment. If the off-site easements for the proposed 60kV alignment are not obtained at the beginning of Phase I, the alternate alignment will be constructed. The alternative alignment follows Pleasant Grove Boulevard east to the Fiddymment substation. If the alternate alignment is constructed, that portion of the 60kV line through the Village Center (from points 500 feet east and west along Pleasant Grove Boulevard), will be placed underground.

At the time of WRSP approval, the City had proposed the Roseville Energy Park. While not within the WRSP, the Roseville Energy Park will be located immediately adjacent to the Plan Area, north of the WWTP. Roseville Energy Park will be a City operated power generation facility planned to include a 150-megawatt natural gas generating facility, a one-megawatt photovoltaic solar energy site, and a Community Energy Center.

9.6 NATURAL GAS

Pacific Gas & Electric Company (PG&E) will provide natural gas upon request and in accordance with the rules and tariffs of the California Public Utilities Commission. PG&E's long-range plans provide for availability of gas service to accommodate increased demand. Service will be provided to the WRSP from existing infrastructure adjacent to the Plan Area. Delivery of gas service to individual projects in the WRSP will be reviewed by PG&E at the time of proposal.

9.7

COMMUNICATIONS

The WRSP is within the service areas of Roseville Telephone Company (SureWest Communications) and SBC/Pacific Bell. The exchange boundary between Roseville Telephone and SBC/Pacific Bell runs directly through the Plan Area in a north-south direction from below Pleasant Grove Boulevard west of the Village Center north to Pleasant Grove Creek and then east to the Plan Area boundary. One or more private cable television companies will provide service to the WRSP.

Distribution lines to individual parcels will be extended from existing infrastructure adjacent to the Plan Area and will occur as development takes place. The appropriate providers will review delivery of telephone and cable television services to individual projects in the WRSP at the time of proposal.

9.8

SOLID WASTE

The City of Roseville will provide solid waste services to the WRSP. Solid waste will be collected and delivered to the Western Placer Waste Management Authority facility located northwest of the City at Athens and Fiddymont Roads. The Authority owns a Material Recovery Facility (MRF) which receives, separates, processes and markets recyclable materials removed from the waste stream. Residual waste is transferred to the Authority's Western Regional Sanitary Landfill located on the same site.

A solid waste recycling area is planned within the WRSP on the west side of Phillip Road, south of the PGWWTP on a site designated for water storage tanks (Parcel W-74). Residents will be able to drive into the recycling area to off-load recyclable materials.

10 VILLAGE CENTER PLAN

10.1 INTRODUCTION

The Village Center is envisioned as the heart and primary focus of the West Roseville Specific Plan. Encompassing an area of approximately 121 acres, the Village Center provides for a true interactive mixed-use neighborhood. Retail, office, community services, public spaces and parks are integrated with a variety of housing types. The mix, density and interface between these uses create an environment that is pedestrian friendly, and where formal and informal activities are key goals.

Successful execution of the Village Center is dependent upon a clear articulation of, and a long-term commitment to, the base concepts and principles that define its distinctive form.

The Village Center presents a unique development form in Roseville, one emblematic of an urban mixed-use environment. It introduces concepts, product types, use patterns, interfaces and development standards that require adjustment to the general philosophies and regulations that typically guide suburban development. Successful execution of the Village Center is dependent upon a clear articulation of, and a long-term commitment to, the base concepts and principles that define its distinctive form. While it is critical that policies and regulations be developed that are firm in ensuring implementation of the Village Center's distinguishing components, it is similarly important to provide sufficient flexibility to accommodate the variety of specific product types and design solutions that could contribute towards its realization.

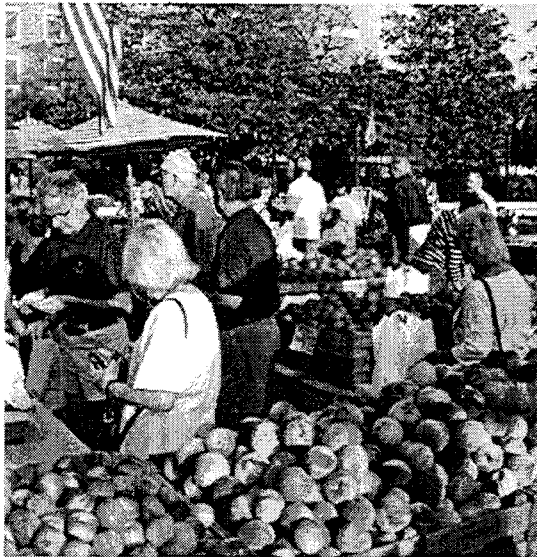


Given the unique nature of the Village Center, and the important role it plays in the WRSP, detailed requirements have been established to guide its development. This section of the Specific Plan provides specific form, planning principles, land use, zoning, development and design standards and processing direction. It supplements other sections of the Specific Plan and includes a more specific level of detail.

10.2 VILLAGE CENTER FORM & PLANNING PRINCIPLES

□ Form

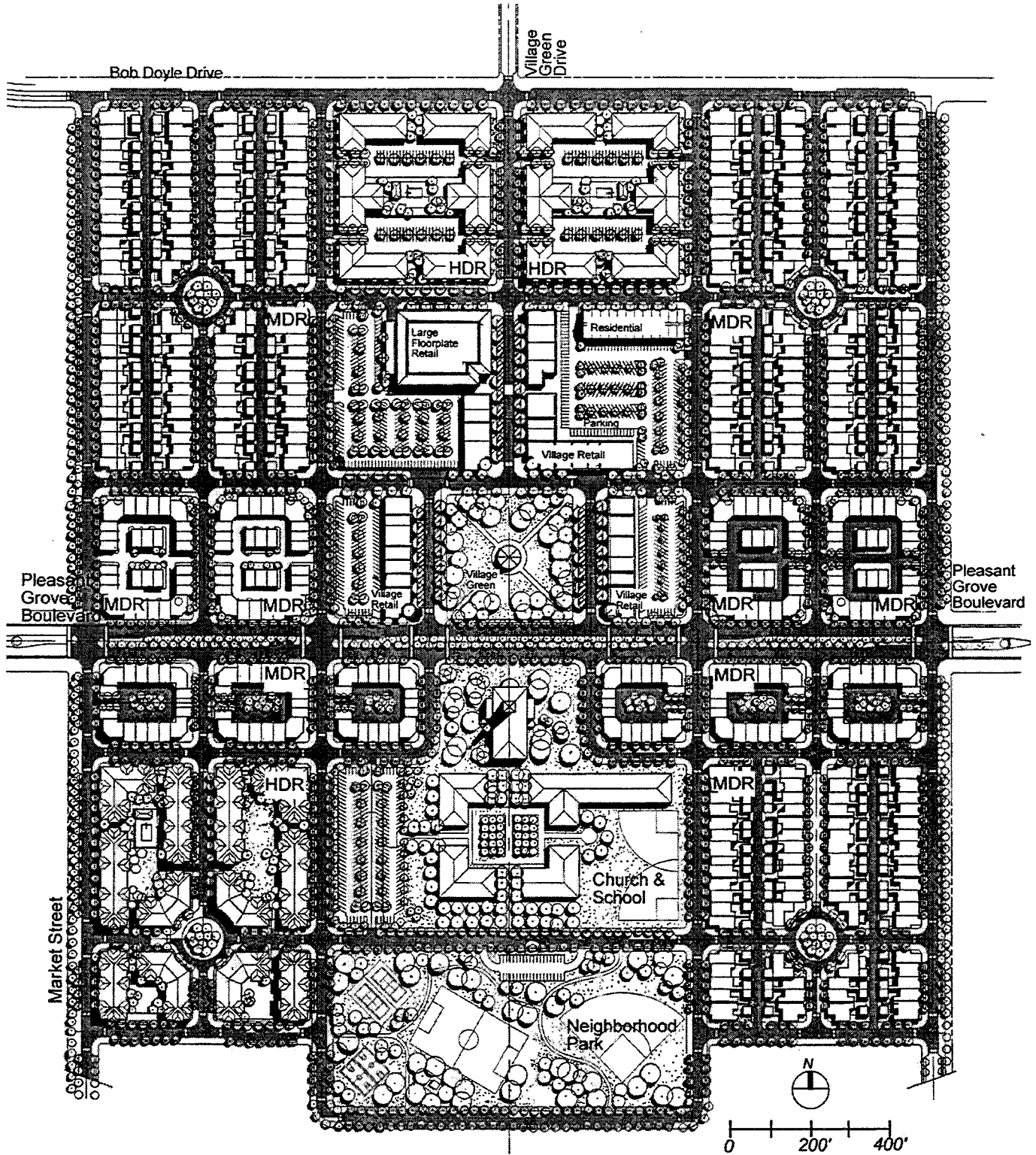
The Village Center is a core feature and a unique place within the WRSP. It is influenced by the concepts embedded in traditional small town urban villages and smart growth principles such as densities that support transit, a mix of uses, compact development form, a variety of housing opportunities and a pedestrian-friendly environment. The Village Center incorporates a broad mix of uses applied over an urban grid street pattern with a central village green. Included are a blend of higher density residential types, commercial and service uses and public/quasi-public spaces. The increased density/intensity of development and spatial relationship between uses promote diversity, activity and pedestrian orientation. Numerous design elements, including buildings oriented toward the street and dense tree canopies, are incorporated to further this intent. The Village Center form and density facilitates a variety of transportation options, and increases the living choices available in Roseville.



The Village Center is planned to function as the commercial, service, social and activity focus of the WRSP. The Village Center will have a visible presence in the community and create a destination where residents may live, shop, eat, recreate and obtain services. It is the gathering spot and a district where preference is given to the pedestrian. Interaction and socialization are primary goals and a catalyst for formal and informal activities. Uses, activities and amenities are offered that encourage people from throughout the community to come often and stay for extended periods.

The key elements of the Village Center are generally reflected on Figure 10-1, Village Center Illustrative Plan.

Figure 10-1: Village Center Illustrative Plan



□ **Planning Principles**

The Village Center is defined by a combination of its form, uses and design. The following principles describe the major attributes of the use and guide its implementation. The Village Center Planning Principles are highly influenced by and reflective of “Smart Growth” values that promote the creation of walkable mixed-use communities:

Form:

- Model the Village Center in the form of a traditional small town village or center.
- Create a clearly identifiable and distinct district that is easy to see, find, get to and comprehend.
- Integrate a mix of uses with multiple destinations within close proximity.
- Increase densities and intensities of uses to support activity and a variety of transportation choices.
- Create a compact development form.
- Provide overlapping interfaces between uses and allow uses to mix.
- Create a pedestrian friendly-walkable community that minimizes barriers, emphasizes the pedestrian over the automobile, and implements the City’s General Plan Pedestrian District policy.
- Incorporate a traditional grid circulation pattern with short blocks, straight streets and intersections at regular intervals.
- Provide for easy and desirable access between uses, and interconnection with adjacent neighborhoods.
- Ensure that streets and sidewalks balance all forms of transportation.
- Orient all development toward the street (rather than inward or to parking lots) to place activity on the street.

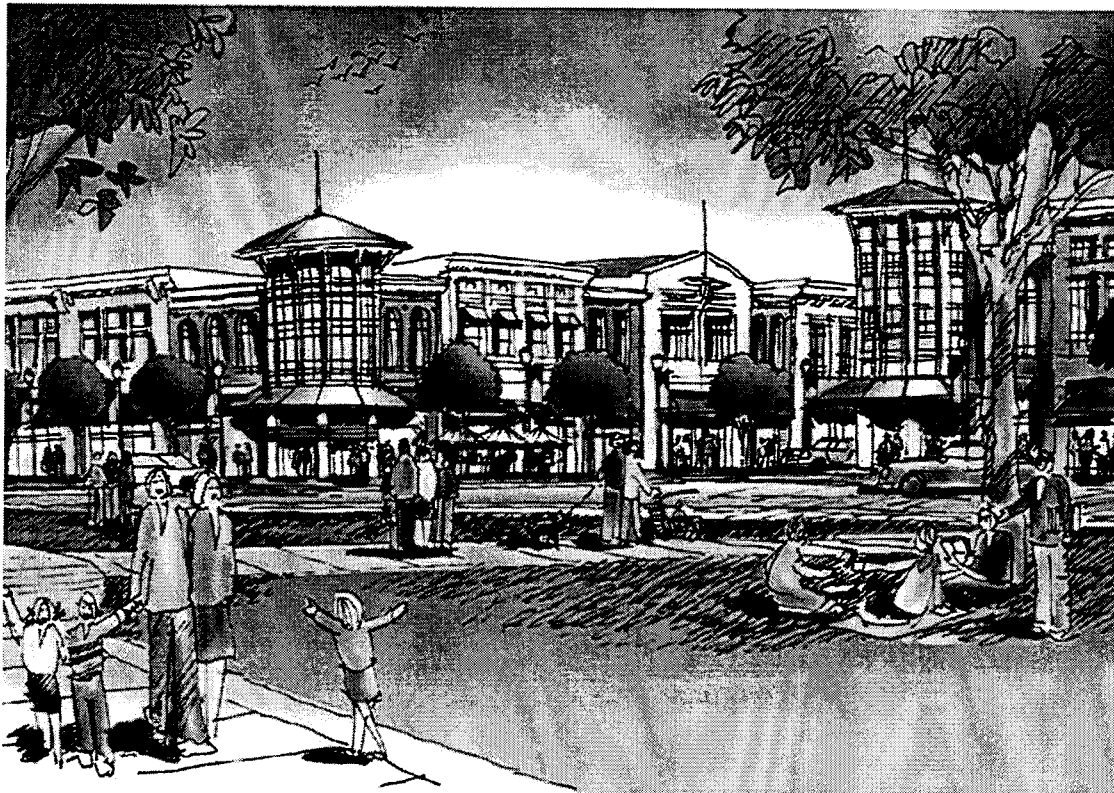
Uses:

- Create a wide range of higher density housing choices such as brownstones, townhouses, stacked flats, and apartments.
- Include retail and services uses in a “Main Street” setting to meet resident’s needs and to attract outside users and activities.
- Promote a “live-work” environment including residential above commercial spaces.
- Incorporate distinctive civic and quasi-public spaces for people to gather and to reinforce community identity.
- Provide a village green as a visual centerpiece and a venue for a wide variety of activities such as farmer’s markets, arts and craft shows, various events and celebrations, and performance arts.

Design:

- Emphasize urban streetscapes as a predominant design element bringing buildings and activities to the sidewalk to create a vertical enclosure that defines comfortable public spaces.
- Incorporate deciduous tree canopies, coordinated street furnishings, plazas and multi-use spaces, pedestrian scale lighting, directional signage and other elements to enhance the pedestrian environment.
- Provide gateways to create a sense of identity and arrival to the Village Center.
- Require street forward building orientations, entries, porches, windows and other elements to create visual interest and activity along the pedestrian ways.
- De-emphasize the visual presence of garages and off-street parking through rear oriented parking areas, alley access and by optimizing on-street parking to meet a portion of the parking demand.
- Permit adequate flexibility to accommodate the variety of specific product types and design solutions that contribute to the realization of the Village Center.

The Village Center presents a distinctive development form that may challenge certain established attitudes and conventions. As a result, it is critical to maintain a determined commitment to the components and principles that define the Village Center, with recognition of long-term goals over short-term market pressures.



10.3 LAND USE PLAN & REGULATIONS

Overview

The Village Center includes a mix of residential, commercial, park and public/quasi-public uses. All uses include the Village Center combining land use designation to recognize their unique nature and requirements. Village Center land uses are reflected on Figure 10-2, and are summarized on Table 10-1. A parcel-by-parcel land use, zoning, unit and square footage summary is included on Table 10-2.

Table 10-1: Village Center Land Use Summary

General Plan Designation	Land Use	Acres	% of Total Acres	Units
VC - MDR	Medium Density Residential	46.1	38%	405
VC - HDR	High Density Residential	22.4	19%	415
VC - CC	Community Commercial	14.4	12%	40
VC - P/R	Park & Recreation	12.6	10%	
VC - P/QP	Public/Quasi-Public	10.9	9%	
	Road Right of Way	14.3	12%	
TOTAL		120.7	100%	860

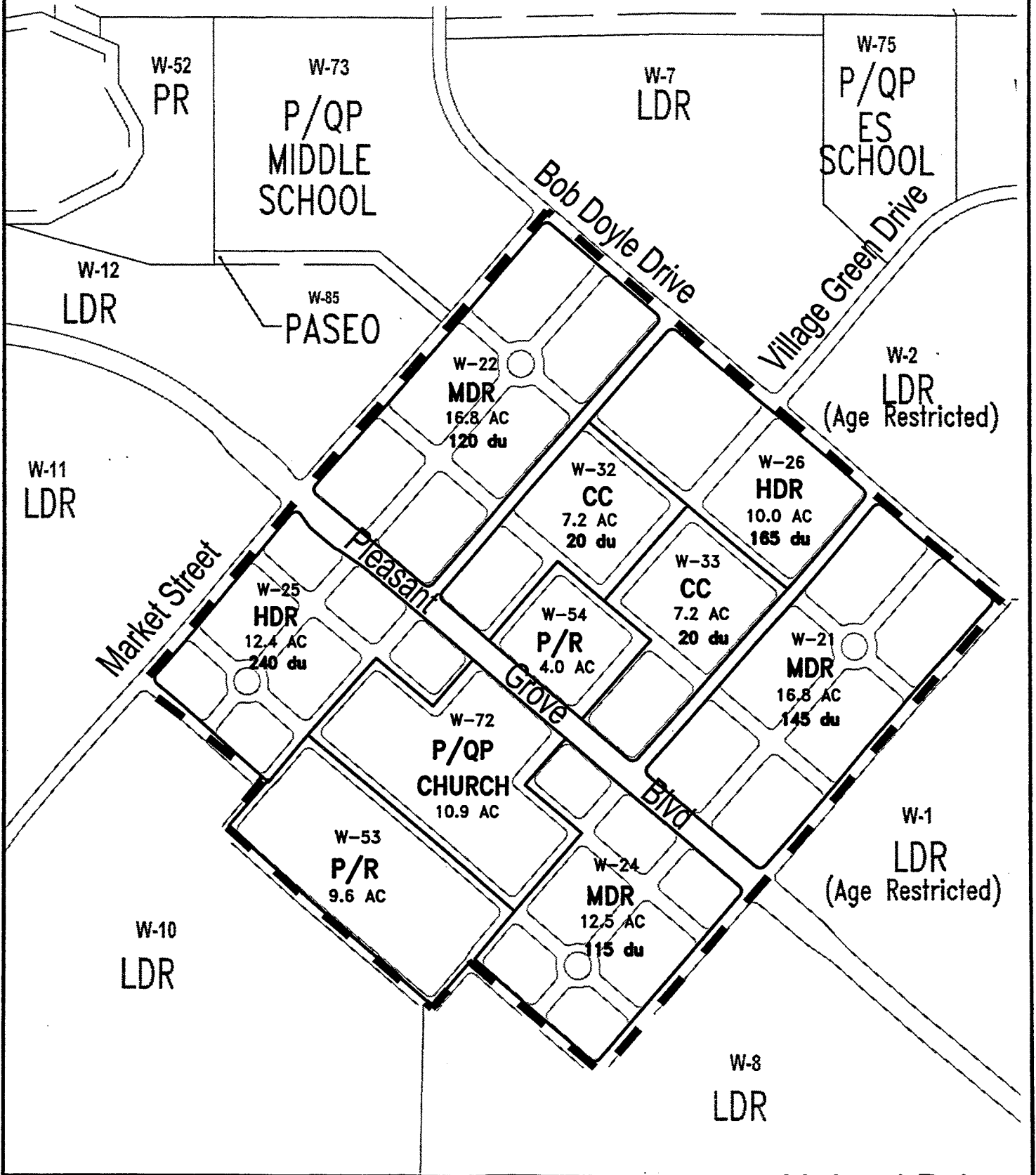
Table 10-2: Village Center Land Use, Zoning, Units & Square Footage by Parcel

Parcel #	Land Use	Zoning	Acres	Density ¹	Units ²
W-21	VC-MDR	R3/DS	16.8	8.9 du/ac	150
W-22	VC-MDR	R3/DS	16.8	8.9 du/ac	150
W-24	VC-MDR	R3/DS	12.5	8.4 du/ac	105
W-25	VC-HDR	R3/DS	12.4	18.9 du/ac	235
W-26	VC-HDR	R3/DS	10.0	18.0 du/ac	180
W-32	VC-CC	CC/SA-WR	7.2		20 ³
W-33	VC-CC	CC/SA-WR	7.2		20 ³
W-53	VC-PR	P/R	9.1		
W-54	VC-PR	P/R	3.5		
W-72	VC-P/QP	P/QP/SA-WR	10.9		
	Road Right-of-Way		14.3		
TOTAL			120.7		860

1. A mix of unit types and resulting densities are anticipated. Identified densities reflect an average as applied to the entirety of each parcel.
2. The WRSP allows for Minor Density Adjustments for LDR and MDR Units. Confirm current unit counts with the City of Roseville Planning Department.
3. A total of 40 units are allocated to be spread between Parcels W-32 and/or W-33. The exact allocation of units between the two parcels shall be determined through the Design Review process.

Figure 10-2

Village Center Land Use Plan



Permitted Uses and Development Standards

Land uses within the Village Center are implemented through application of zone districts as specified by the City of Roseville Zoning Ordinance. In recognition of its distinctive form, the Development Standards (DS) or Special Area (SA) overlay zones have been applied to most Village Center Uses. The overlay zones customize development standards and/or permitted uses of general zone districts to reflect the unique nature of the Village Center. A summary of zoning districts and overlay zones applied to the Village Center is included in Table 10-3. Guidance relating to the application of these overlay zones is provided in this section.

The WRSP Design Guidelines (Section 12) include additional detail to be considered in the design, review and approval of individual projects within the Village Center

Table 10-3: Summary of Village Center Applied Zoning Districts

Land Use	Applied Zoning Districts	Permitted Uses	Development Standards
VC – MDR: <i>Medium Density Residential</i>	R3/DS	Per Zoning Ordinance	Modified by WRSP ¹
VC – HDR: <i>High Density Residential</i>	R3/DS	Per Zoning Ordinance	Modified by WRSP ¹
VC – CC: <i>Community Commercial</i>	CC/SA-WR	Modified by WRSP ¹	Modified by WRSP ¹
VC - P/R: <i>Parks & Recreation</i>	P/R	Per Zoning Ordinance	Per Zoning Ordinance
VC - P/QP: <i>Public/Quasi-Public</i>	P/QP/SA-WR	Modified by WRSP ¹	Per Zoning Ordinance

1. Section 10.3

RESIDENTIAL USES

A wide range of higher density single-family attached, detached and multi-family housing types are included and encouraged within the Village Center. Housing is to be of an urban nature with higher densities than found in a typical suburban setting. Densities within the Village Center will range from 7.0 to over 20 units per acre, with an overall average density of approximately 12 units per acre. Emphasis is placed on defining the relationship between residential units and the street. Dwelling units are to be brought forward with entry doors and active living areas facing the street. Off-street parking is to be de-emphasized through alley access and rear yard garages. The intent is to create an active, pedestrian friendly community environment reminiscent of a traditional neighborhood.

Medium Density Residential (VC-MDR)	
<i>Density Range:</i>	7.0 to 13.0 dwelling units per acre
<i>Applied Zoning District:</i>	R3/DS: Attached or Detached Housing/Design Standard Overlay
<i>Description:</i>	<p>Medium Density Residential (MDR) land use is anticipated to accommodate urban density housing with a strong orientation toward the street. Key components of any MDR unit type shall include:</p> <ul style="list-style-type: none"> ▪ Single family attached or detached units facing the public street ▪ Entries/porches facing and moved forward towards the street ▪ Alley loaded or recessed rear yard garages <p>The preferred model is the “brownstone” concept reflective of the above elements. Other anticipated unit types include townhomes, and single-family detached. Figures 10-3 through 10-5 include illustrative layouts for potential unit types. There are other unit types that may be accommodated within the MDR land use and achieve the Planning Principles for the Village Center. All unit types shall be consistent with the R3/DS development standards and the WRSP Design Guidelines.</p> <p>Table 10-2 includes specific unit allocations and resulting densities for each MDR parcel. While it is anticipated and encouraged that unit types be mixed on a large lot parcel, the overall density in the Village Center shall average that prescribed on Table 10-2. In no case may a specific unit type on any MDR large lot parcel be below or above the density range specified by the MDR land use (7.0 to 13.0 du/ac).</p>
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	<p>The Design Standard (DS) Overlay provides for the unique nature and variety of housing types anticipated in the Village Center and the MDR density range. Development standards are included on Table 10-4. Standards are specified for both detached and attached units. Product types, building footprints (with associated fit lists) and architectural design of units shall be approved with the tentative small lot subdivision map for each Village Center large lot parcel. Modifications to the approved architectural design of units may subsequently be approved administratively if in compliance with Table 10-4. Other product types consistent with the intent of the Village Center development standards and Design Guidelines may be considered subject to approval of a Design Review Permit for Residential Subdivisions (DRRS) concurrent with approval of a tentative subdivision map.</p>

Table 10-4: Village Center MDR Development Standards

R3/DS Development Standards (MDR)			
General Characteristics			
Product Type	Detached units (Single-Family)	Attached units (Brownstone/Townhome/Condominium)	Other Products (Courtyard/"6-pak"/ other deviations to standards)
Garage Access	Via rear alley or through front via 10 ft.-wide "Hollywood Drive" with a minimum 3 ft.-wide central landscape strip. See Figure 10-5 for example of "Hollywood Drive".	Via rear alley or auto court	As approved, see below.
Driveway Access	Direct access from street to garage shall be prohibited along key Village Center roadways to minimize disruption to on-street parking opportunities as specified on Figure 10-6.		
Lot Size (minimum)			
Area Interior Lot	4,000 sq. ft.	Varies	City will consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards, and provided the products demonstrate superior siting characteristics and architectural design merits.
Area Corner Lot	5,000 sq. ft.	Varies	
Width Interior Lot	40 ft.	Varies	
Width Corner Lot	50 ft.	Varies	
Permitted Density (maximum per lot)			
Residential Density	1 dwelling 1 second unit	1 dwelling	1 dwelling
Setbacks and Treatments (front setbacks measured from back of sidewalk, others from property line)			
Front Setback (Figure 10-7)	10 ft. minimum 15 ft. maximum	10 ft. minimum 15 ft. maximum	City will consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards, and provided the products demonstrate superior siting characteristics and architectural design merits.
Side	5 ft. interior; 3 ft. interior for detached garage; 12.5 ft. street side on corner	12.5 ft. street side on corner	
Rear	20 ft. (1st floor of dwelling area) 10 ft. (2nd floor) 5 ft. (garage and rear fencing from alley property line when alley access) 3 ft. (detached garage with front access)	5 ft. (minimum garage and rear fencing from alley property line.)	
Front Off-Street Parking Setback (See Figure 10-6 for limitations on front access off-street parking)	30' minimum for garages	Garage access from alley only.	

R3/DS Development Standards (MDR)			
Front Porches/ Stoops/ Bay Windows (Figure 10-7)	Open porches may project up to 6 ft. into front setback; Bay windows and other projections may project up to 3 ft. into setback. A projecting porch/stoop may be elevated, but shall not extend more than 4 feet 6 inches above the grade of the adjacent sidewalk.	Open porches may project up to 6 ft. into front setback; Bay windows and other projections may project up to 3 ft. into setback; Steps/stoop may project to back of sidewalk. A projecting porch/stoop may be elevated, but shall not extend more than 4 feet 6 inches above the grade of the adjacent sidewalk.	
Front Articulation	<p>The residential design intent is to achieve a design that is varied, provides visual interest, and maintains a pedestrian scale for attached and detached housing. The front street façade shall be well articulated and shall reflect the following:</p> <ul style="list-style-type: none"> ▪ <i>Wall Planes</i> – incorporating building projections and recesses, bay windows, front porches, entries, balconies, chimneys, and other elements. Two-story high walls in the same plane should be minimized and in no case shall any two-story wall comprise more than 50% of the front façade. ▪ <i>Rooflines</i> – utilizing a variety of roof forms, heights and styles. ▪ <i>Architectural Enhancements</i> – including decorative trim, shutters, columns and other articulation. ▪ <i>Finish</i> – combining multiple materials and textures to enhance architectural detail ▪ <i>Unit Variation</i> – using the above and other elements to create distinction between individual units. 		
Coverage			
Site Coverage	n/a	n/a	As approved, see above.
Height			
Height (maximum)	35 ft.	45 ft.	As approved, see above.
Parking (minimum)			
Spaces	Two (2) off-street spaces per unit. Second units do not require additional off-street parking spaces. Guest parking may be provided on-street where adequate curb length is provided through use of rear accessed garages (no additional off-street parking required).		
Development Approval			
Processing	As approved by Tentative Subdivision Map & Design Review ¹ of housing product types, footprints and architectural design. Each large-lot parcel shall be planned, processed, and approved comprehensively. Large lot parcels may not be split for purposes of development entitlement processing.	As approved with concurrent processing of Tentative Subdivision Map, Design Review for Residential Subdivision, & Design Review ¹ of housing product types and footprints.	

1. Design Review approval effective until unit is occupied.

Figure 10-3: Village Center MDR Brownstone-Picture and Plan View

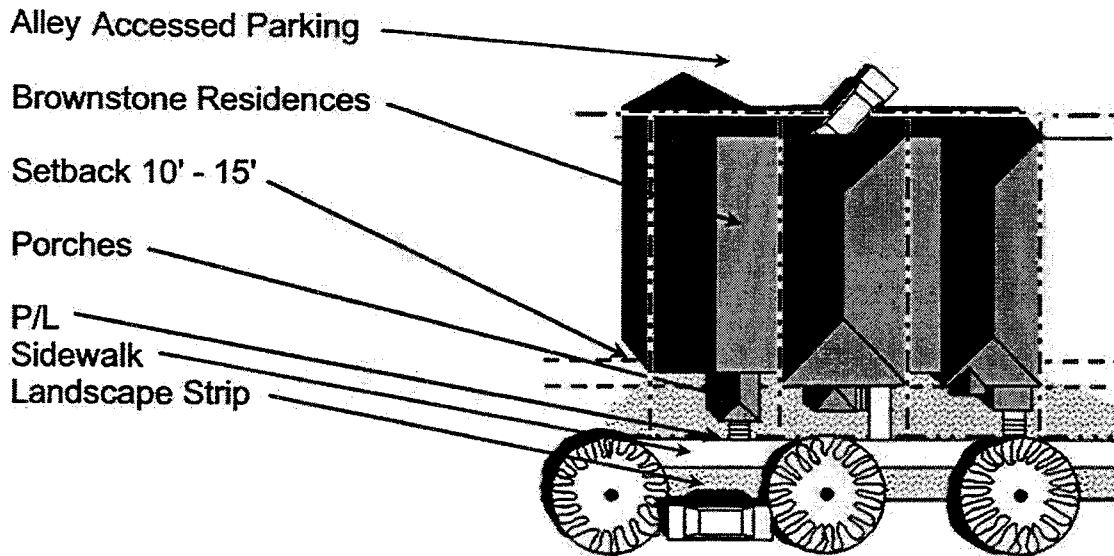
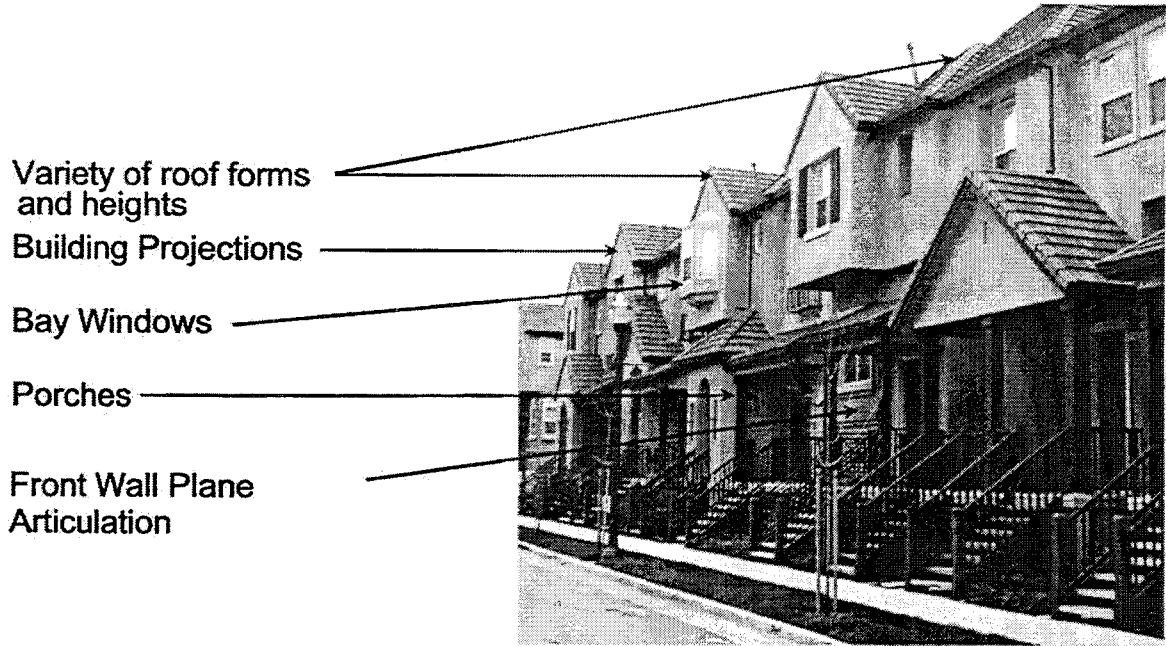


Figure 10-4: Village Center MDR Townhouse-Picture and Plan View

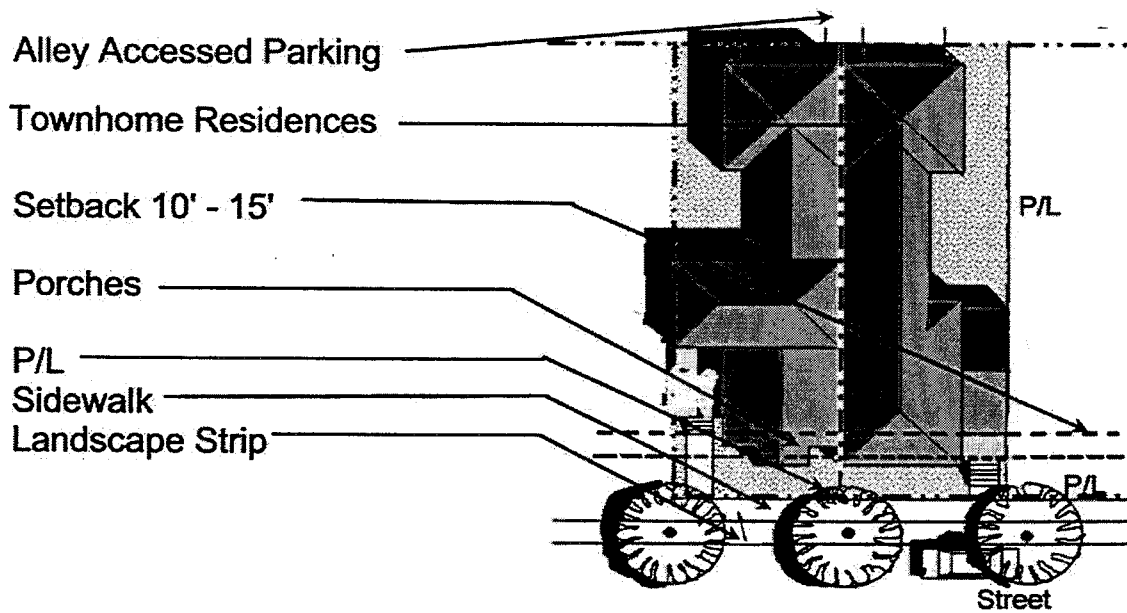
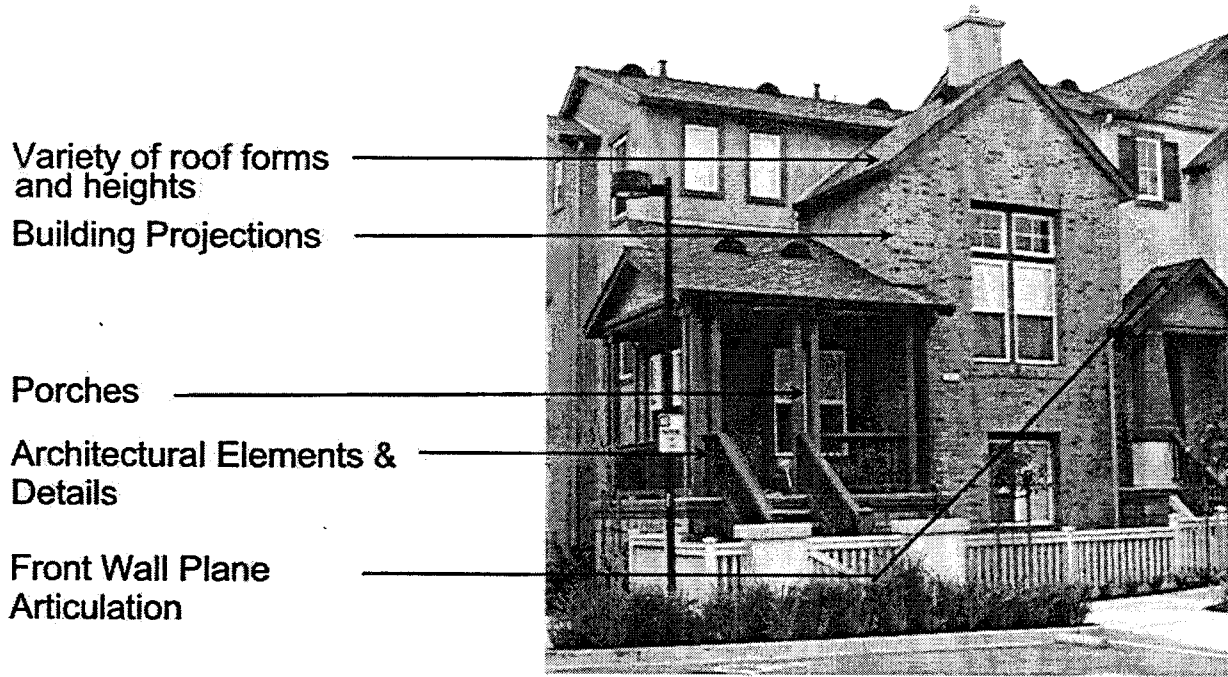


Figure 10-5: Village Center MDR Single Family Detached-Picture and Plan View

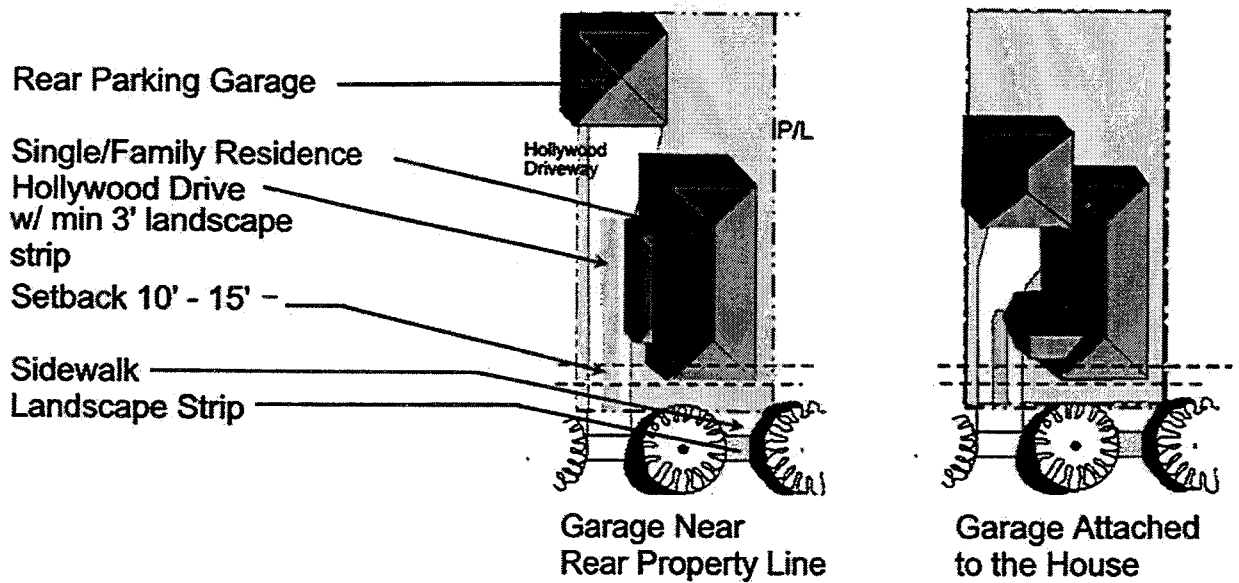
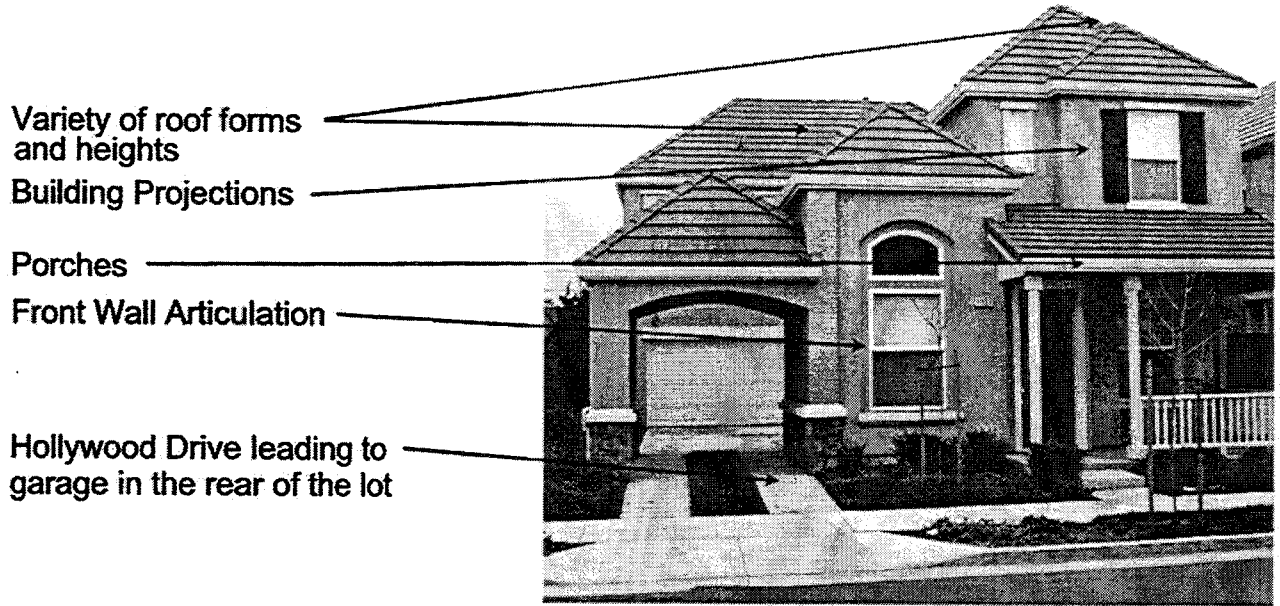


Figure 10-6: Front Driveway Prohibited Access Locations

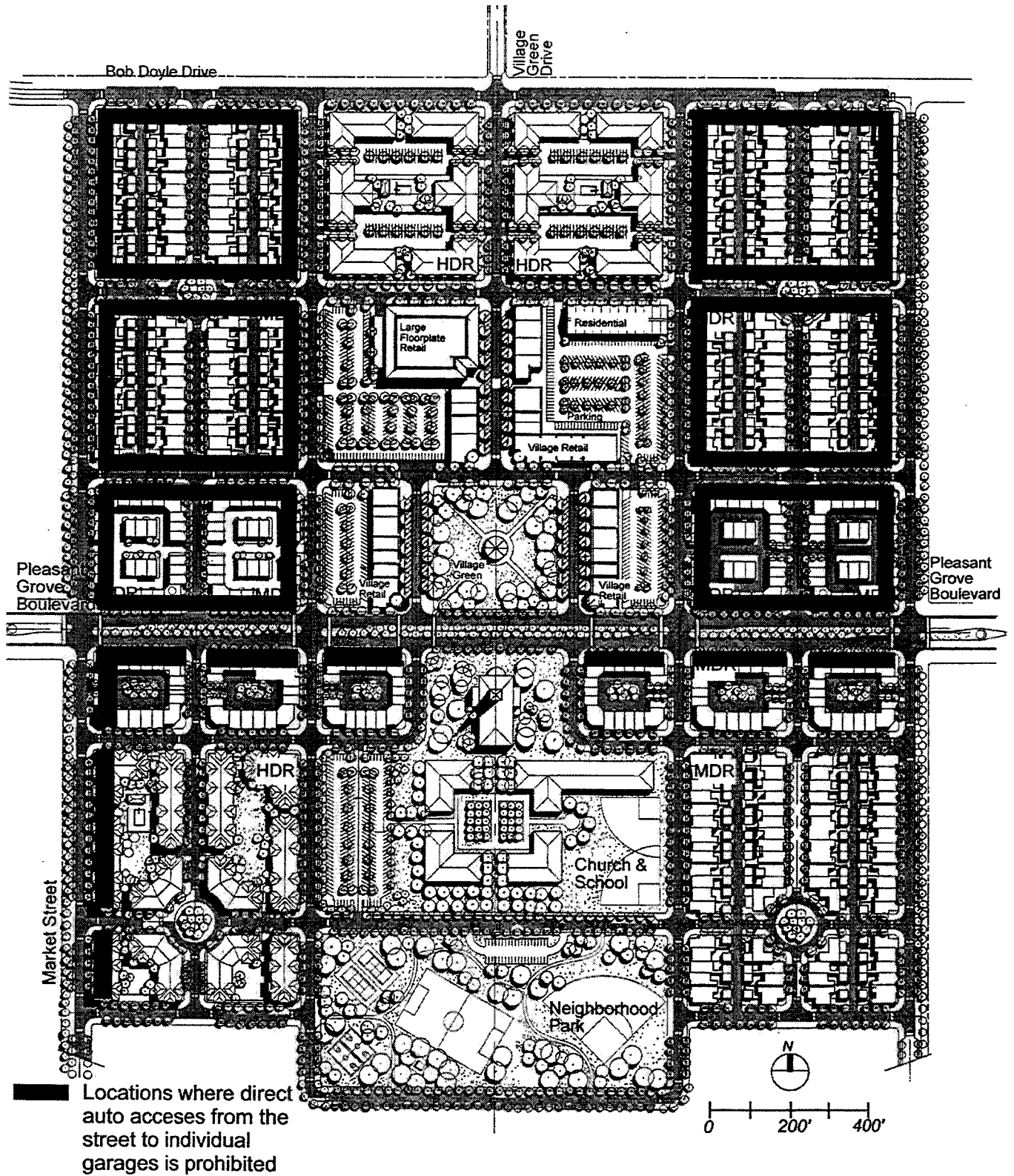
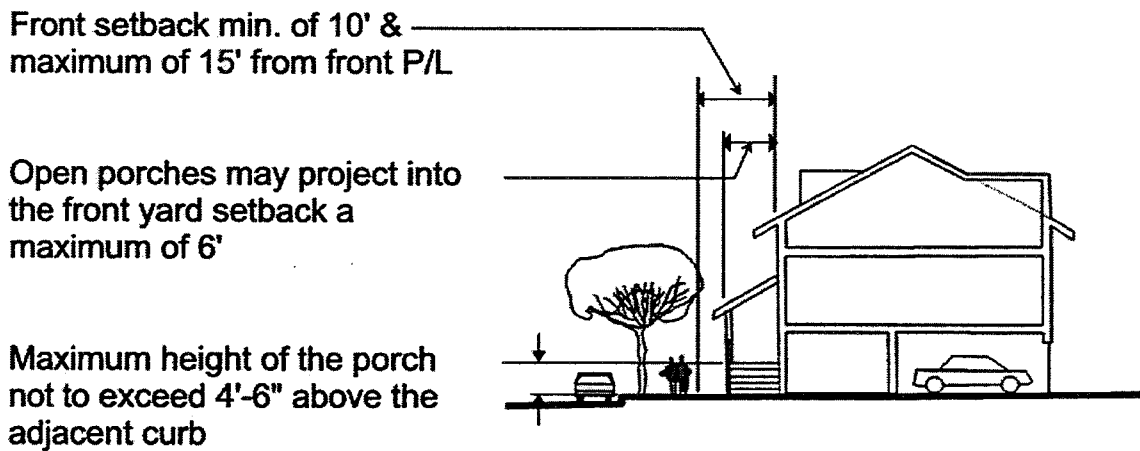
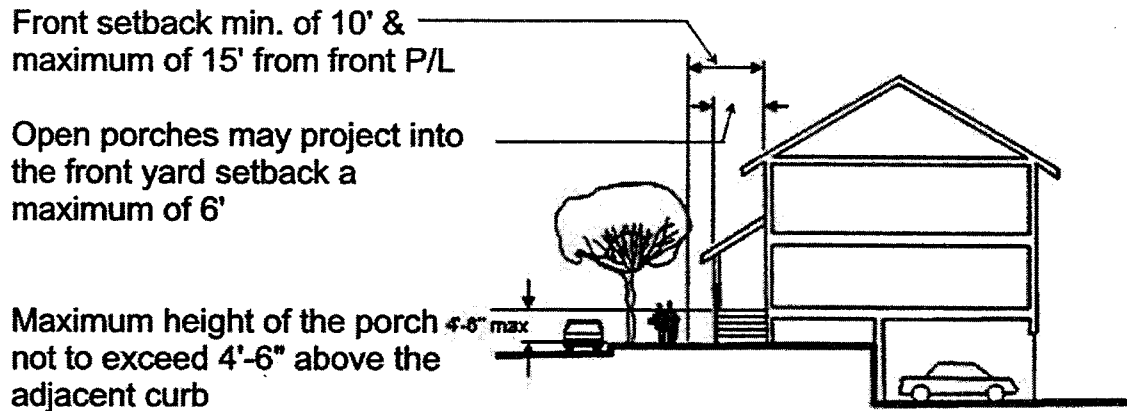


Figure 10-7: Village Center MDR Front Setback-Cross Sections



High Density Residential (VC-HDR)	
<i>Density Range:</i>	13.0 dwelling units per acre and higher
<i>Applied Zoning District:</i>	R3/DS: Attached Housing/Design Standards Overlay
<i>Description:</i>	<p>High Density Residential (HDR) land use is intended to accommodate attached multifamily housing. Similar to MDR, HDR units are required to have a strong orientation toward the street. A variety of higher density housing types are appropriate if designed with front forward architecture which includes front entry doors and porches facing the street, and off-street parking located in the rear of the lot and accessed from alleys or internal driveways. Typical unit types may include apartments, townhomes and condominiums. Figure 10-8 includes pictures of potential HDR unit types.</p> <p>Table 10-2 includes specific unit allocations and resulting densities for each HDR parcel. While it is anticipated that unit types may be mixed on a large lot parcel, the overall density shall average that prescribed on Table 10-2. In no case may a specific unit type on any HDR large lot parcel be below the density range specified by the HDR land use (13.0 du/ac).</p>
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	The Design Standards Overlay (DS) has been applied to all HDR parcels to provide for the unique nature and variety of housing types anticipated in the Village Center. Development standards are included on Table 10-5. All HDR development is subject to City approval of a Design Review Permit.



Typical HDR Product

Table 10-5: Village Center HDR Development Standards

R3/DS Development Standards (HDR)	
General Characteristics	
Product Type	Attached units (Apartment/Townhome/Condominium)
Garage Access	Via rear alley or auto court
Lot Size	
Area, Interior Lot	Varies
Area, Corner Lot	Varies
Width, Interior	Varies
Width, Corner	Varies
Permitted Density (maximum per lot)	
Residential Density	Per General Plan Land Use Designation
Setbacks and Treatments	
Street Setback ¹ (Figure 10-9)	10 ft. minimum 15 ft. maximum
Off-Street Parking Setback	All parking areas and garages shall be located and accessed internally via alleys or driveways.
Front Porches/ Stoops/Bay Windows (Figure 10-9)	Open porches may project up to 6 feet into front setback; Bay windows or other projections may project up to 3 feet into front setback; Steps/stoop may project to back of sidewalk. A projecting porch/stoop may be elevated but shall not exceed 4 feet 6-inches above the grade of the adjacent sidewalk.
Front Articulation	The residential design intent is to achieve a design that is varied, provides visual interest, and maintains a pedestrian scale for attached and detached housing. The front street façade shall be well articulated and shall reflect the following: <ul style="list-style-type: none"> ▪ <i>Wall Planes</i> – incorporating building projections and recesses, bay windows, front porches, entries, balconies, chimneys, and other elements. Multiple story (two or three story) walls in the same plane should be minimized and in no case shall any multiple-story wall comprise more than 50% of the front façade. ▪ <i>Rooflines</i> – utilizing a variety of roof forms, heights and styles. ▪ <i>Architectural Enhancements</i> – including decorative trim, shutters, columns and other articulation. ▪ <i>Finish</i> – combining multiple materials and textures to enhance architectural detail ▪ <i>Unit Variation</i> – using the above and other elements to create distinction between individual units.
Coverage	
Site Coverage	n/a
Height (maximum)	45 ft.
Parking (minimum)	
Spaces	Studio Unit - 1 off-street space per unit. One (1) Bedroom Unit- 1 off-street space per unit. Two (2) and above Bedroom Unit - 2 off-street spaces per unit. Age-Restricted Unit - 1 off-street space per unit. Guest parking shall be provided on-street (no additional off-street parking required).
Development Approval	
Processing	Each large-lot parcel shall be planned, processed and approved comprehensively. Large-lot parcels may not be split for purposes of development entitlement processing.

1. Setback adjacent to public roadway measured from back of sidewalk. Units adjacent to public roadways shall front to such roadways.

Note: The City may consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards and WRSP Design Guidelines, and provided the products demonstrate superior siting characteristics and architectural design merits.

Figure 10-8: Village Center HDR-Pictures

Variety of roof forms & heights within the facade

Variations in wall plane projections

Balcony details

Porch & Front Entries along the street Facade



Variety of roof forms & heights within the facade

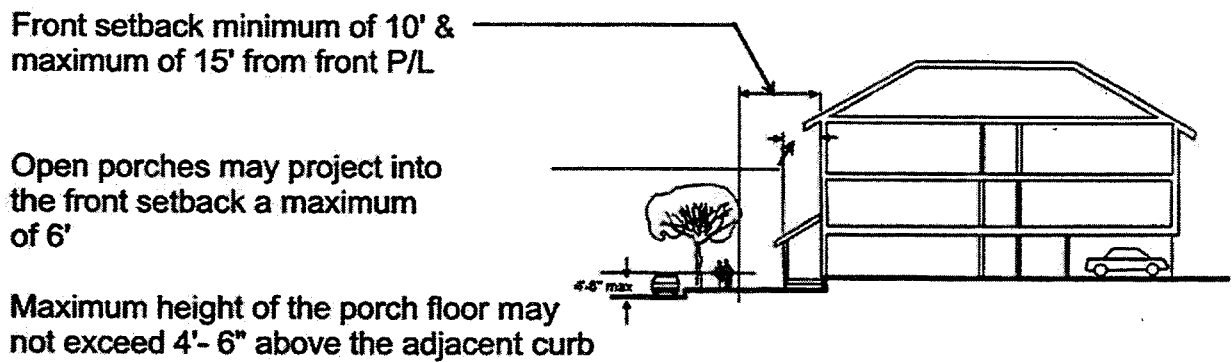
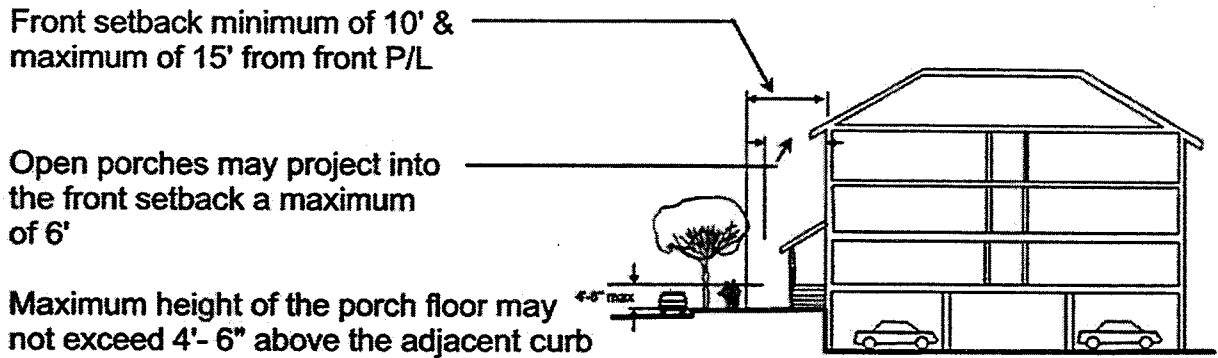
Variations in wall plane projections

Balconies

Porch & Front Entries along the street Facade



Figure 10-9: Village Center HDR Street Setback-Cross Sections



RETAIL & SERVICE USES



The integration of commercial land use that can accommodate retail businesses, service uses and community activities is a critical component of the Village Center and a central focus of its design. These uses, in combination with adjacent and mixed residential and public uses, create a diverse community with a variety of options that promote interaction. Both businesses and residents benefit from the proximity of uses, increasing the viability of walking, bicycling and transit as transportation options.

Similar to residential uses, emphasis is placed on the relationship of buildings to the street. Building facades are oriented to the street and located adjacent to the sidewalk. Off-street parking is located on well designed lots to the rear of the stores. On-street parking is maximized to provide a visual parking presence while meeting a portion of the parking demand. Numerous design elements, such as transparent storefronts, wide sidewalks, traditional style lighting, dense tree canopies, and coordinated street furnishings are provided to create a comfortable and distinctive “main street” setting. Restaurant seating, and other accessory services, are permitted to encroach onto public spaces to enhance an active pedestrian friendly environment and encourage social activity.

Community Commercial (VC-CC)	
<i>Applied Zoning District:</i>	CC/SA-WR: Community Commercial/Special Area Overlay
<i>Description:</i>	The Community Commercial (CC) land use is anticipated to accommodate a variety of retail and service activities. Typical uses may include retail shops, restaurants, local pubs, banks, grocery stores, convenience services, theaters and offices. These uses, within walking distance to and mixed with the Village Center residential, are planned to meet the everyday needs of local residents and promote non-vehicular forms of transportation. In addition, the commercial uses in the Village Center act to draw residents from throughout the community due to their unique form and setting.
<i>Permitted Uses:</i>	The Special Area (SA) overlay has been applied to modify permitted, conditionally permitted and administratively permitted uses. The intent is to provide for a unique mix of uses that support the Village Center concept. This includes permitting residential uses over commercial spaces to create live-work opportunities. In addition, in order to promote retail and other pedestrian attractive service activities along the streetscape, certain uses such as offices, vocational schools and community assembly are restricted (require a Conditional Use Permit) on ground floor spaces. Permitted uses are included on Table 10-6.
<i>Development Standards:</i>	Development standards have been modified by the Special Area (SA) overlay to provide for the distinctive nature of the Village Center. Development standards are included on Table 10-7. All commercial development is subject to City approval of a Design Review permit.



Table 10-6: CC/SA-WR Permitted Uses

AGRICULTURAL AND OPEN SPACE USE TYPES	CC/SA-WR
Resource Protection & Restoration	-
Resource Related Restoration	-

CIVIC USE TYPES	CC/SA-WR
Community Assembly	P/CUP(1)
Community Service	CUP
Essential Services	P
Hospital Services	
General Hospital Services	-
Psychiatric Hospital Services	-
Libraries & Museums, Private	P/CUP(1)
Public Parking Services	P
Schools	
College & University	P/CUP(1)
Elementary & Secondary	-
Private Elementary & Secondary	-
Social Services	
Food Distribution	-
Food Service	-
Temporary Resident Shelter	-

RESIDENTIAL USE TYPES		CC/SA-WR
Caretaker/Employee Housing		-
Dwelling		
	Multi-Family	P(2)
	Single-Family	-
	Two-Family	-
Family Day Care Home, Small		-
Family Day Care Home, Large		-
Single Room Occupant		-

COMMERCIAL USE TYPES		CC/SA-WR
Adult Business Establishments (3)		-
Animal Sales & Services		
	Grooming & Pet Stores	P
	Kennels	-
	Veterinary Clinics	P
	Veterinary Hospital	-
Automotive & Equipment		
	Automotive Rentals	-
	Automotive Repairs	-
	Automotive Sales	-
	Car Wash and Detailing	-
	Commercial Parking	-
	Heavy Equipment Rental, Repair and Sales	-
	Equipment Repair	-
	Gasoline Sales	-
Banks & Financial Services		P
Bars & Drinking Places		P
Broadcasting and Recording Studios		-
Building Materials Stores		-
Business Support Services		P
Commercial Recreation		
	Amusement Center	P
	Indoor Entertainment	P
	Indoor Sports and Recreation	P
	Outdoor Entertainment	-
	Outdoor Sports and Recreation	-
	Large Amusement Complexes	-
Community Care Facility		-
Day Care Center		-
Eating and Drinking Establishment		
	Fast Food with Drive Through	-
	Convenience	P
	Full Service	P
Food & Beverage Retail Sales		P
Funeral & Internment Services		-
Lodging Services		P/CUP(1)
Long Term Care Facility		-
Maintenance and Repair		P
Medical Services		
	General	P/CUP(1)
	Substance Abuse Treatment Clinic	-

COMMERCIAL USE TYPES (cont.)		CC/SA-WR
Neighborhood Commercial		-
Nightclubs (4)		-
Nursery, Retail		-
Offices, Professional		P/CUP(1)
Personal Services		P
Retail Sales and Services		P
Specialized Education & Training		
	Vocational Schools	P/CUP(1)
	Specialty Schools	P/CUP(1)
Storage, Personal Storage Facility		-

INDUSTRIAL USE TYPES		CC/SA-WR
Laundries, Commercial		-
Printing & Publishing		-
Research Services		-
Wholesale & Distribution, Light		-

TRANSPORTATION AND COMMUNICATION USE TYPES		CC/SA-WR
Antennas & Communications Facilities (5)		
	Developed Lot	-
	Undeveloped Lot	-
Heliport		-
Intermodal Facilities (6)		-

All highlighted uses have been modified by the WRSP SA Overlay.

- (P) Principally Permitted
- (CUP) Permitted through approval of a Conditional Use Permit
- (A) Administratively Permitted
- (-) Not Permitted

Notes:

- (1) Permitted by right on second floor, Conditional Use Permit required on ground floor.
- (2) Permitted on second floor only.
- (3) Additional Requirements contained in Chapter 19.32 of the City of Roseville Zoning Ordinance
- (4) Additional Requirements contained in Chapter 19.49 of the City of Roseville Zoning Ordinance
- (5) Additional Requirements contained in Chapter 19.34 of the City of Roseville Zoning Ordinance
- (6) Additional Requirements contained in Chapter 19.36 of the City of Roseville Zoning Ordinance

Table 10-7: Modified CC Development & Design Standards

CC/SA-WR Development & Design Standards	
Sidewalks	
<i>Design Intent: To provide adequate space to accommodate and promote pedestrian related activity.</i>	
Sidewalks	Minimum of 14-feet in width as measured from back-of-curb to the build-to-line; Sidewalks shall be decoratively paved with a pattern of unit pavers and colored concrete; Utility appurtenances located in sidewalks shall be avoided. Where required, they shall be integrated with the paving pattern to the extent feasible.
Setbacks	
<i>Design Intent: To ensure that buildings and associated activities are pulled forward toward the sidewalk and street.</i>	
Front Building Build-to-Line (Figure 10-10)	Back of sidewalk (14-feet from back-of-curb); All building facades facing the street shall be built up to the build-to-line.
Front Parking Setback	Minimum 6-foot landscape setback from back of sidewalk with screening. See Parking Screening Standards below.
Side & Rear Building Setbacks	Adjacent to Street - Front Build-to-Line Interior - None (zero side yard setbacks)
Entries, Recesses and Projections	
<i>Design Intent: To focus activity to the street and create inviting and highly accessible store fronts and activity spaces.</i>	
Entries	All buildings abutting Village Green Drive, Village Green Drive East or Village Green Drive West shall have their primary entry facing the street; Entrances shall not be spaced more than 50-feet apart along the length of the frontage.
Front Entry Recesses (Figure 10-10)	Maximum of 6-feet behind build-to-line (away from back of walk) for storefront entries with the width of the recess not to exceed 10-feet.
Front Building Recesses (Figures 10-10 & 10-11)	Maximum of 10-feet behind the build-to-line (away from back of walk) to provide for active outdoor uses (dining, seating, etc.) that are covered and oriented to the sidewalk and street.
Front Non-Building Projections (Figures 10-10 & 10-11)	Maximum of 5-feet beyond the build-to-line (sidewalk) to accommodate outdoor dining; The grade elevation of a projection shall be at the same level as the adjacent sidewalk; No permanent structures are allowed within the projection with the exception of ADA accommodations; A separation between outdoor dining areas and the sidewalk may be provided through low planters or other removable barriers no more than 4-feet in height.
Building Articulation	
<i>Design Intent: To create a mix of architectural styles that incorporate three traditional elements: base (bulkhead), body (storefront with windows) and cap (roofline/cornice). Building facades should emulate the character and composition of retail/commercial buildings typically found in small towns, define the open space of the street and achieve quality retail space.</i>	
Street Facades (Figure 10-12)	The front or street facade shall be well articulated using a variety of architectural elements including a variety of roof forms, building projections and recesses, window styles, balconies, decorative trim, flower boxes, awnings, cornice and belt courses, paint colors, and multiple finish materials; Corner parcels are encouraged to incorporate rounded or cut corners, special display windows, outdoor cafes, and other architectural features. Added height at corner elements is permitted to create a focal point (see Height Exception Standards below).
Store Fronts (Figure 10-12)	For those buildings fronting the build-to-line (sidewalk), 75% of the ground floor street facade shall be windows or transparent with the ability to see into the store. The use of tinted or reflective glass is prohibited; Window bulkheads shall not exceed 30-inches in height; No building abutting Village Green Drive, Village Green Drive East or Village Green Drive West shall have a blank wall exceeding 10-feet in length along the street; Solid or grided roll-down gates in front of storefronts are prohibited.
Upper Floors (Figure 10-12)	Shall be built to front build-to-line (sidewalk) and distinguished from ground floor storefronts through the use of a strong belt course or architectural expression line.

CC/SA-WR Development & Design Standards	
Rooflines (Figure 10-12)	Shall be varied and capped with a strong cornice line, roof treatment or combination of treatments.
Bay Windows	Ground Floor – May project 12-inches into the build-to-line (sidewalk) for a length not to exceed 12-feet. Upper Floors – May project up to 3-feet into the build-to-line (sidewalk) for a length not to exceed 10-feet. Shall maintain a minimum clearance of 15-feet above the sidewalk.
Entry Porticos	May project 12-inches into the build-to-line (sidewalk).
Awnings	May project up to 5-feet into the build-to-line (sidewalk) and shall maintain a minimum clearance of 9-feet above the sidewalk.
Flower Boxes & Hangings	Flower boxes/planters may project up to 18-inches into the build-to-line (sidewalk) and shall not exceed 2-feet in height (unless used to define outdoor dining area); Hanging flowerpots may project up to 18-inches into the build-to-line (sidewalk) and shall maintain a minimum clearance of 8-feet above the sidewalk.
Signage	Shall complement the style of architecture and be integrated into the building's design; Acceptable sign types include, halo, lit or reverse pan channel, and indirectly lit panel signs; Internally illuminated cabinet signs, pan channel signs and exposed railways are prohibited; Blade or shingle-type signs are encouraged under canopies and awnings as close as practical to the horizontal middle of the building; Signs may not project more than 4-feet horizontally into the build-to-line (sidewalk) and shall maintain a minimum clearance of 9-feet above the sidewalk. A maximum of 10% of the window area may be used for temporary signs.
Rear Facades	Building facades facing off-street parking shall be designed to present an inviting façade to the parking area and surrounding street, and may include direct retail entrances; Service doors and areas shall be designed as an integral part of the architectural building façade.
Lighting	Shall be designed as an integral part of the building façade to provide an ambient level of light on the street front building façade. Lighting shall provide low-level wash on the face of the building.
Height	
Design Intent: <i>To create building walls and massing that define, but do not overwhelm, the public spaces.</i>	
Height Limit (Figure 10-13)	35-feet maximum; In no case shall the top of a building parapet or roof façade along the storefront be less than 25-feet.
Ground Floor (Figure 10-13)	Minimum 16-feet as measured from ground floor grade to second floor or roof to ensure quality retail space.
Exceptions	Towers, cupolas, steeples, dormers, spires, flagpoles, and similar architectural elements if the element is integrated into the overall architectural design and occurs at significant locations within the Village Center (eg. building corners) providing a landmark to reinforce the sense of place.
Mid Block Pedestrian Ways	
Design Intent: <i>To link the off-street parking to the primary retail streetscape which provides the primary pedestrian circulation within the Village Center.</i>	
Location	One mid-block pedestrian way is required between off-street parking and the street (sidewalk) in each block. The pedestrian ways are encouraged to align on opposite blocks to facilitate a mid-block street crossing on the primary retail streets.
Width (Figure 10-14)	Pedestrian ways shall be a minimum of 15-feet and a maximum of 25-feet in width.
Treatment (Figure 10-14)	Paving, landscaping and illumination along pedestrian ways shall be consistent with the design and quality of the streetscape; Retail entries, show windows and displays are encouraged along the facades aligning the pedestrian ways; Porticos or trellises that project from the face of the buildings should be located along the street and within the parking area to announce the location of the pedestrian ways. The portico or trellis may project into the build-to-line (sidewalk) a maximum of 6-feet and into the parking area a maximum of 8-feet, while maintaining a minimum vertical clearance of 9-feet.

CC/SA-WR Development & Design Standards	
Large Floor Plate Retail	
<i>Design Intent: To break down the mass of large buildings so that they are generally consistent with the smaller Village Center retail buildings.</i>	
Building Façades: Articulation and Form (Figure 10-15)	Building facades should emulate the scale of the small Village Center retail by physically articulating walls and/or providing changes of materials with a varying module of 35 to 50-feet along the street. Changes in the roof or parapet height are encouraged when consistent with the articulation of the wall. Special architectural features, such as turrets, raised roof elements etc., should be at the entries to signify and highlight the entrances to the building. The building must meet the requirements of the build-to line on the primary ¹ and secondary ² retail streets.
Building Façades: Window Placement and Rhythm (Figure 10-15)	Building façades shall have windows with glazing that permits visibility into the store along all façades facing a primary retail street ¹ and along the primary building wall facing the store's major parking field. <ul style="list-style-type: none"> ■ a 30-inch maximum high bulkhead for a minimum of 75% of the length of the wall. ■ constructed of quality materials and is highly articulated with architectural detailing. ■ above a 30-inch maximum high bulkhead for a minimum of 50% of the length of the wall. ■ constructed of quality materials and is highly articulated with architectural detailing.
Building Entries (Figure 10-15)	A primary entry shall be located facing the primary retail street ¹ with the doors located within 25-feet of the build-to line (sidewalk). For the purposes of creating activity along the retail street frontage, the primary entry shall be functional and not treated as a "back door" to the business; A small pedestrian plaza (minimum 25' X 25') is encouraged to be located at the entry adjacent to the primary retail street; ¹ A secondary entrance may be allowed facing the main parking field.
Service Areas (Figure 10-15)	Truck dock and service areas shall be fully enclosed (including a roll up door) and of sufficient size to accommodate the largest service truck within the enclosed service area, mechanical equipment, trash and cardboard collection, etc.; Service areas shall be accessed from the secondary streets. ² No service access is allowed from a primary retail street. ¹
Parking Access	Access to parking areas from streets that have diagonal parking shall be limited. The City may approve an access point to the street between the large floor plate retail and adjacent in-line shop buildings.
Shopping Carts	A shopping cart management plan is required and shall be approved concurrently with the associated Design Review permit.
Parking	
<i>Design Intent: To provide adequate parking while minimizing the visual impacts of off-street parking areas.</i>	
Off-Street Parking (minimum and maximum)	Retail/Office/Service – One (1) space per 300 square feet. Residential – One (1) space per unit. All other uses per City of Roseville Zoning Ordinance.
On-Street Parking	On-street parking shall be in addition to required off-street parking; Diagonal on-street parking will be privately owned and maintained. Such spaces shall be distinguished from the public street (right-of-way) by a colored paving mark.
Off-Street Parking Access	Vehicular access to off-street parking shall be from secondary streets and not from Village Green Drive, Village Green Drive East or Village Green Drive West. (See exception for Large Floor Plate Retail above.)
Screening (Figure 10-16)	The perimeter of off-street parking areas abutting a street shall have a minimum 6-foot wide landscape setback, with a 4-foot high concrete/masonry wall along the parking lot side of the setback.
Development Approval	
Processing	All CC/SA-WR large-lot parcels shall be planned, processed and approved concurrently.

1. Streets with retail on both sides or retail on one side and park on the opposite side.
2. Streets with retail on one side and residential on the opposite side.

Figure 10-10: Village Center CC Front Setbacks Recesses and Projections-Plan View

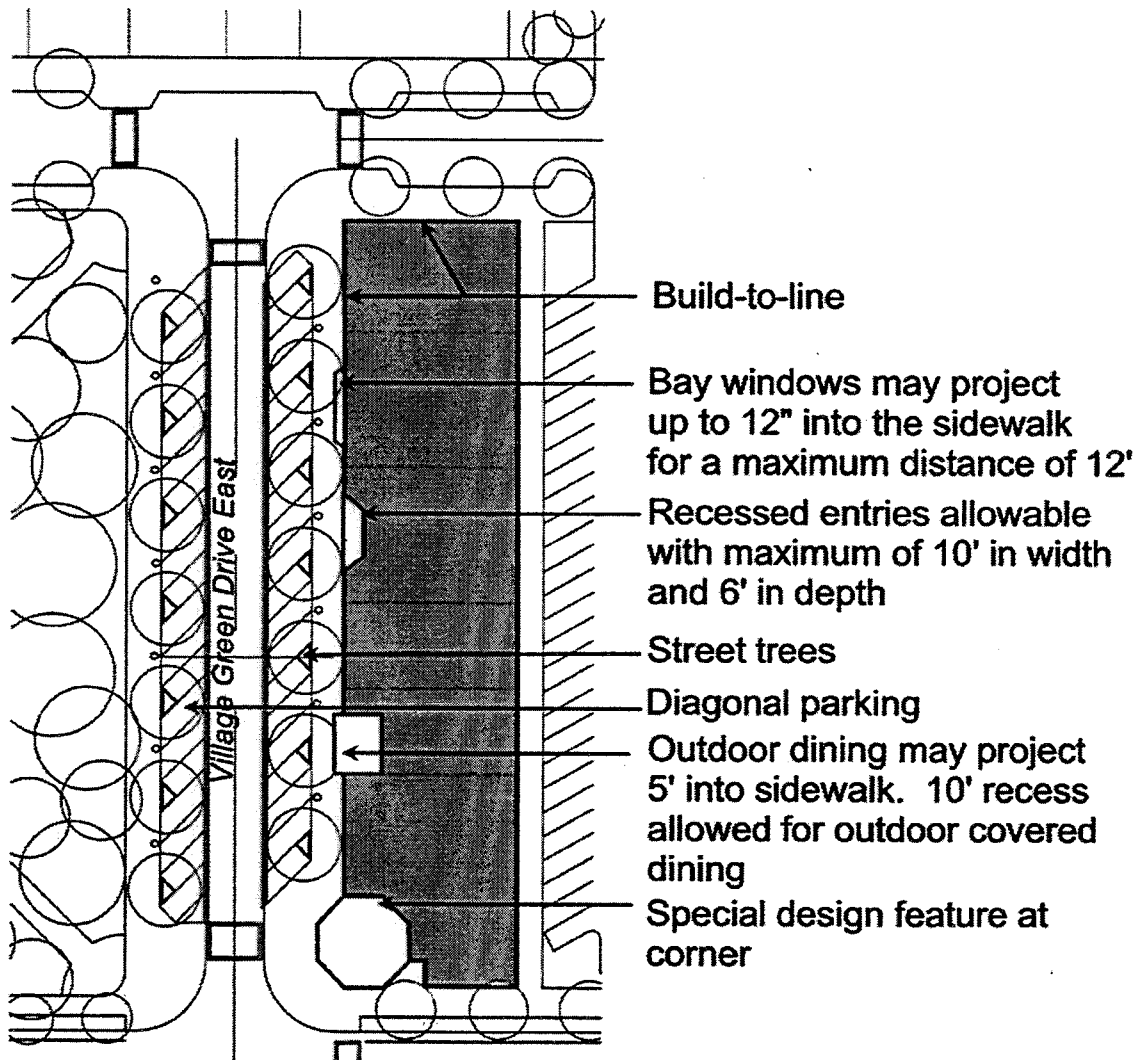


Figure 10-11 Village Center CC Front Recess and Projection-Cross Section

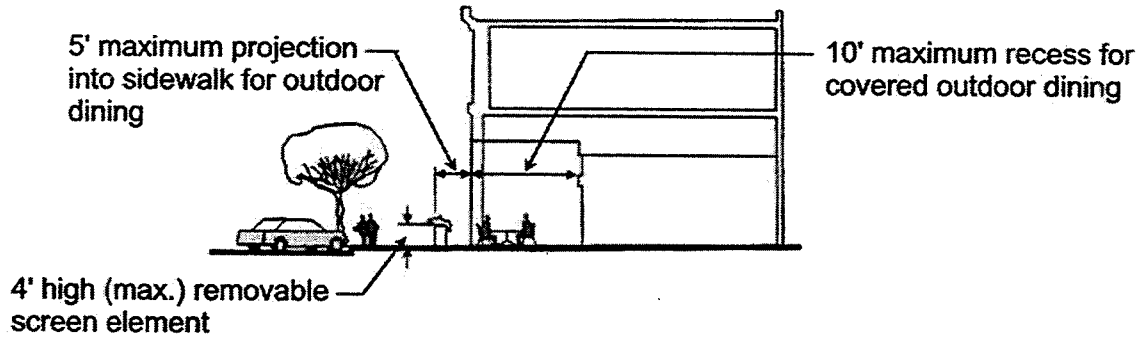


Figure 10-12: Village Center CC Façade-Illustrative Concept

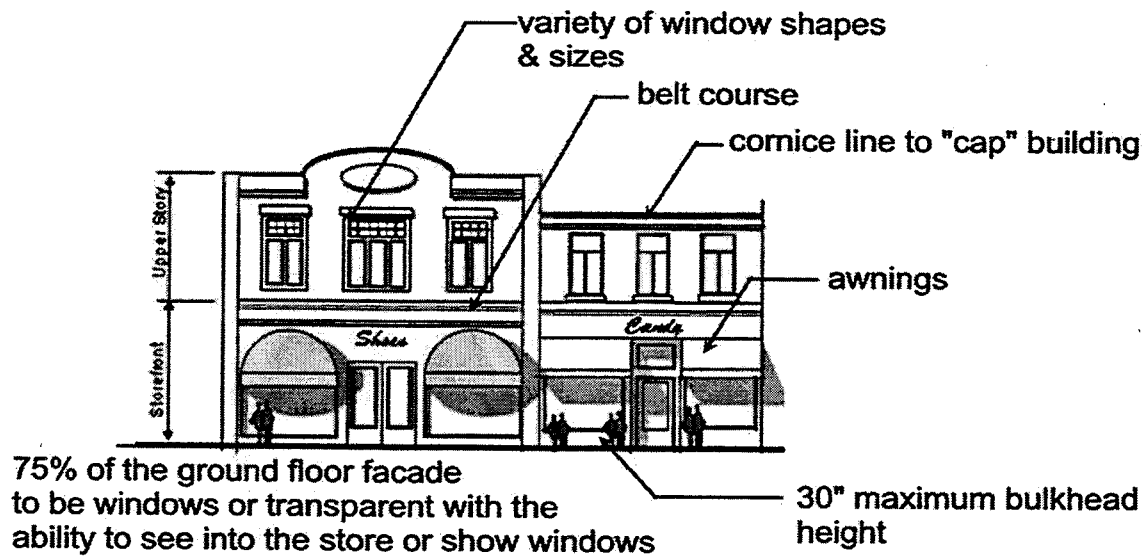


Figure 10-13: Village Center CC Building Height-Cross Section

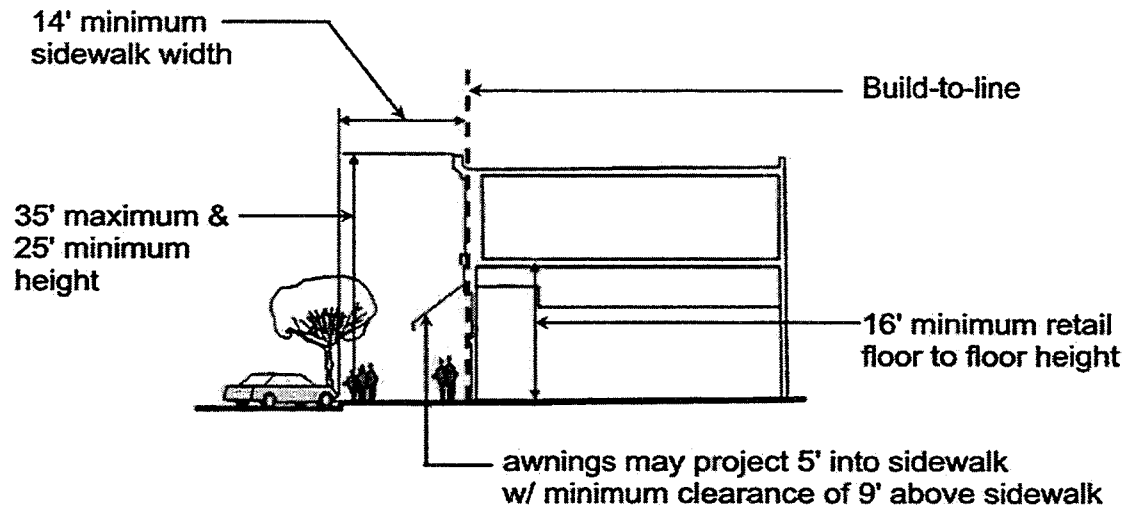


Figure 10-14: Mid-Block Pedestrian Way-Plan View

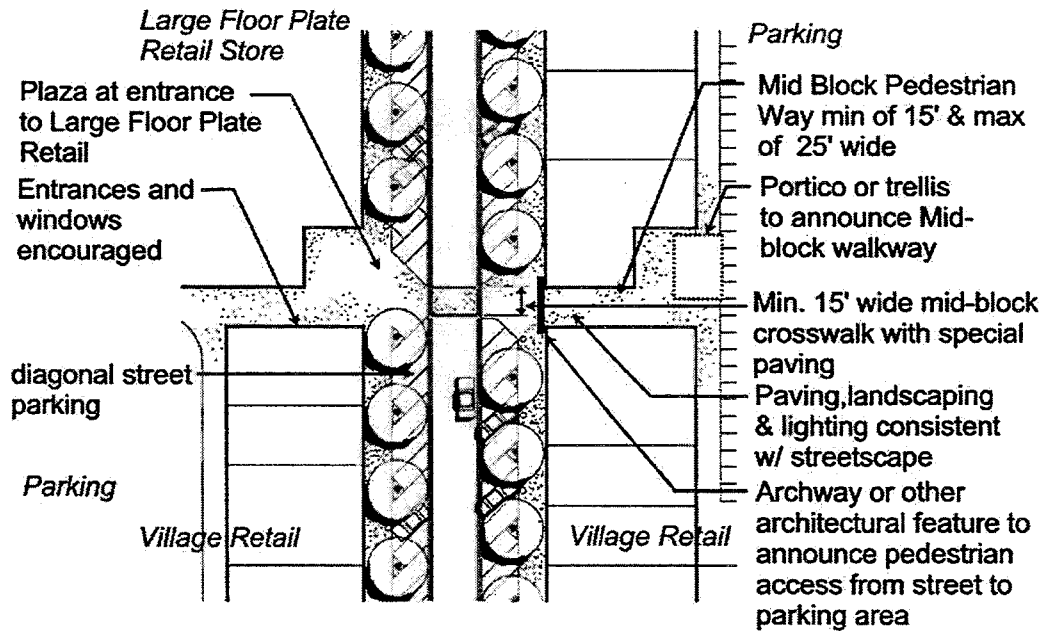


Figure 10-15: Village Center Large Floor Plate Retail-Plan View

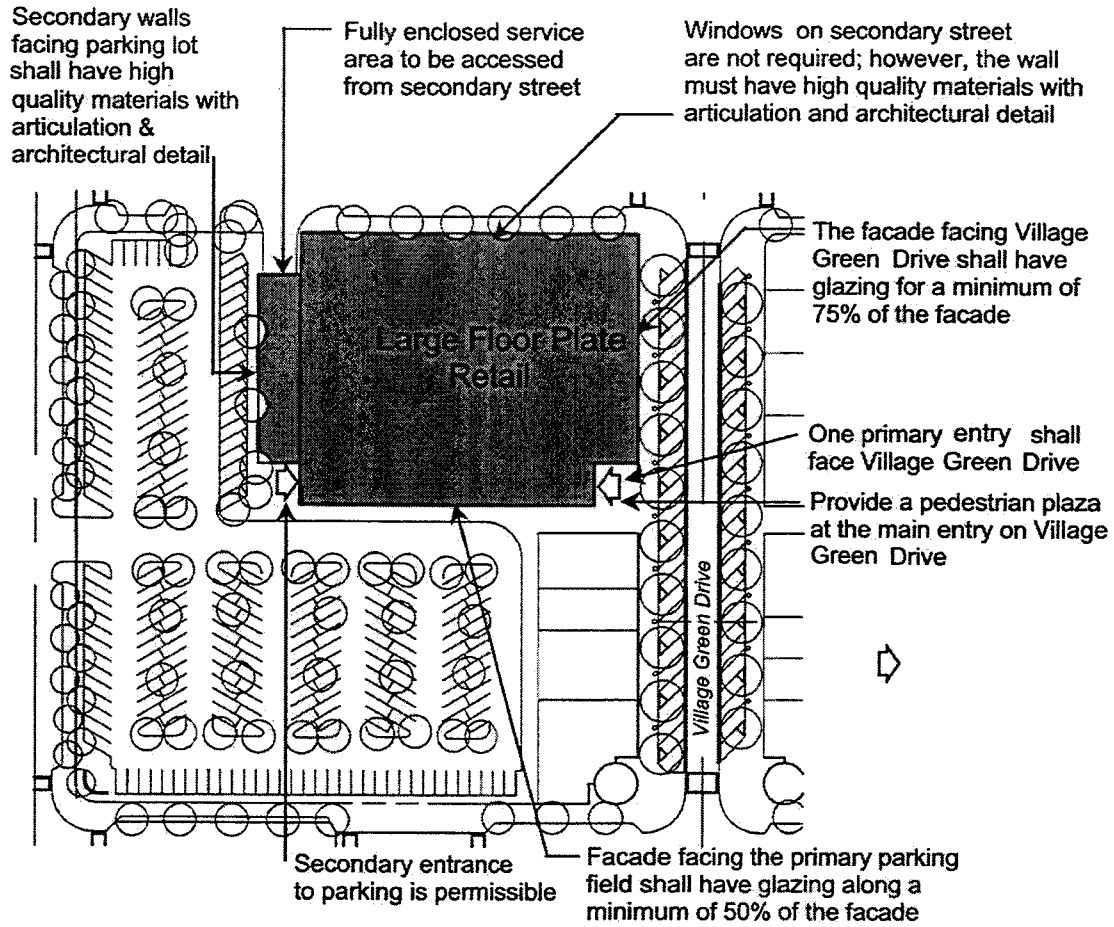
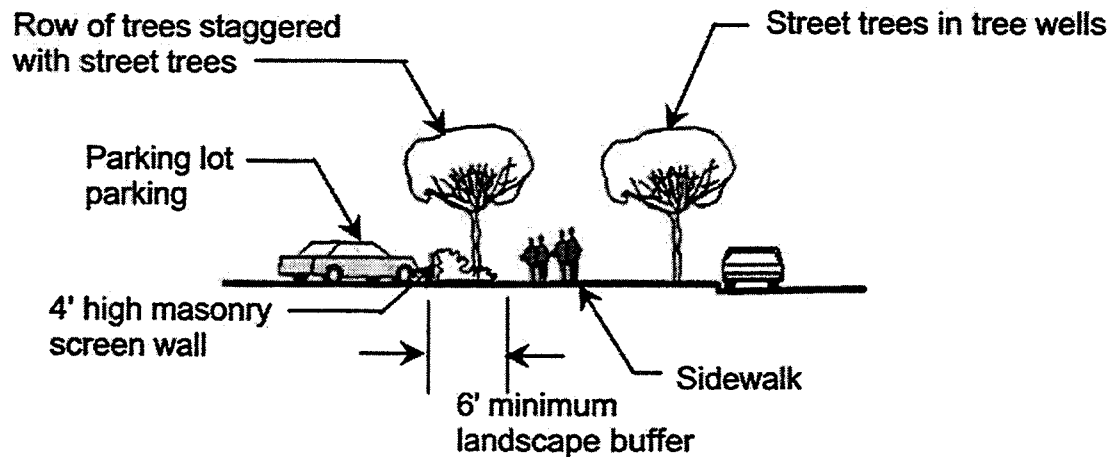


Figure 10-16: Village Center Parking Screening-Cross Section



PUBLIC & QUASI-PUBLIC SPACES



The Village Center provides public and quasi/public spaces for people to gather and to reinforce community identity. The centrally located Village Green, a 3.5-acre traditional urban park element, provides the visual focus of the Village Center. The Village Green is intended to accommodate passive recreation, provide a visual respite for residents, shoppers and employees, and form a community gathering place. It includes a central pavilion as a venue for a wide variety of community activities,

including concerts, fairs, exhibits, markets and other events that will bring the community to the Village Center. The form of the Village Center calls for the adjacent commercial uses to face onto the Village Green. A second park site, a City neighborhood park, is included on the south end of the Village Center. This 9.1-acre park is intended to meet the active recreational needs of residents within the Village Center and adjacent neighborhoods.

The Village Center incorporates a 10.9-acre Public/Quasi-Public site to be used for a church and private school. The church and private school create another identifying community element and activity center. The church site has been positioned to face the Village Green from across Pleasant Grove Boulevard, and is aligned with the axis of Village Green Drive creating a visual focal point. The Church is required to include a spire, tower or other architectural element to align with the visual centerline of Village Green Drive and provide a focal point for the Village Center.



Park & Recreation (VC-P/R)	
<i>Applied Zoning District:</i>	PR – Parks & Recreation
<i>Description:</i>	The Village Center includes two park sites, the Village Green and a traditional neighborhood park. These facilities are further described in Section 10.6. While located within the Village Center, these parks are components of the overall WRSP park and recreation program discussed in the Public Services Plan, Section 8.
<i>Permitted Uses:</i>	As specified in the City of Roseville Zoning Ordinance.
<i>Development Standards:</i>	As specified in the City of Roseville Zoning Ordinance.

Public/Quasi-Public (VC-P/QP)	
<i>Applied Zoning District:</i>	P/QP/SA-WR
<i>Description:</i>	The Public/Quasi-Public land use is anticipated to accommodate a church and private school.
<i>Permitted Uses:</i>	The Special Area (SA) overlay has been applied to modify permitted, conditionally permitted and administratively permitted uses. The intent is to focus permitted uses on the church and associated private school. Permitted uses are included on Table 10-8.
<i>Development Standards:</i>	As specified in the City of Roseville Zoning Ordinance. The church is required to include a spire, tower or other architectural element to align with the visual centerline of Village Green Drive to provide a focal element and point of orientation for the Village Center.

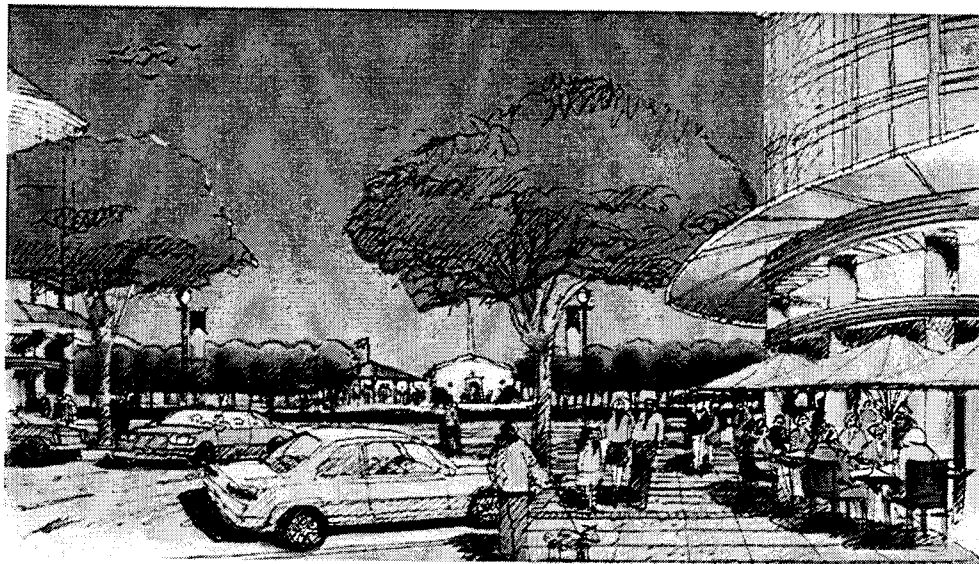


Table 10-8: P/QP/SA-WR Permitted Uses

AGRICULTURAL AND OPEN SPACE USE TYPES	P/QP/SA-WR
Agricultural	-
Animal Keeping	-
Resource Protection & Restoration	-
Resource Related Restoration	-

CIVIC USE TYPES	P/QP/SA-WR
Community Assembly	P
Community Service	P
Essential Services	P
Hospital Services	
General Hospital Services	-
Psychiatric Hospital Services	-
Intensive Public Facilities	-
Libraries & Museums, Private	-
Public Parking Services	P
Schools	
College & University	-
Elementary & Secondary	-
Private Elementary & Secondary	P

RESIDENTIAL USE TYPES	P/QP/SA-WR
Caretaker/Employee Housing	A
Dwelling, Single-Family	A

COMMERCIAL USE TYPES	P/QP/SA-WR
Commercial Recreation	
Indoor Sports and Recreation	P
Outdoor Entertainment	CUP
Outdoor Sports and Recreation	CUP
Large Amusement Complexes	-
Day Care Center	P

All highlighted uses have been modified by the WRSP SA Overlay.

- (P) Principally Permitted
- (CUP) Permitted through approval of a Conditional Use Permit
- (A) Administratively Permitted
- (-) Not Permitted

MINOR RESIDENTIAL DENSITY ADJUSTMENTS

Minor Residential Density Adjustments are permitted within the Village Center subject to the requirements of Section 4.3, with the following additions:

1. The Minor Density Adjustment may not result in a reduction of units within the Village Center.
2. The Minor Density Adjustment may only be approved if determined by the Planning Director that it improves the ability to achieve the Planning Principles for the Village Center and produce the residential product types envisioned.

10.4 AFFORDABLE HOUSING PLAN

Consistent with the General Plan affordable housing goal, ten percent (10%) of the units in the WRSP have been designated for middle-, low- and very-low income households. The WRSP affordable housing goal has been allocated to specific residential parcels. This includes the allocation of 72 low-income rental and 72 very low-income rental units to Parcel W-25 in the Village Center. Affordable units shall be provided in accordance with Section 5 of the Specific Plan. Included is the requirement for a *Supplemental Affordable Housing Development Agreement* to detail specific requirements and obligations prior to the issuance of any building permits for Parcel W-25.

10.5 CIRCULATION PLAN

The Village Center is a district where preference is given to the pedestrian rather than the automobile. The mix, proximity, and intensity of uses, along with the underlying grid street pattern, encourage alternative forms of transportation including walking, bicycling and transit. All uses are in easy walking distance, within 1,500 feet of the Village Green and a central transit stop. Numerous design elements, including wide sidewalks, enhanced pedestrian crossings, mid-block pedestrian ways, dense tree canopies, street furnishings, pedestrian scale lighting, signage and front forward building designs are intended to promote a comfortable and inviting pedestrian environment.

In accordance with the City's General Plan, the Village Center is designated as a Pedestrian District. The intent of the Pedestrian District is to place a greater emphasis on the pedestrian rather than the automobile by implementing measures to improve walkability through enhanced safety, security and convenience within and through the District. The Village Center incorporates numerous elements, from its basic form to unique pedestrian and vehicular

circulation standards, which promote walkability and support the intent of the Pedestrian District. In designating the Village Center as a Pedestrian District, the City acknowledges that certain design features may slow the speed of vehicle travel and may reduce the vehicle level of service. The Pedestrian District overlay has been applied to the entire Village Center area.

□ ROADWAYS

The Village Center incorporates a traditional grid circulation pattern providing multiple routes and options for both pedestrians and automobiles. The conventional suburban emphasis on a hierarchy or functional classification of roadways is replaced by a general uniformity of street options. All roadways place equal emphasis on vehicular and pedestrian transportation. The unique nature of the Village Center roadway system necessitates variations to typical City street standards including on-street diagonal parking, wider sidewalks abutting commercial frontages, and sidewalk “bulb-outs” at intersections.

The Village Center’s grid roadway system shall include:

- Short blocks of 300-feet in length or less;
- Straight streets; and,
- Intersections at regular intervals.

Roads will be in compliance with emergency vehicle access requirements. Internal streets included within projects on Village Center large lot parcels are required to complement the overall grid network. In addition, coordination is required to ensure that roadways and pedestrian access points in adjacent development connect to Village Center roadways and sidewalks.

The Village Center Circulation Plan is reflected on Figure 10-17, with lane capacity, right-of-way, sidewalk widths, parking and general landscape treatments summarized on Table 10-9. All roadways within the Village Center are required to be in conformance with Figure 10-17 and Table 10-9. Specific roadway sections for Village Center roadways are included as Figures 10-18 through 10-22. Landscape treatments, street light details and other streetscape elements are included in the WRSP Design Guidelines (Section 12).

Figure 10-17

Village Center Circulation Plan

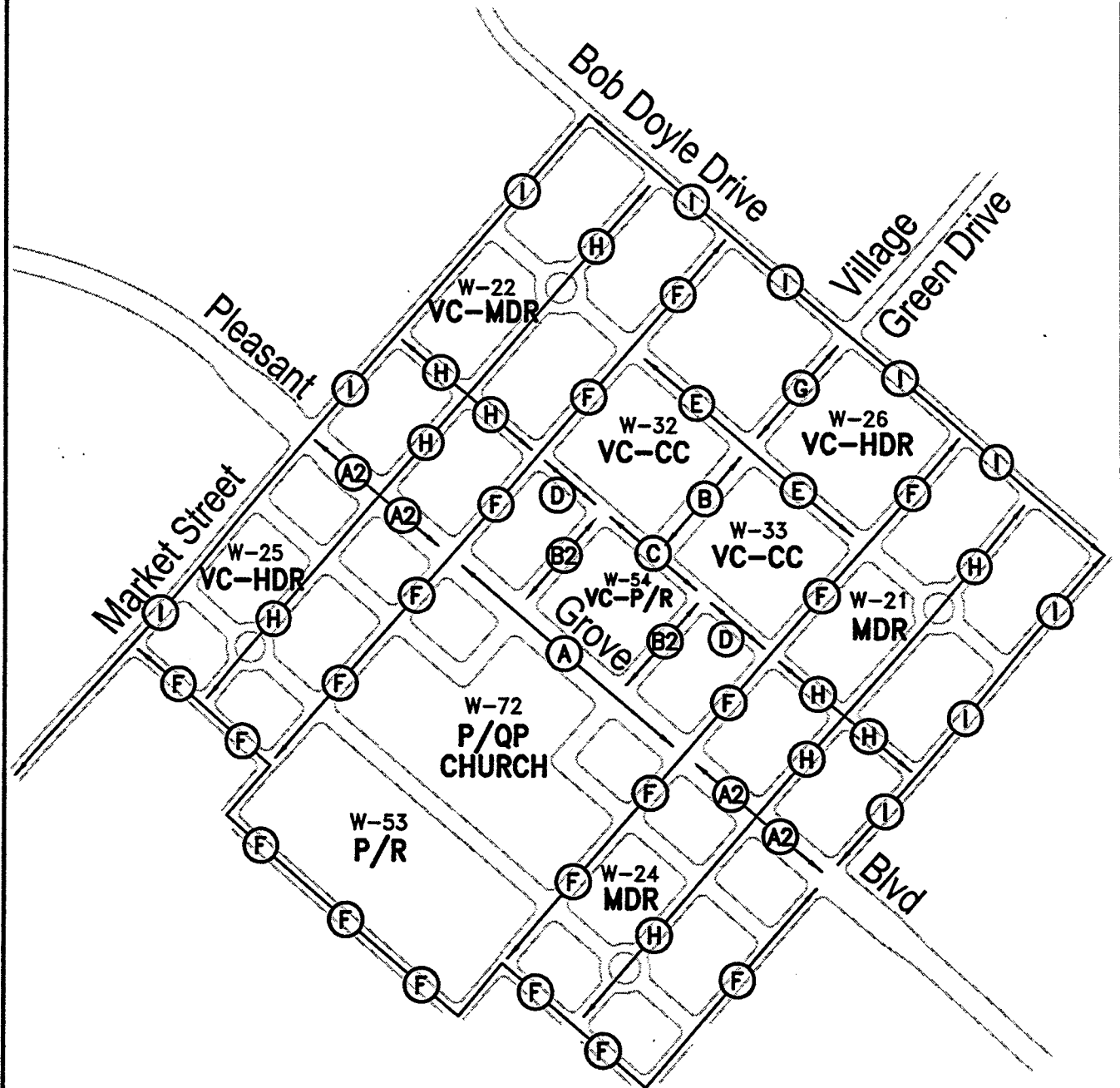


Table 10-9: Village Center Roadway Summary

Street	Lanes	Right-of-Way	Sidewalk Width	On-Street Parking	Landscape Treatment	Landscape Median	Figure
ARTERIAL ROADWAY							
Pleasant Grove Blvd. (A) (Adjacent to Commercial & Park)	4	92'	6'-12' ¹	None	6' Planter Strip	30'	10-18
Pleasant Grove Blvd. (A2) (Residential on both sides)	4	92'	6'	None	6' Planter Strip	30'	10-18
COMMERCIAL ROADWAYS							
Village Green Drive (B)	2	27'	14'	Diagonal	Tree Planters	-	10-19
Village Green Drive East/West (B2)	2	27'	12'-14' ²	Diagonal	Tree Planters	-	10-19
East/West at Village Green (C)	2	38'	12'-14' ²	Parallel	Tree Wells	-	10-20
East/West at Commercial (D)	2	38'	14'	Parallel	Tree Wells	-	10-20
Commercial/Residential Street (E)	2	38'	6'-12' ³	Parallel	Tree Wells & 6' Planter Strip ⁴	-	10-20
Collector at Perimeter (I)	2	42'	5'	Parallel	5' Planter Strip	-	10-22
RESIDENTIAL ROADWAYS							
Major Residential Street (F)	2	58'	5'	Parallel	5' Planter Strip	-	10-21
Residential Entry Street (G)	2	62'	6'	Parallel	6' Planter Strip	-	10-21
Minor Residential Street (H)	2	54'	5'	Parallel	5' Planter Strip	-	10-22
Alley (public)	1	20'	-	-	-	-	-

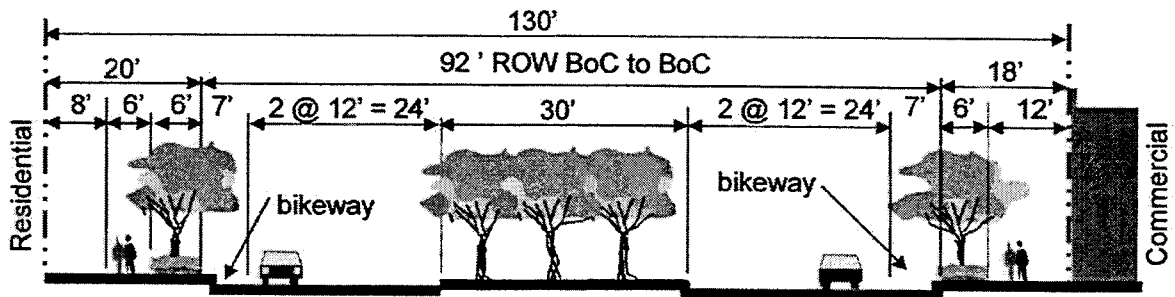
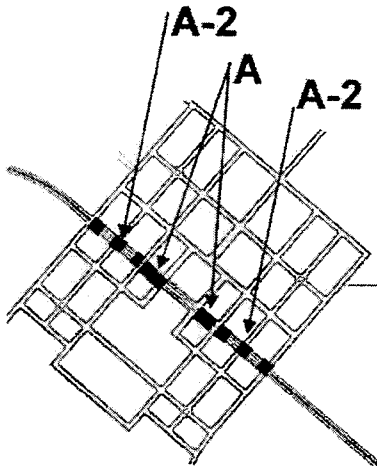
1. 12-foot sidewalk required adjacent to commercial and park; 6-foot sidewalk required adjacent to residential and public/quasi-public.
2. 14-foot sidewalk required adjacent to commercial; 12-foot wide sidewalk adjacent to park.
3. 12-foot sidewalk required adjacent to commercial; 6-foot sidewalk required adjacent to residential.
4. Tree wells adjacent to commercial; 6-foot planter strip adjacent to residential.

Note: See WRSP Design Guidelines, for landscape requirements and other details.

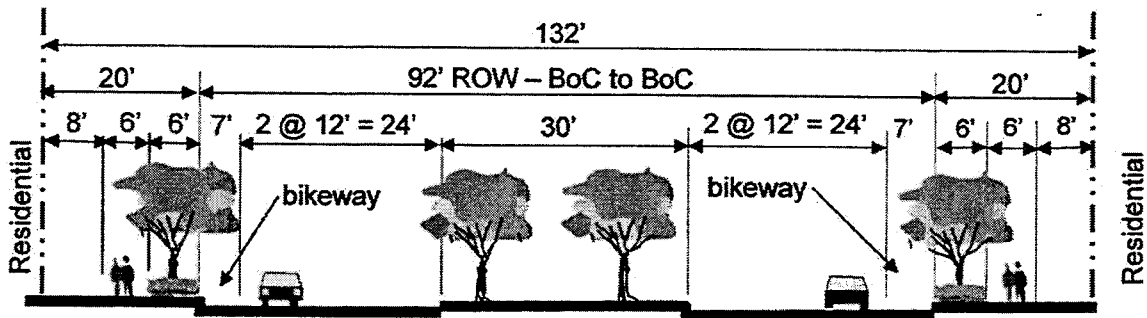


Typical Commercial Roadway

Figure 10-18: Village Center Pleasant Grove Boulevard Street Sections

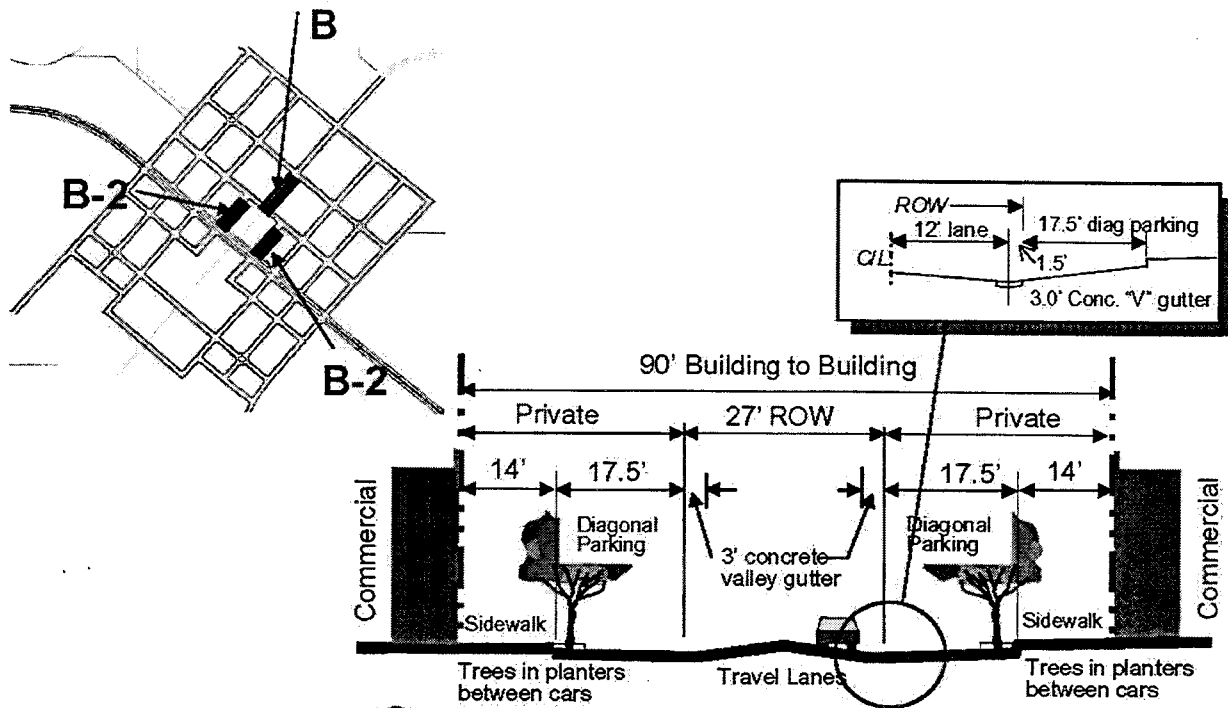


Section A
Pleasant Grove Blvd. @ Commercial

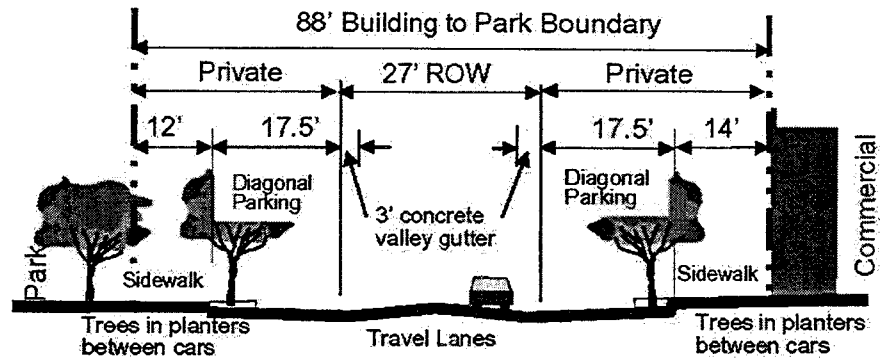


Section A-2
Pleasant Grove Blvd. @ Residential

Figure 10-19: Village Center Village Green Drive Street Sections

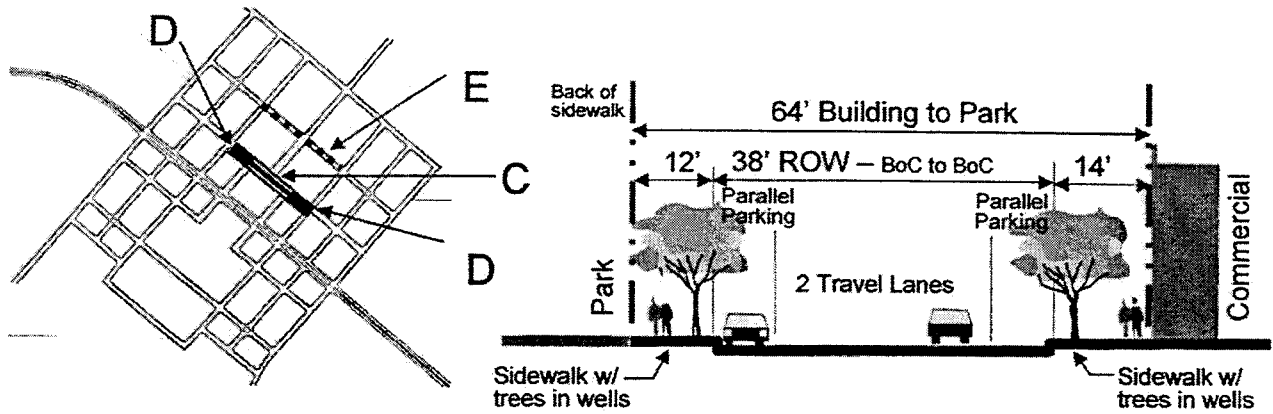


Section B
Village Green Drive

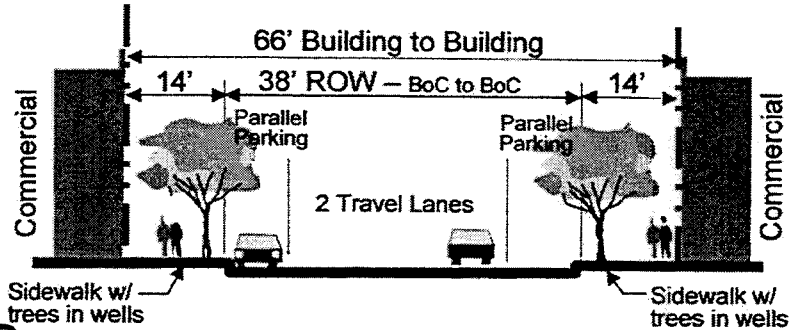


Section B-2
Village Green Drive East & West

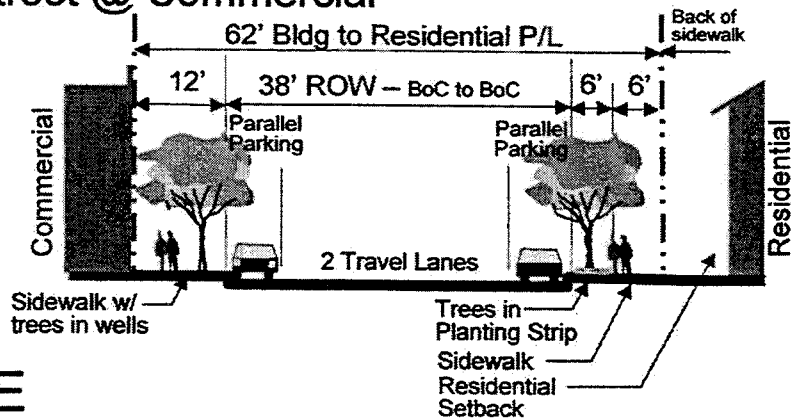
Figure 10-20: Village Center Commercial Street Sections



Section C
East/West Street @ Village Green

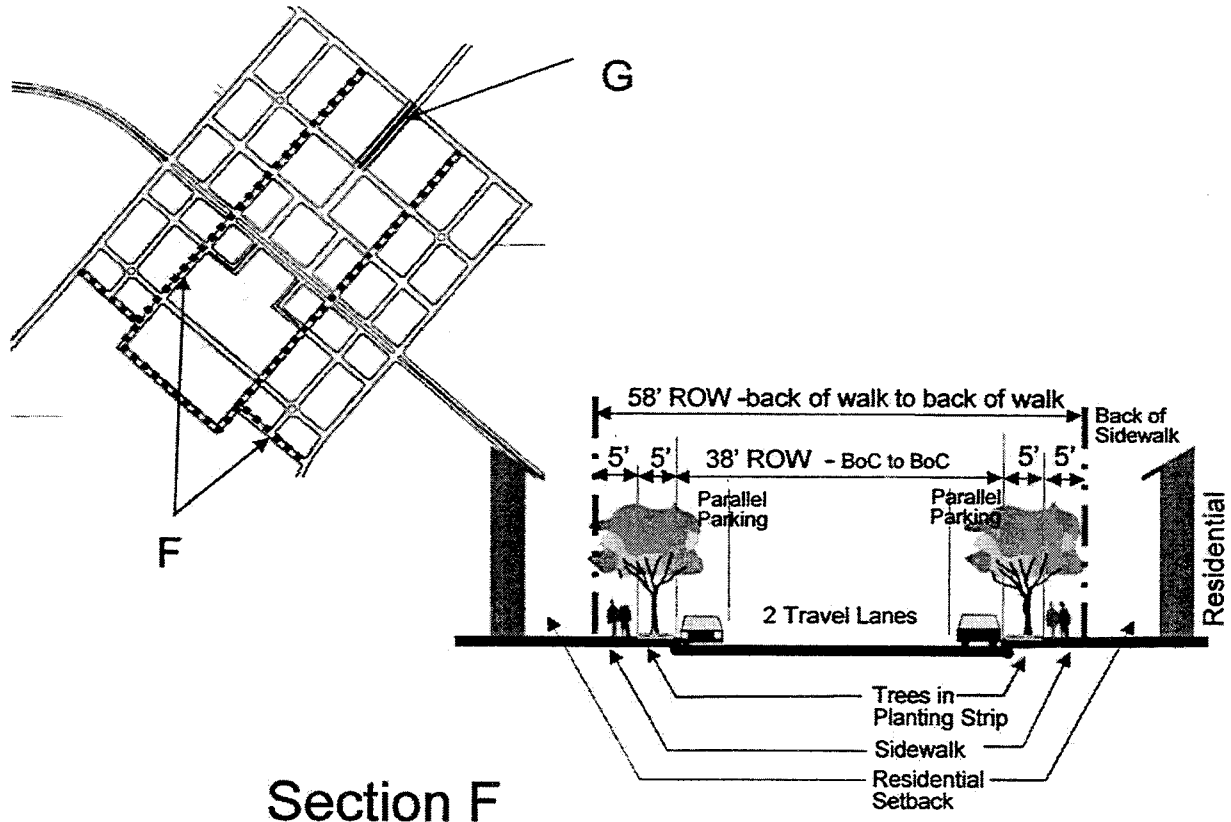


Section D
East/West Street @ Commercial

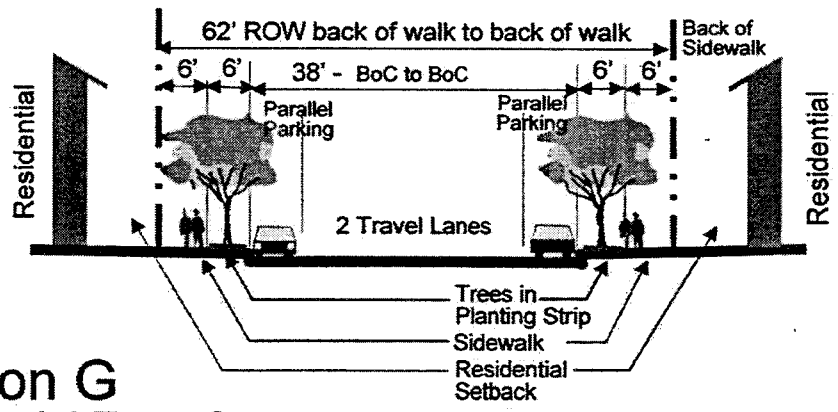


Section E
East/West Street @ Commercial/Residential

Figure 10-21: Village Center Master Residential and Entry Residential Street Sections

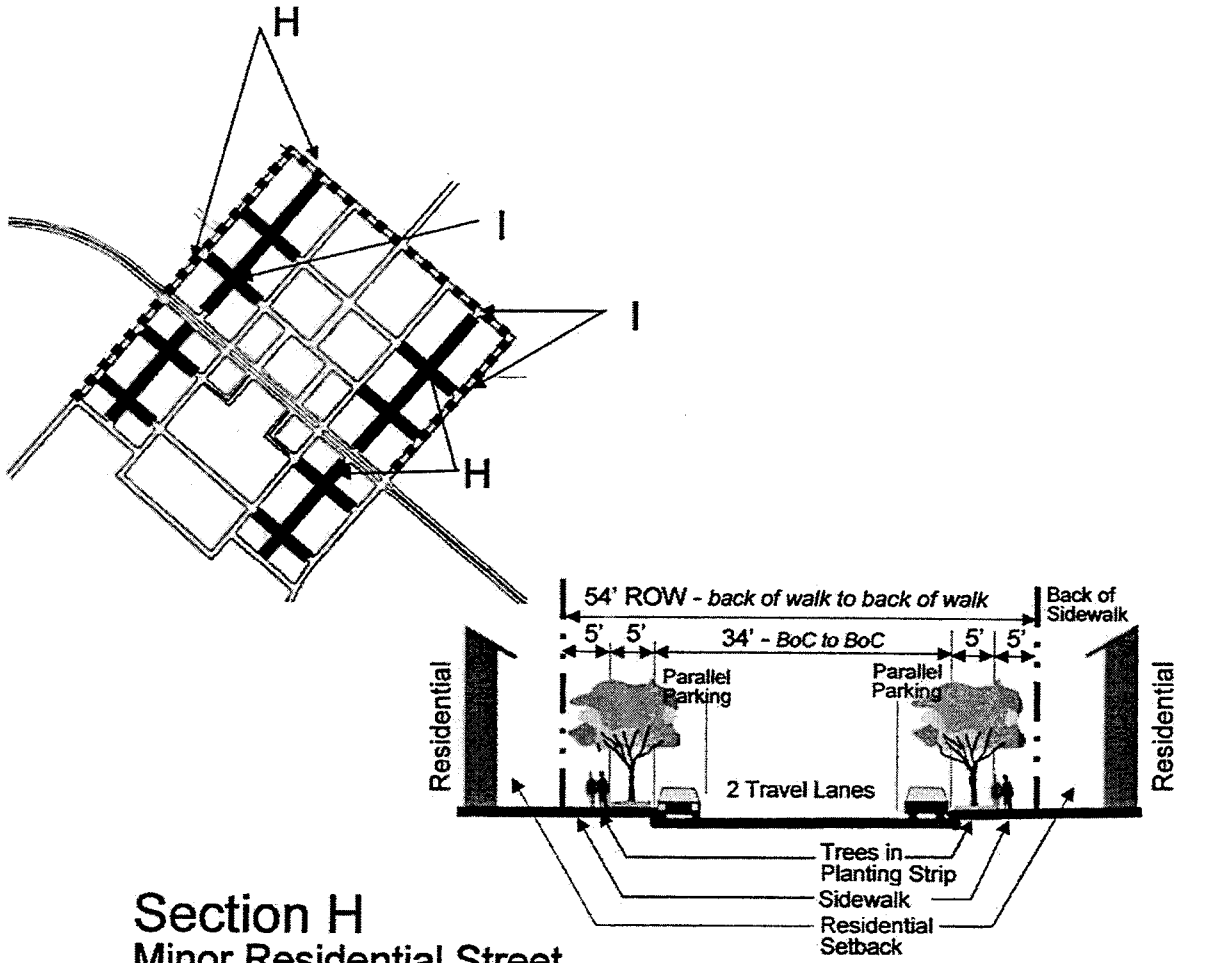


Section F
Major Residential Street

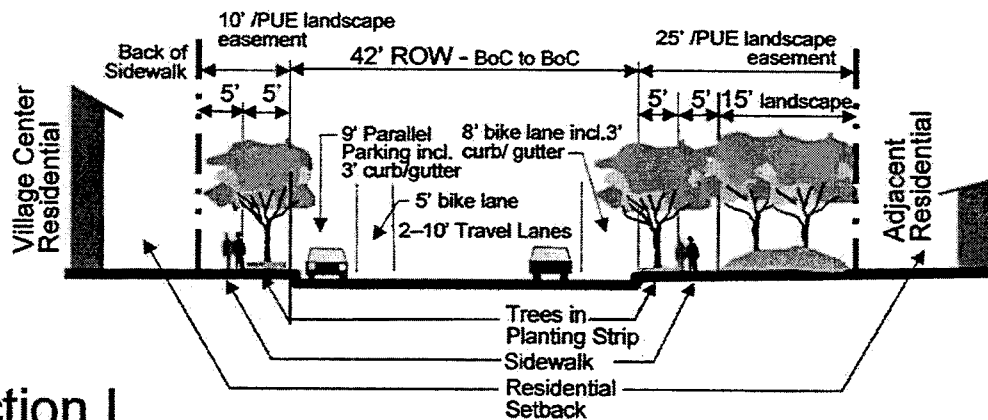


Section G
Residential Entry Street

Figure 10-22 Village Center Minor Residential and Perimeter Street Sections



Section H
Minor Residential Street



Section I
Modified Collector Perimeter Street

Key components of the Village Center roadway system include:

Pleasant Grove Boulevard (Figure 10-18) is a 4-lane arterial that provides access through the WRSP connecting to other portions of the City. This roadway passes through and provides a high degree of exposure to the Village Center. As Pleasant Grove Boulevard enters the Village Center there is a noticeable change in the streetscape environment. The landscape median more than doubles in width and typical suburban landscape corridors and soundwalls are replaced by planter strips and front-forward building architecture.

- Gateways and Enhanced Pedestrian Crossing – Gateways are provided at both ends of the Village Center along Pleasant Grove Boulevard. The intent is to create a sense of arrival and calm traffic flows. In addition, an enhanced pedestrian crossing will be provided in order to facilitate the safe and efficient crossing of pedestrians and cyclists across Pleasant Grove Boulevard between the north and south sides of the Village Center, and provide the bikeway link between Class I paths north and south of the Village Center. The enhanced pedestrian crossing will be located at the signalized intersection of Pleasant Grove Boulevard and Market Street, and shall incorporate the following elements:
 - Extra wide street crossing
 - Change of texture/color within crossing (stamped concrete)
 - Elevated design elements (such as planters or other elements) at the corners of the crossing
 - Use of bollards at corners and within median to announce crossing

Figures 10-23 and 10-24 provides an illustrative and plan view of the gateways and enhanced pedestrian crossing. Additional detail is included in the WRSP Design Guidelines (Section 12).

- Turning movements – Traffic signals are located along Pleasant Grove Boulevard at the east and west ends of the Village Center to control traffic and access through the area. Separate turn lanes shall be provided for all turning movements of all legs of the signalized intersections with the exception of separate right turn lanes which may be eliminated along Pleasant Grove Boulevard within the Village Center. Vehicle turning movements are allowed and pedestrian crossings are encouraged at intersections flanking the commercial and Village Green uses. Figure 10-25 provides the turning movement plan for Pleasant Grove Boulevard within the Village Center.

Figure 10-23 Village Center Gateway and Enhanced Pedestrian Crossing-Illustrative

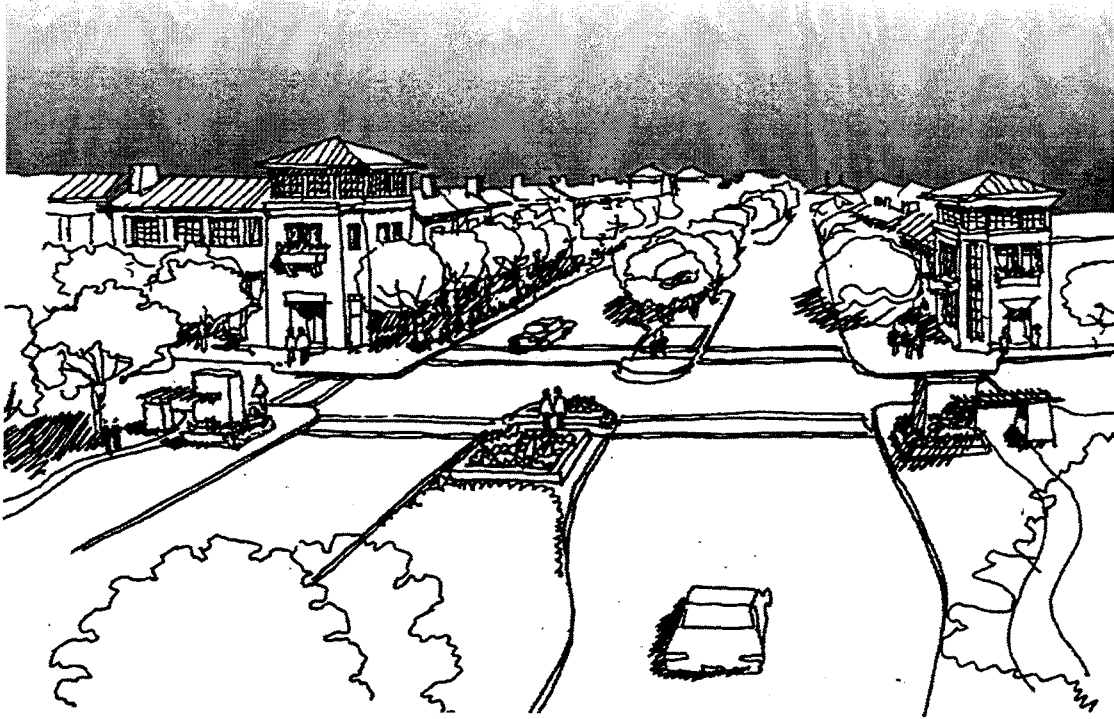


Figure 10-24: Village Center Gateway and Enhanced Pedestrian Crossing-Plan View

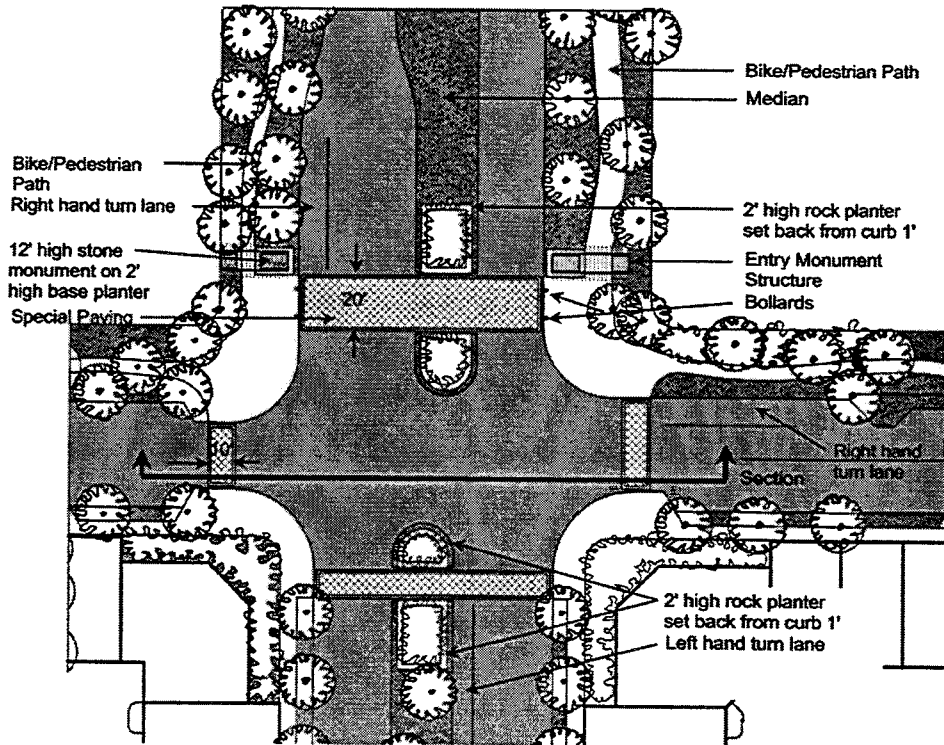
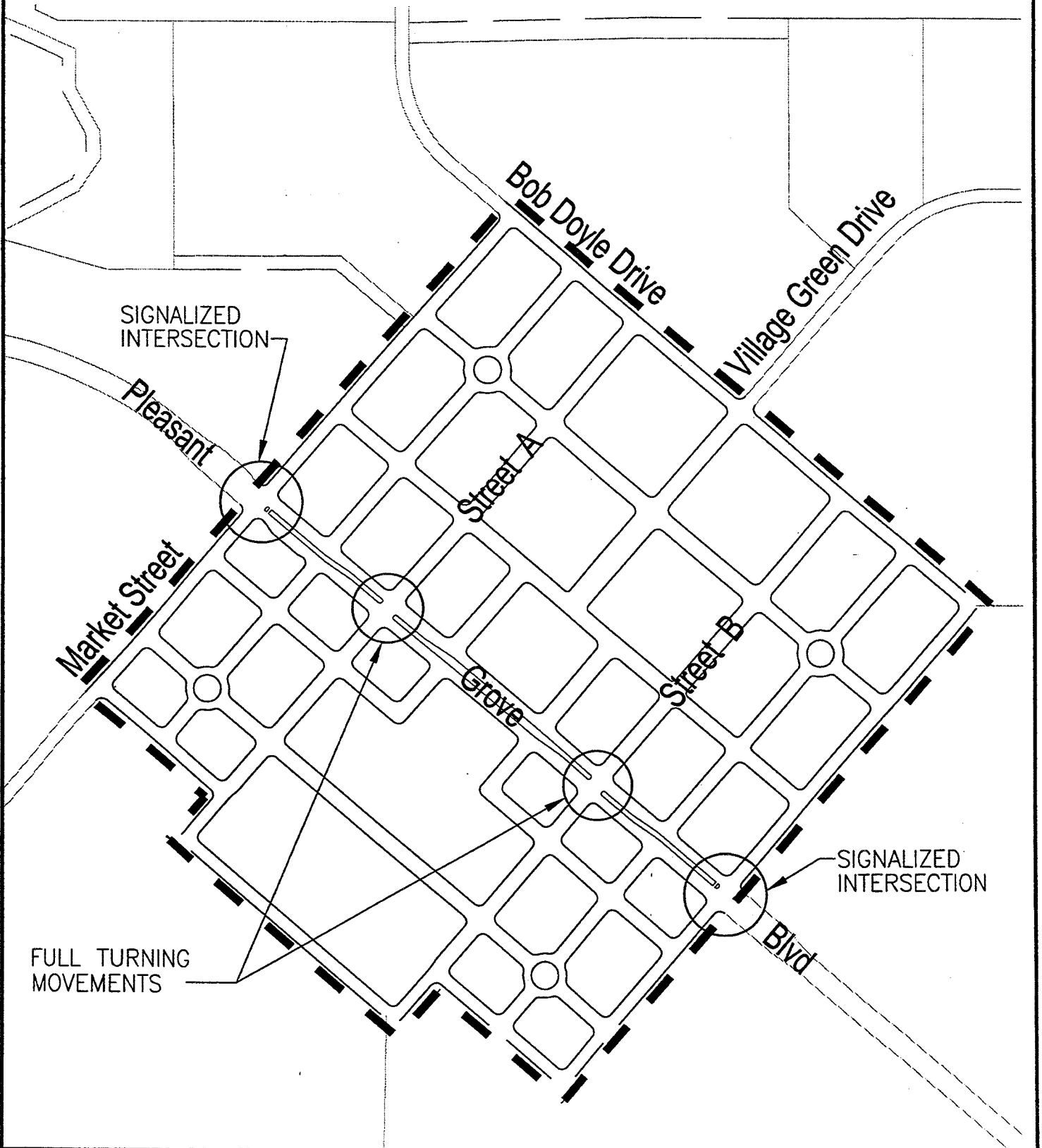


Figure 10-25

Pleasant Grove Boulevard Turning Movement Plan

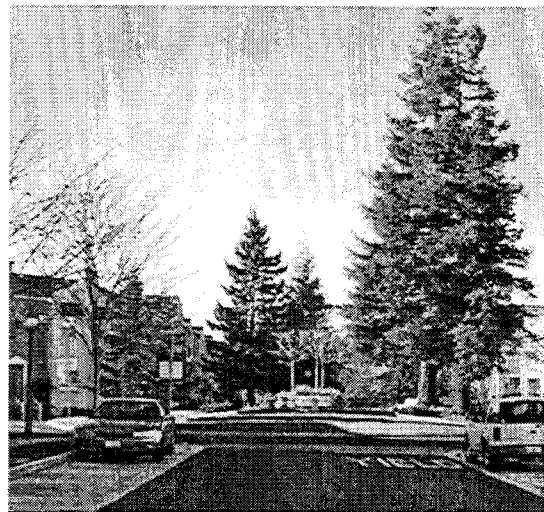


Commercial Roadways (Figures 10-19 and 10-20), specifically Village Green Drive, Village Green Drive East and Village Green Drive West are the focus of commercial and social activity within the Village Center. These 2-lane streets include wide sidewalks, significant tree plantings and other elements to enhance the pedestrian environment. Adjacent commercial development is oriented towards the street and sidewalks with restaurant seating and activities permitted to spill out into the public spaces. In addition, Village Green Drive and its east and west extensions include diagonal on-street parking. As a result of off-street parking areas being placed to the rear of commercial uses, curb cuts along Village Green Drive and its extensions are limited to minimize conflicts with pedestrian traffic and allow on-street parking to be optimized to supplement required off-street parking.



Residential Roadways (Figures 10-21 and 10-22) are 2-lane streets that provide access to Village Center housing. Residential streets includes 5-foot to 6-foot wide planter strips between the curb and sidewalk for tree plantings and other landscaping. Housing faces onto the streets and is encouraged to include entries, porches and other features promoting activity. Alley access may be provided to Medium Density Residential (MDR) small-lot parcels to allow for rear loaded garages and services. Direct auto access from street to garage is prohibited along key Village Center roadways to minimize disruption to on-street parking opportunities (see Figure 10-6).

Four small privately maintained circular greens or roundabouts are located within the intersections of residential streets in the four quadrants of the Village Center (see Figure 10-17 for locations). The circular greens are landscaped elements that interrupt the visual linearity of the roadway system, provide points of orientation, and add to the green landscape quality of the Village Center. Each circular green will be landscaped to assist in orienting pedestrians and motorists within the Village Center. Circular greens are part of the landscaped right-of-way.



Circular Green

INTERFACE WITH ADJACENT DEVELOPMENT

Development on the perimeter of the Village Center consists primarily of Low Density Residential (LDR) uses. These uses are encouraged to front onto the Village Center. Where uses do not front onto the Village Center, typical 25-foot wide landscape corridors will be provided. To ensure connection between uses, adjacent development shall provide either direct vehicular or pedestrian connections to Village Center roadways. Pedestrian connections shall consist of an open cul-de-sac with a landscaped pedestrian path and crosswalks. See Village Center/Adjacent Development Interface, Figure 10-26, and WRSP Design Guidelines (Section 12) for additional detail.

PEDESTRIAN AND BIKEWAY NETWORK

As previously noted, the Village Center has been designated as a Pedestrian District. It is intended to be a pedestrian friendly-walkable community that minimizes barriers, emphasizes the pedestrian over the automobile, and implements the City's General Plan pedestrian level of service policy. Numerous elements are included to achieve this intent from the basic form and mix of uses, grid circulation pattern, wide sidewalks, dense tree canopy, pedestrian scale lighting and required connections from adjacent uses.

The Village Center will link to the WRSP and City-wide bikeway system through Class II and III bikeways along connecting roadways. In addition, a paseo (Parcel W-85) with a Class I bikeway terminates at the western edge of the Village Center. This Paseo links to the larger WRSP open space and Class I bike trail system (see Circulation Plan, Section 7).

PUBLIC TRANSIT

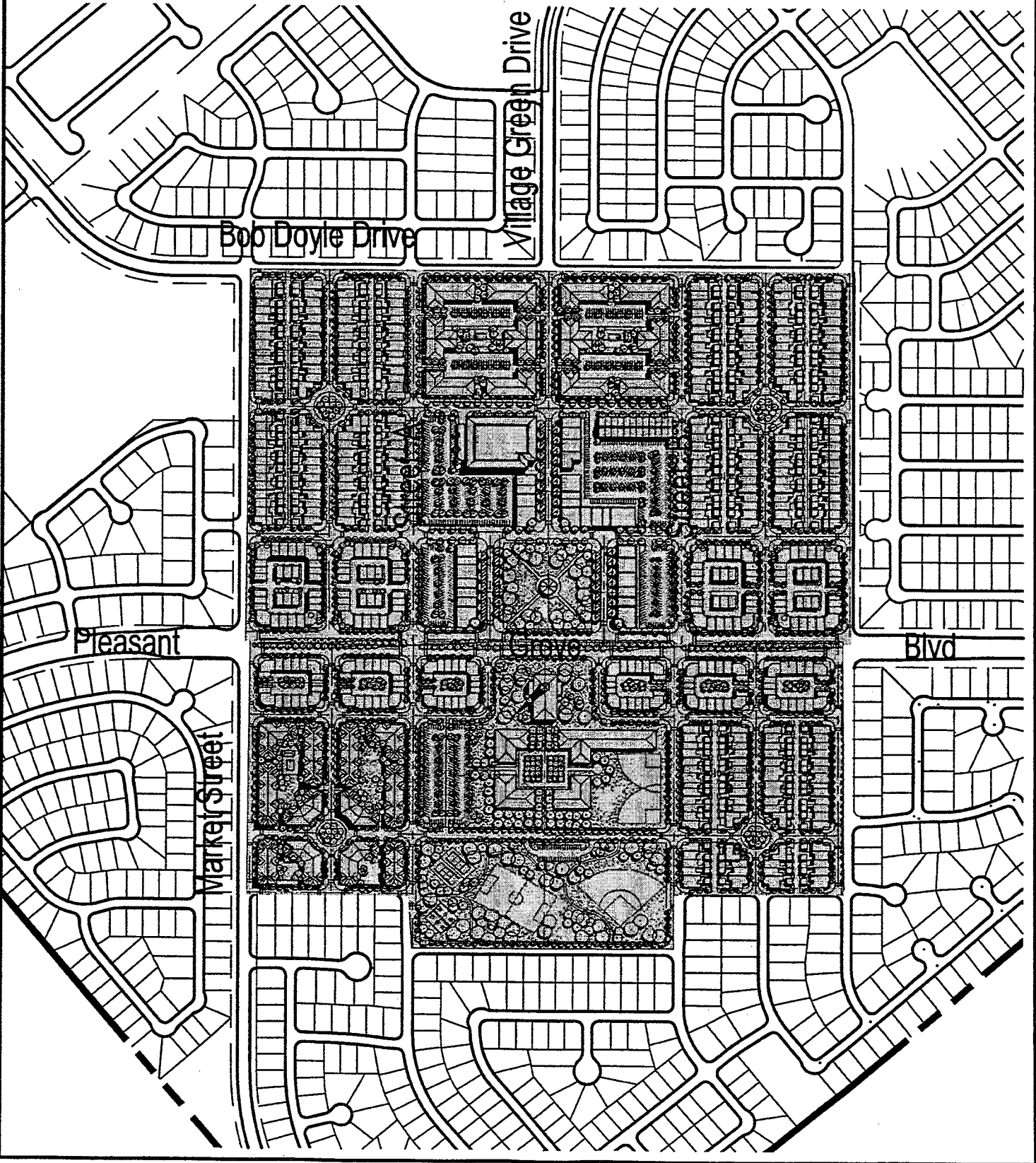
The Village Center includes residential densities considerably higher than a typical suburban setting, averaging approximately 12 dwelling units per acre. This higher density will help to support the use of transit as an alternative to the automobile. Transit stops, with enhanced shelters, are located on either side of Pleasant Grove Boulevard adjacent to the Village Green. Transit service will be provided to the WRSP as demand warrants and funds are available (see Circulation Plan, Section 7).

10.6 PUBLIC SERVICES PLAN

Public Services in the Village Center include those services provided to the rest of the WRSP. All services will generally be provided as presented in Section 8 of the Specific Plan.

Figure 10-26

Village Center Plan Adjacent Development Interface



□ PARKS & RECREATION

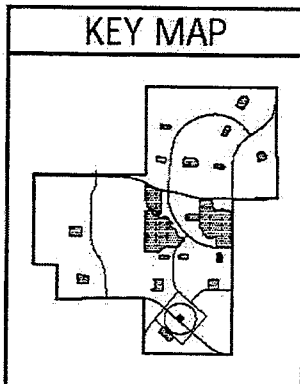
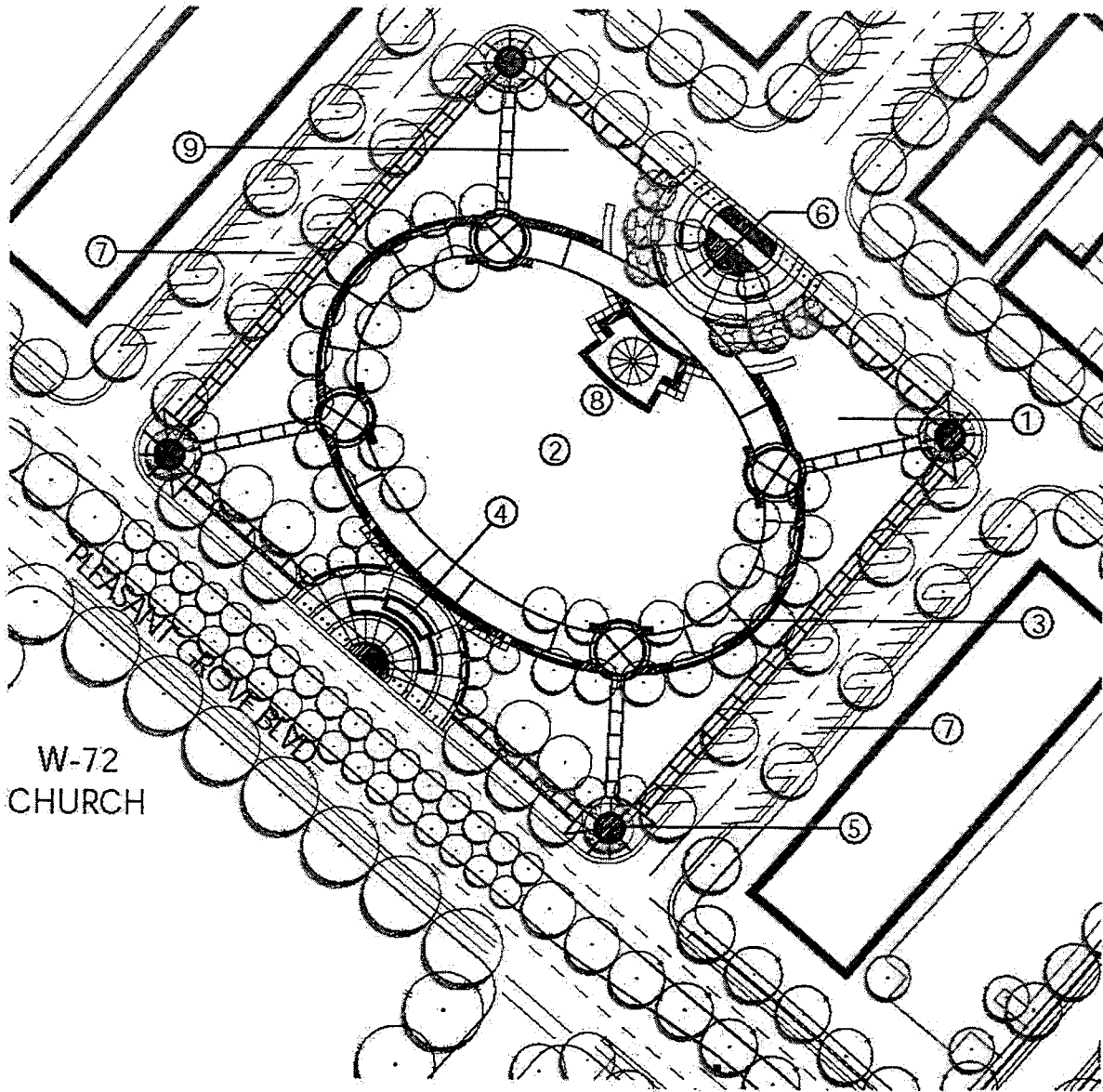
Distinct to the Village Center are two City park sites, the Village Green and a traditional neighborhood park. Totalling approximately 12.6 acres net and 13.6 acres gross, the park facilities provide for both social and recreational opportunities and help define the character of the Village Center.

Village Green (Parcel W-54) is a unique 3.5-acre (net) urban square forming the visual focus of the Village Center. The Village Green consists of grass with a symmetrical set of centrally oriented walkways. In the center is a pavilion that functions as a venue for musical concerts and community events. Large trees are located along the perimeter to provide shade and define the central community space. The Village Green is designated to provide passive recreation and as a gathering place for the community. Besides its passive recreation value and visual presence, the Village Green is designed to accommodate a variety of community activities such as a farmer's market, arts and crafts shows, celebrations, and performance arts that will attract the community to the Village Center. The Village Green will be owned and operated by the City of Roseville. A conceptual plan for the Village Green is included as Figure 10-27.

Neighborhood Park (Parcel W-53) is a 9.1-acre (net) active recreation facility. The park will include soccer and baseball fields, hard courts, a playground, picnic area and parking lot. The neighborhood park will serve the recreational needs of the Village Center and adjacent residential area. This facility will be dedicated to and maintained by the City of Roseville. A conceptual plan for the Village Center Neighborhood Park is included as Figure 10-28.



Figure 10-27: Village Green Conceptual Plan



PARK W-54 AMENITIES	
3.5 ACRE PARK	
1.	INDEPENDENT PLAY STRUCTURE
2.	VILLAGE GREEN
3.	CENTRAL WALKWAY CONCOURSE
4.	CIVIC PLAZA WITH FOUNTAIN
5.	ENTRY PLAZA AT CORNERS
6.	CIVIC PLAZA
7.	DIAGONAL VILLAGE PARKING
8.	CENTRAL STRUCTURE/ STAGE
9.	INTERACTIVE WATER PLAY FEATURE

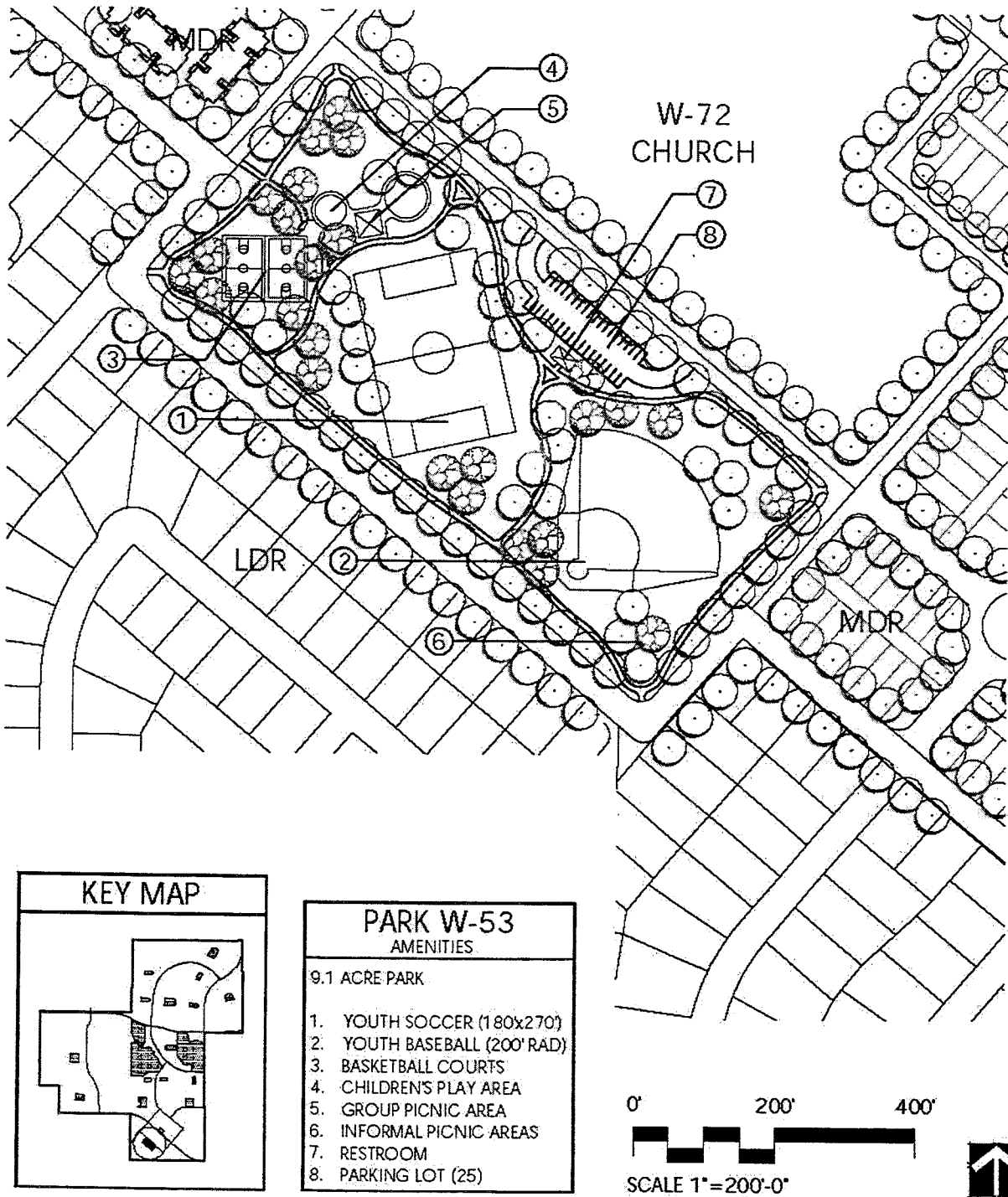
0' 100' 200'



SCALE 1"=100'-0"



Figure 10-28: Village Center Neighborhood Park Conceptual Plan



10.7 UTILITIES PLAN

Utilities provided to the Village Center include water, wastewater, recycled water, drainage and flood control, electric service, natural gas, communications and solid waste disposal. All services will generally be provided as presented in Section 9 of the Specific Plan.

UTILITY EASEMENTS

MDR parcels within the Village Center may include public alley access. Utility easements may be located within either the adjacent residential streets or alleys.

10.8 IMPLEMENTATION & ADMINISTRATION

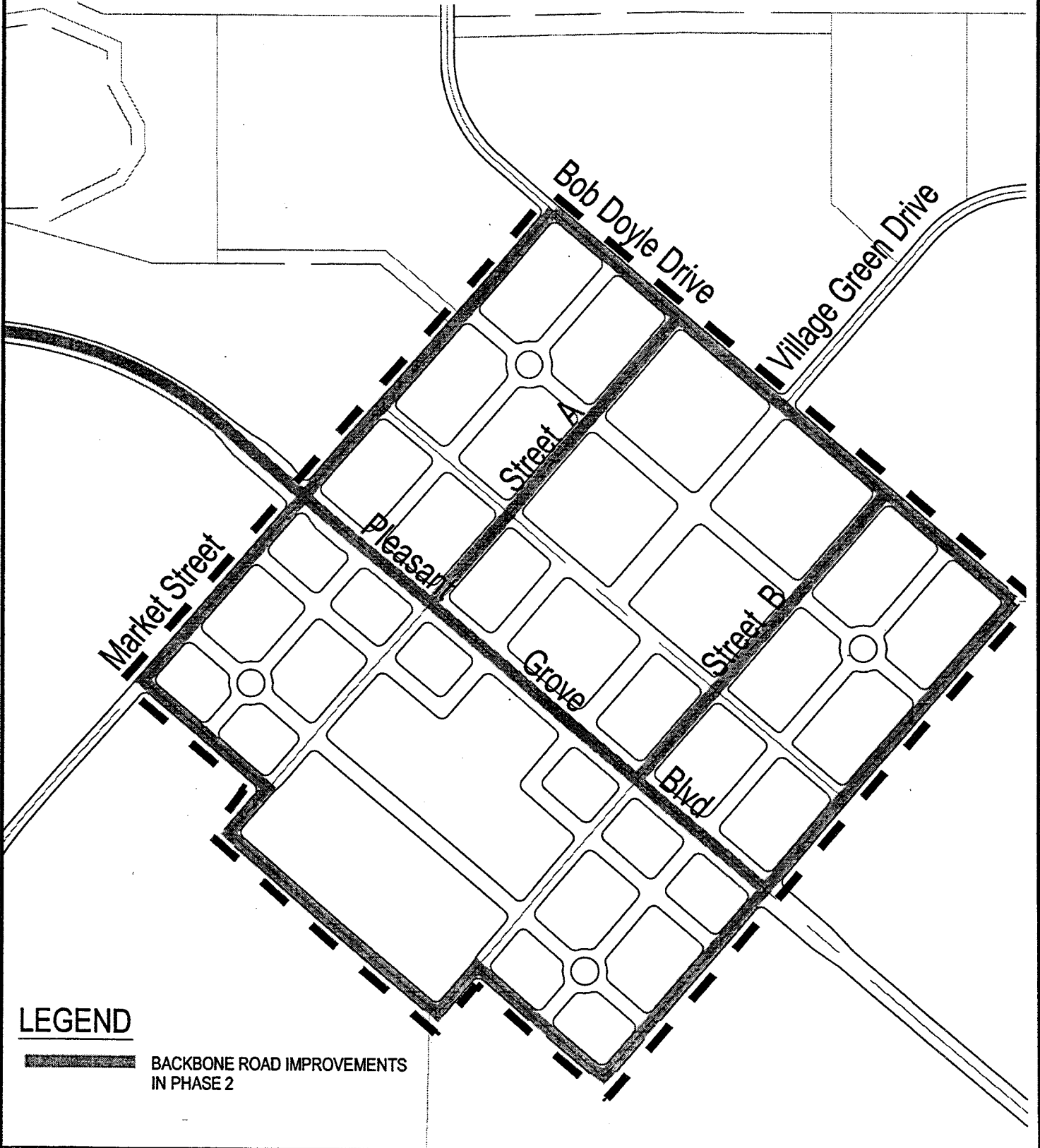
The Village Center will be subject to the Implementation and Administration provisions presented in Section 11 of the Specific Plan.

The Village Center is within Phase 2 of the WRSP. To help facilitate its development, Phase 2 supplies the backbone roadway infrastructure necessary to service each large-lot Village Center parcel. Phase 2 backbone road improvements within the Village Center are reflected on Figure 10-29. Given these Phase 2 improvements, no additional internal phasing of the Village Center is required.

Development of the Village Center will necessitate the construction of additional parcel specific improvements. These parcel specific improvements are detailed in the Westpark development agreement.

Figure 10-29

Village Center Roadway Improvements In Phase 2



11 IMPLEMENTATION & ADMINISTRATION

11.1 OVERVIEW

Implementation of the West Roseville Specific Plan includes phasing of development, financing and construction of improvements and review of individual development projects. The City of Roseville is the public agency responsible for administration of the Specific Plan and related documents. The WRSP is to be implemented consistent with all City rules, regulations and policies.

11.2 PHASING

The West Roseville Specific Plan provides for a comprehensively planned infrastructure system with coordinated phasing and construction of facilities. A total of four (4) phases are proposed in the WRSP. The geographic boundaries of each phase are reflected on Figure 11-1, with land use by phase summarized in Table 11-1. In general, the phasing plan has been structured to ensure that the improvements in each phase can support associated development in compliance with City policies and standards, and that the development in each phase can support the costs of the required improvements.

The infrastructure requirements for each phase of development include all on-site backbone infrastructure and off-site facilities necessary for each phase to proceed. Included are roadway, sewer, water, recycled water, storm drainage, dry utility, recreation, school and other facilities and improvements. Development will occur by phase in sequential order. The opportunity exists for certain parcels to move forward out of sequence, subject to review and approval of the City. Once development is initiated, some phases may have reduced infrastructure requirements if improvements are provided in an earlier developed phase.

A full listing of improvements, and specific details relating to those improvements, are included in the Specific Plan development agreements. All in-tract sewer, storm drain, water and dry utilities will be installed as part of local project improvements.

Table 11-1: Land Use by Phase

Land Use	Use	Phase 1		Phase 2		Phase 3		Phase 4	
		Acres	DU	Acres	DU	Acres	DU	Acres	DU
LDR	Low Density Residential	307.5	1,213	413.6	1,479	484.4	1,565	149.1	585
LDR	LDR – Age Restricted	146.9	710						
MDR	Medium Density Residential	17.3	174	37.0	185	20.6	160	21.9	165
HDR	High Density Residential	52.2	710	11.2	198	14.9	286	9.0	175
VC	Village Center			107.4	825				
CC	Community Commercial	15.4		9.3		5.4		4.0	
BP	Business Professional	9.1				10.5			
LI	Light Industrial							74.2	
IND	General Industrial							34.3	
OS	Open Space	40.5		135.0		227.6		267.0	
OS	Paseo	4.7		1.1		8.7			
P/R	Parks & Recreation	63.8		21.6		151.6		15.0	
P/R	Pocket Park	7.2		6.0		6.2			
P/QP	Public/Quasi-Public	72.8		28.0		6.2		30.1	
TOTAL		737.4	2,807	770.2	2,687	936.1	2,011	604.6	925

11.3**FINANCING OF PUBLIC IMPROVEMENTS**

The construction of public improvements to serve the WRSP will be funded by a variety of mechanisms including City Impact Fees, School Impact Fees, establishment of a Community Facilities District, developer financing and other potential methods.

City Impact Fees – The City of Roseville has adopted a set of development impact fees to finance capital improvements. The fee structure requires the payment of fees prior to issuance of a building permit. The City collects citywide and neighborhood park fees, drainage fees, sewer fees, water connection fees, traffic mitigation fees, public facilities fees and the Fire Service Construction Tax.

School Impact Fees – The various school districts have established fees, in accordance with state regulations, to be used to construct school facilities. School impact fees are collected by the City prior to issuance of a building permit, and are forwarded to the applicable school districts.

Community Facilities District – A Community Facilities District (CFD) will be established to help fund the construction and/or acquisition of backbone infrastructure and facilities within the WRSP. The 1982 Mello-Roos Community Facilities Act enables cities and other entities to establish a CFD to fund various facilities and services. The proceeds of the Mello-Roos special tax can be used for direct funding of facilities and/or to pay off bonds. A separate Community Services District (CSD) will be established for maintenance of certain facilities that provide special benefit

to the WRSP. Such facilities will include landscape corridors and medians, open space areas, bikepaths, detention facilities, City parks, pocket parks and paseos.

Developer Financing - Direct developer/merchant builder financing may be used to contribute towards backbone improvements and facilities, shortfall financing, and for in-tract subdivision improvements.

As noted, other financing mechanisms may be utilized, including creation of private districts or associations to fund maintenance of certain facilities within the WRSP including those within the Village Center. Specific financing requirements, improvement obligations, fees, reimbursements, land and easement dedications and conveyances, maintenance, and other financing and improvement related obligations are detailed in the Specific Plan development agreements. The general financing mechanisms to be used in the WRSP are summarized on Table 11-2.

Table 11-2: Public Improvement Financing Mechanisms

Improvement/Facility	Financing Options
Road Improvements	CFD/Traffic Mitigation Fee/Developer Financing
Bike Trails	CFD/Developer Financing/Park Fees (reimbursement)
Fire Facilities	Fire Service Construction Tax
Schools	CFD/School Impact Fee/Developer Financing
Parks	CFD/Park Fees/Developer Financing
Pocket Parks	Developer Financing
Paseos	Developer Financing/Paseo Fee
Library	Public Facilities Fee
Water Improvements	CFD/Water Connection Fee
Sewer Improvements	CFD/Sewer Fee
Recycled Water Improvements	CFD
Drainage Improvements	CFD/Drainage Fee
Electric Facilities	CFD/Rates/Developer Financing
County Facilities	County-Wide Facilities Fee
Maintenance ¹	CSD

1. Landscaping, open space, trails, detention facilities, City and pocket park facilities.

11.4

SUBSEQUENT ENTITLEMENTS

Processing

Individual development projects within the WRSP are subject to review and approval of subsequent permits and entitlements by the City of Roseville. Application and processing requirements shall be in accordance with the City's Zoning Ordinance and other regulations, unless otherwise modified by this Specific Plan. All subsequent development projects, public improvements and other activities shall be consistent with this Specific Plan and accompanying

Design Guidelines, the Specific Plan development agreements, and all applicable City of Roseville policies, requirements and standards. In acting to approve a subsequent project or permit, the City may impose conditions as are reasonably necessary to ensure that the project is in compliance with the Specific Plan and all applicable plans and regulations. Appropriate Local Agency Formation Commission (LAFCO), state and federal approvals and permits are required prior to any development activity.

Environmental Review

Each subsequent development project shall be reviewed to ensure compliance with the California Environmental Quality Act (CEQA). The *Environmental Impact Report for the West Roseville Specific Plan and Sphere of Influence Amendment* (EIR) certified concurrent with the WRSP, serves as the base environmental document for subsequent entitlements within the Plan Area. Development applications will be reviewed on a project-by-project basis to determine consistency with the EIR.

In general, if it is determined that a subsequent project is consistent with the Specific Plan and within the scope of the EIR, no further environmental review may be necessary. Section 65457(a) of the California Government Code and Section 15182(a) of CEQA provide that no EIR or negative declaration is required for any residential project undertaken in conformity with an adopted Specific Plan for which an EIR has been certified. If it is determined that a development application is inconsistent with the Specific Plan and/or substantial evidence exists that supports the occurrence of any of the events set forth in CEQA Guidelines Section 15183, a determination will be made as to the appropriate subsequent environmental document.

A mitigation monitoring program has been adopted with the EIR in accordance with Public Resources Code 21081.8 to help ensure implementation of EIR mitigation measures.

11.5

SPECIFIC PLAN AMENDMENTS AND MINOR REVISIONS

Specific Plan Amendments

Proposed changes to a specific plan typically require approval of a Specific Plan Amendment (SPA). Specific Plan Amendments are processed in the same manner as the initial Specific Plan adoption, requiring review by the Planning Commission and action by the City Council.

Minor Revisions

It is anticipated that the West Roseville Specific Plan may need to respond to changing conditions and expectations during the course of its implementation. To address this intent, the WRSP provides for Minor Revisions to the Specific Plan in addition to typical Specific Plan Amendments. The Planning Director shall determine whether a proposed revision is minor, and may act upon a minor revision to the Specific Plan administratively.

A minor revision to the Specific Plan may be processed if determined by the Planning Director to be in substantial conformance with:

1. The Planning Principles and overall intent of the West Roseville Specific Plan;
2. The applicable Specific Plan development agreement(s);
3. The City of Roseville General Plan; and,
4. The Specific Plan Environmental Impact Report.

Examples of minor revisions to the Specific Plan include, but are not limited to:

- The addition of new or updated information that does not substantively change the Specific Plan.
- Minor adjustments to land use boundaries and street alignments where the general land use pattern is maintained.
- Changes to the provision of public infrastructure and facilities that do not impact the level of service provided or affect the development capacity in the Plan Area.
- Modifications to the Design Guidelines, such as revisions to design treatments or changes in specified plant materials, if it is determined that such changes achieve the design intent to the same or better level.

Any proposed minor revision to the Specific Plan may, at the sole discretion of the Planning Director, be referred to the Planning Commission and City Council for action. Determinations and actions by the Planning Director may be appealed to the Planning Commission.

If the Planning Director determines that a proposed amendment does not meet the above criteria, a Specific Plan Amendment (SPA) shall be required.

12 DESIGN GUIDELINES

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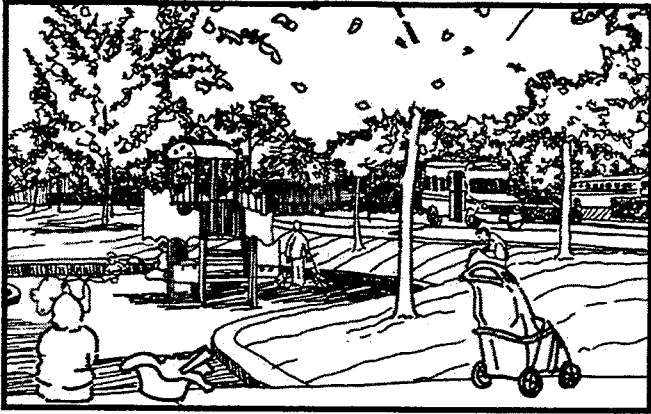
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12.1 Introduction

12.1.1 Purpose

The West Roseville Specific Plan (WRSP) Design Guidelines is a separate document adopted as an Appendix to the West Roseville Specific Plan. The intent is to describe the design requirements for the WRSP in a single document.

The guidelines provide detailed performance criteria and standards to be considered by City staff, Design Review Committee, Planning Commission, and City Council in the review of individual developments within the Plan Area.

The Guidelines address:

- Elements that are common throughout the Plan area and should be applied uniformly;
- Landscape and entry requirements;
- Buffers and adjacency issues;
- Conditions that are unique to the Plan area;
- Details that define the character of the Specific Plan, not found in the City-wide documents; and
- Specific parcel considerations.

The guidelines are intended to encourage creativity in solutions to specific design opportunities. However, in order to meet the overall objectives of the Plan, certain standards must be fulfilled. Where the provisions of the WRSP Design Guidelines are more restrictive than the Community Design Guidelines and/or the Zoning Ordinance, the WRSP Design Guidelines shall govern development within the Plan Area.

These guidelines address design considerations in the WRSP Plan Area, however in some situations, the design approach for the Westpark and Fiddyment Ranch properties differ. In those situations, the specific design approach for Fiddyment Ranch and Westpark is described. Figure XX in the Specific Plan identifies the areas within the Fiddyment Ranch and Westpark property boundaries.

12.1.2 Relationship to Other City Documents

Other standards and guidelines applicable to the West Roseville Specific plan area are set forth in the following documents, which should be referenced in the design of all uses in the Plan Area:

- Roseville Municipal Code-Title 19, Zoning Ordinance.
- Community Design Guidelines.
- Roseville Sign Ordinance.

In all cases, the WRSP shall govern except where silent, in which case City regulations shall apply.

12.1.3 Neighborhood Character and Overall Design Intent

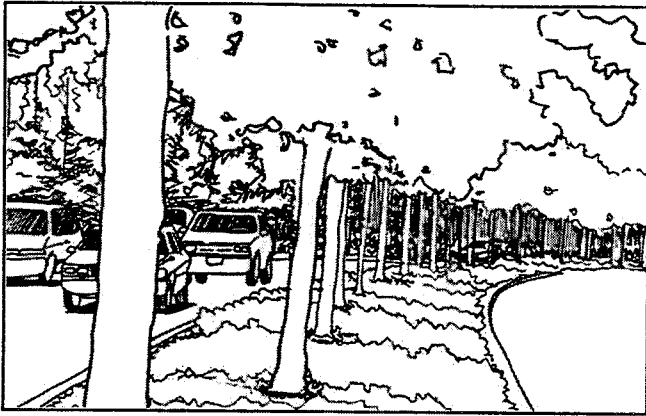
The design theme of the WRSP is centers around the recreational amenities provided in the Plan Area (Citywide parks and Class I bikeway system) and its historic land uses.

- Architecture that incorporates Ranch, Nature, Craftsman, Bungalow and other traditional styles unique to the early history of the California region.
- Community design elements that include large boulders, natural stone-like materials and textures, earthen colors and warm hues of grasslands, which are common to the region and create a unique identity for the Plan Area.
- The landscape design theme is organized around the application of native wildlife oak woodland and orchard design elements, .

12.1.4 Administration

The Design Guidelines are intended to provide added detail for several elements within the Plan, and therefore, the requirements contained herein will be required of all projects as the Plan Area builds out. The City recognizes the need for flexibility in the implementation of the Guidelines if special conditions warrant. To this end, the following administrative process shall be followed to review and approve deviation requests to the design requirements of this document (assumes no Specific Plan changes are proposed).

- Developer must submit formal request to the Planning Director with proposed deviations to design requirements in this document.
- Proposal shall specify: a) what deviations are requested, b) the proposed design for each deviation, and c) why the resulting design warrants such deviation(s).
- The Planning Director will determine whether the requested deviations are consistent with the spirit and intent of the design guidelines, and if so, may approve the deviations administratively.
- If approved, the developer shall be responsible for preparing change-pages for the Design Guidelines document.



12.2 Common Elements

This section addresses items where Design Guidelines are to be applied uniformly to all aspects of the Plan Area. The intent is that as the Plan Area builds out, certain features are designed consistently (i.e. streetscape) to ensure that a visually cohesive community is created.

12.2.1 Street Landscaping

The streetscapes in the WRSP will be consistent with the high-quality landscape design concepts and elements in the existing specific plan areas adjacent to the WRSP. Using similar planting concepts, materials, and sidewalk configurations, this will ensure that the WRSP is well-integrated into the City's built environment.

12.2.1.1 Typical Landscape Corridor Design

All streets shall be landscaped with a combination of trees, shrubs, and groundcover, as illustrated in the attached roadway sections (Figures 12-2 to 12-10), consistent with the following standards:

Primary Street Trees are located closest to the roadway and provide each street with its scale and form. Primary street trees shall be:

- The dominant element on the streetscape.
- Consistent with the attached plant palette.
- Spaced 30-feet on center.
- Planted from a minimum 15-gallon container (except in Village Center where minimum 24"-box specimens are required).
- Planted in a regular linear fashion, set back from the curb far enough to accommodate ultimate growth (a minimum of 5-feet).
- Drought-tolerant when established.

Secondary Street Trees, where required, shall be used to add contrast and background to the linear plantings of primary street trees. Secondary trees can also be used to provide color and accents at neighborhood entries and at points of interest along the streetscape. Secondary trees shall be:

- Planted in informal fashion as determined by space and tree species (unless otherwise specified by a particular street section).
- Consistent with the attached plant palette.
- Distinctive in form and/or color.
- Complementary to the form of the primary street tree.
- Planted from a minimum 15-gallon container.
- Spaced an average of 30-feet on center, or in equivalent quantities if planted in informal fashion.

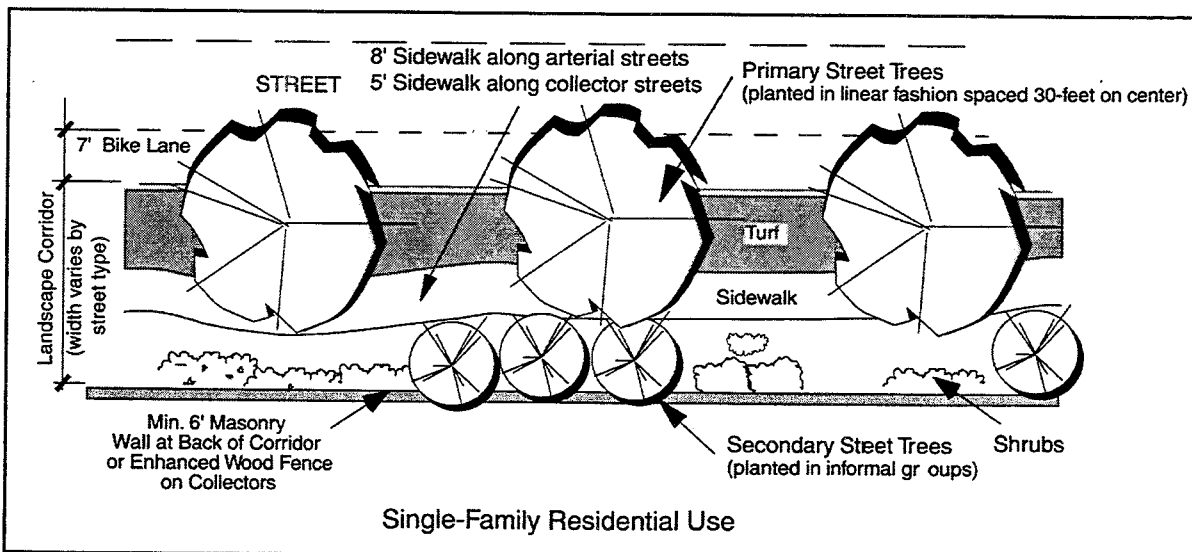


Figure 12-1 - Typical Landscape Corridor Design

Shrubs shall be used in landscape easements and medians to provide a visual barrier to fences, walls, and utility equipment, soften the ground plane, and visually link all landscape materials. Shrubs shall be:

- Planted from a minimum 1 to 5-gallon container.
- Selected according to size, color, texture, and seasonal interest.
- Consistent with the attached plant palette.
- Placed to not obstruct important pedestrian or vehicular sight lines or threaten the safety of pedestrians.
- Shall not conflict with utility screening.

Groundcover shall be planted in all portions of landscape easements and/or medians not planted with shrubs. Selection of plant material should consider the pedestrian use of a particular area. High-activity areas such as through parks and pedestrian corridors, should be planted with turf. Low-activity areas, such as along major streets, should use a combination of turf and foliage-type

groundcovers. Utilization of groundcover shall consider the following:

- Turf should be planted in parkway strips between the sidewalk and curb on arterial and collector streets.
- Non-turf groundcover (or a combination of turf and non-turf groundcover) is preferred behind the back of sidewalk.
- Turf shall not be planted in medians
- Where turf is planted via hydroseeding, those areas shall have strict weed-abatement measures implemented.
- Turf and groundcover areas shall be defined with concrete mow strips.
- Turf may be installed in areas with slopes of 3:1 or less. Non-turf groundcovers shall be installed on slopes steeper than 3:1.
- Drought-tolerant groundcover species, including turf that requires low-water usage, are encouraged.
- Mow strips shall be utilized at the edges of formal landscape areas, or where needed to delineate the limits of maintenance.

12.2.1.2 Arterial Roadway Landscape Corridors

Blue Oaks Boulevard

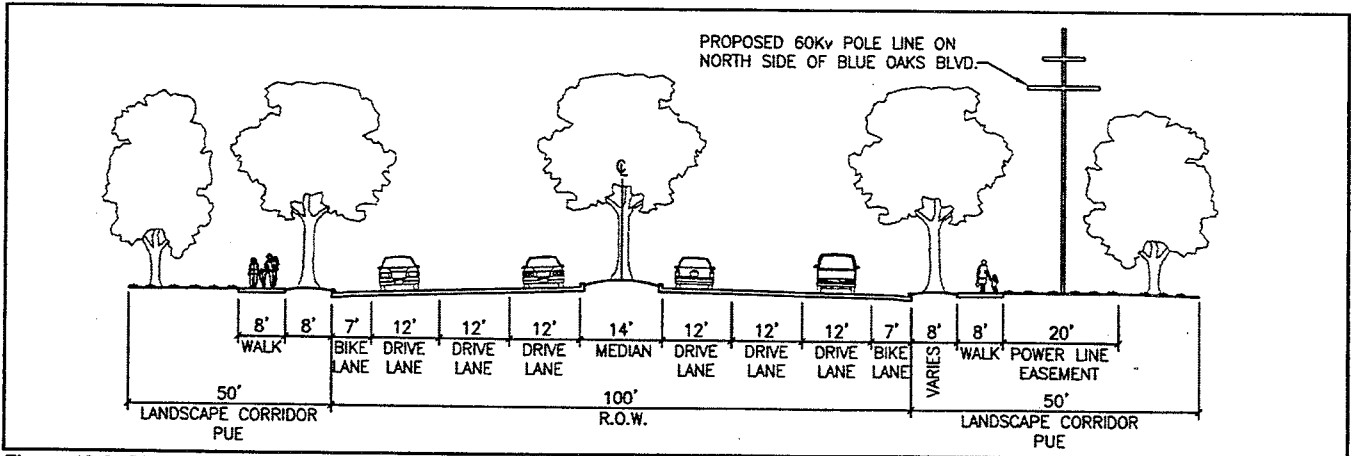


Figure 12-2: Blue Oaks Blvd.

West Side Drive

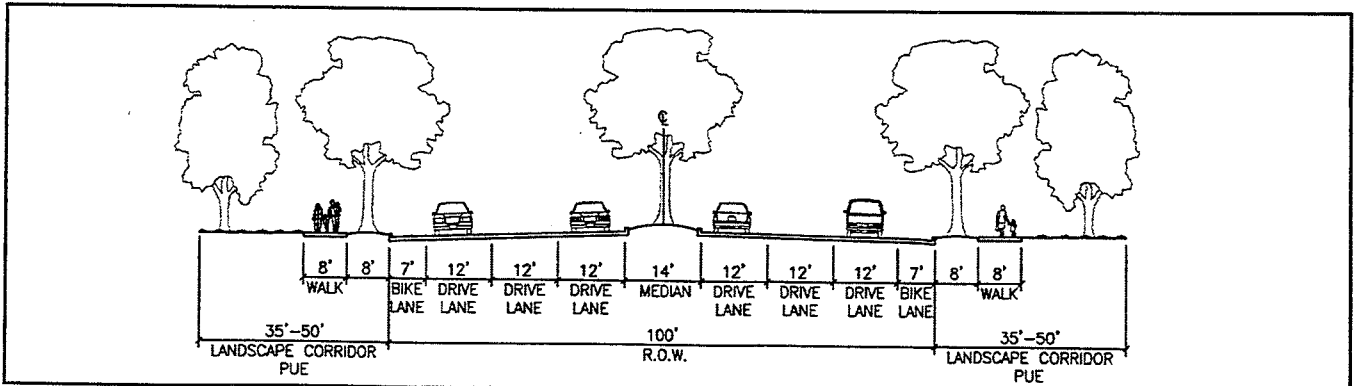


Figure 12-3: West Side Drive

12.2.1.2 Arterial Roadway Landscape Corridors (cont.)

Pleasant Grove Boulevard

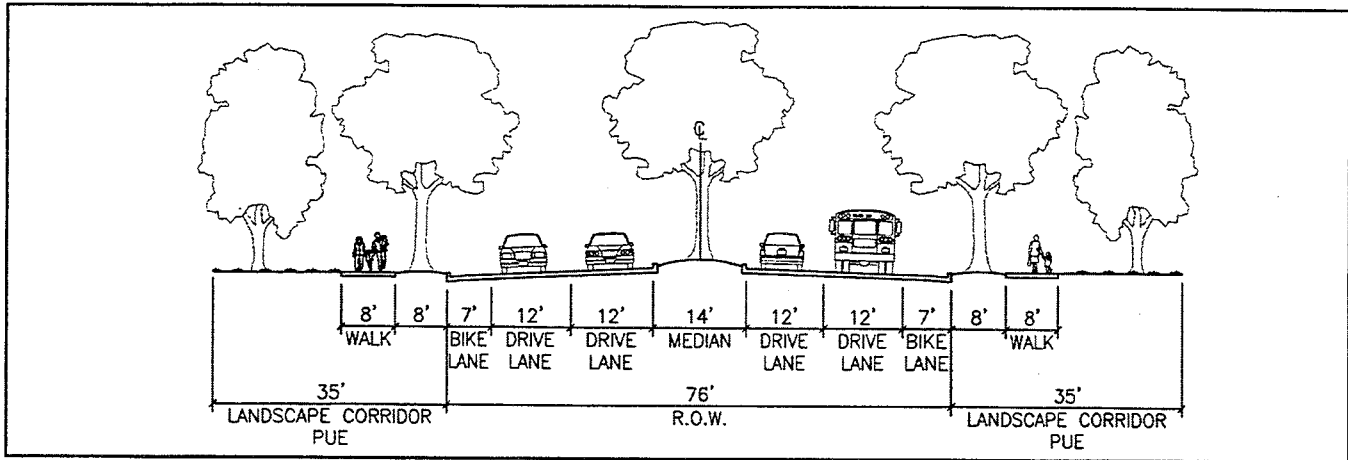


Figure 12-4: Pleasant Grove Blvd. Section

Note: Refer to the Village Center chapter of the Specific Plan document (Chapter 10) for specific requirements for Pleasant Grove Boulevard through the Village Center.

Fiddymont Road Landscape Corridor

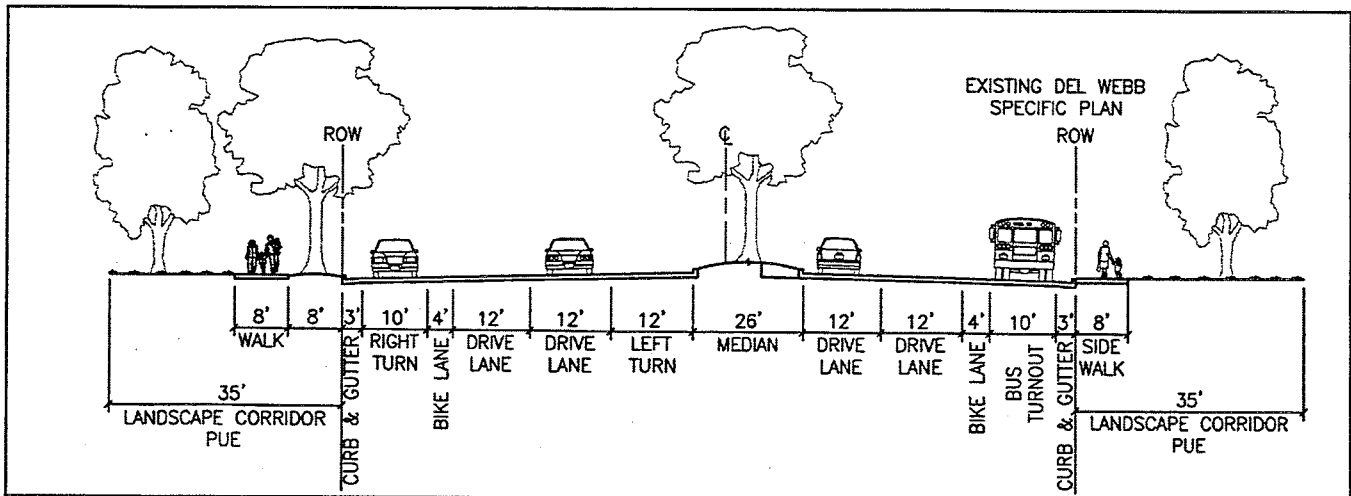


Figure 12-5: Fiddymont Road Section

12.2.1.3 Blue Oaks Blvd. Landscaping within Power Line Easement

In addition to standards for all landscape corridors noted above, the following additional standards shall apply in that portion of the landscape corridor that may be used for the powerline, as shown in Figure 12-2.

- Landscaping within the 20 foot power line easement is restricted to shrubs, groundcover, lawn and trees selected from Secondary Tree Group 2, Appendix 01.
- No permanent structures other than electric utilities may be placed upon this electric easement.
- Lighting structures and landscaping within the powerline easement should not exceed 15-feet above ground elevation, and should not be within

25-feet of the nearest high-voltage transmission line conductor.

- Berms should not be placed next to the base of powerline poles.
- Drip lines of all trees shall be maintained at a minimum of 6-feet.
- All grading, landscape structures (including lighting and fencing) and landscaping on a public-utility easement or near a public utility is subject to final approval by the City.
- The 8-foot wide sidewalk is allowed to meander within and adjacent to the existing 20-foot wide public-utility easement.

12.2.1.4 Collector Street Landscape Corridors

Typical Collector Street

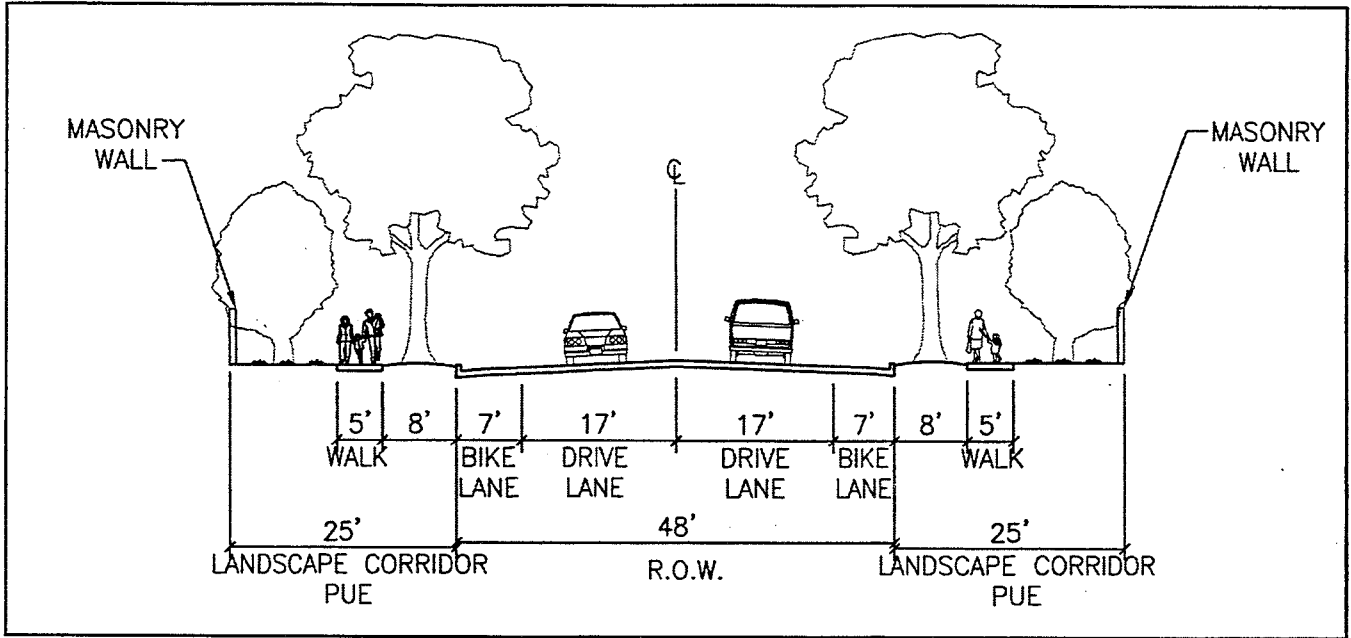


Figure 12-6: Typical Collector Street

Hayden Parkway (Modified Collector with landscaped median)

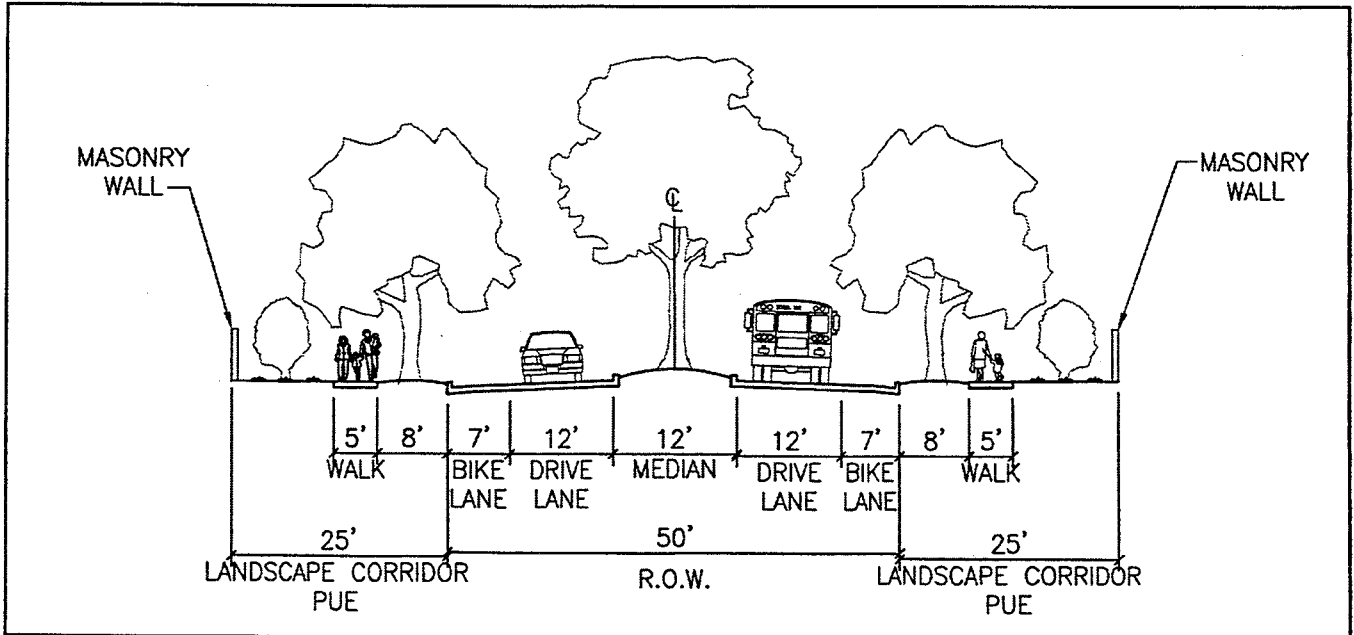


Figure 12-7 : Hayden Parkway

12.2.1.5 Phillip Road Landscape Corridor

Special landscape requirements are specified for Phillip Rd. due to the limited right-of-way available near the wastewater treatment plant. **Please refer to the Circulation section for more information.**

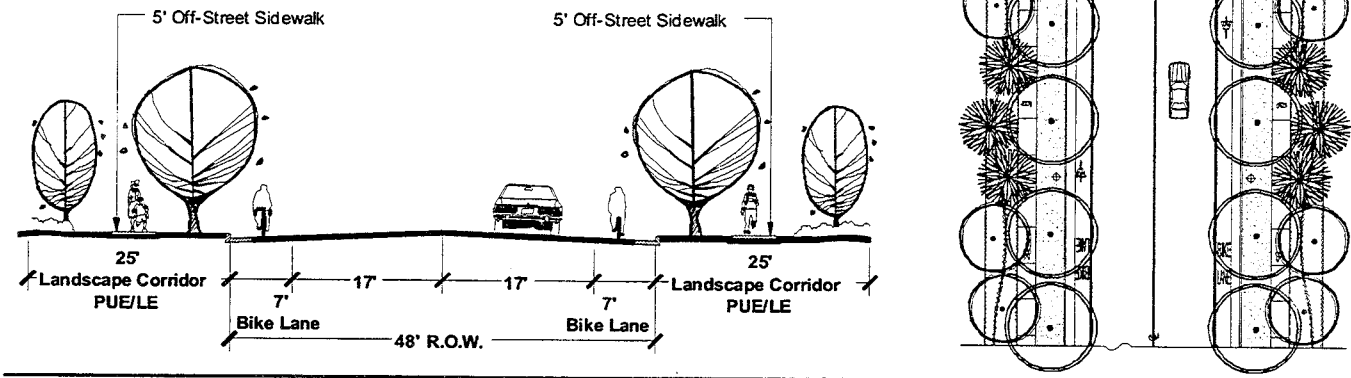


Figure 12-8: Phillip Road

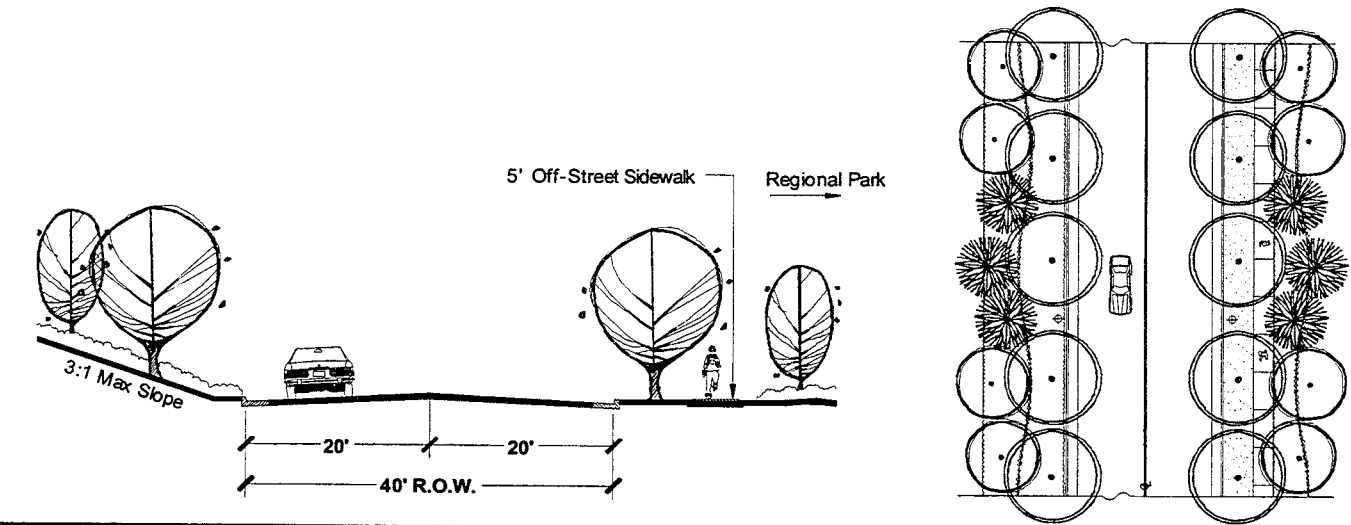


Figure 12-9: Phillip Road

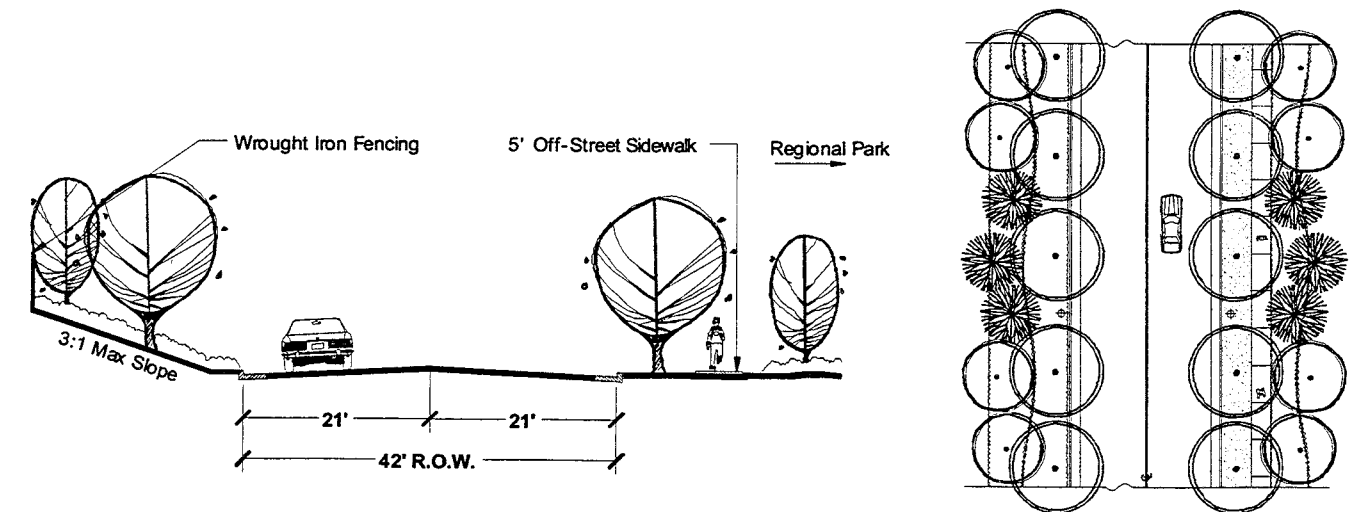


Figure 12-10: Phillip Road

12.2.1.6 Street Tree Master Plan

BOTANICAL NAME	COMMON NAME	BOTANICAL NAME	COMMON NAME
Primary Street Trees		Secondary Street Trees	
Acer rubrum 'Red Maple'	Red Sunset Maple	Cedrus deodara	Deodar Cedar
Celtis sinensis	Chinese Hackberry	Ginkgo biloba	Ginkgo Tree
Cinnamomum camphora	Camphor Tree	Oelreuteria paniculata	Goldenrain Tree
Fraxinus oxycarpa	Raywood Ash	Sequoia sempervirens	Coast Redwood
"Raywood"		'Soquel'	
Gleditsia tracanthos	Shademaster Honey Locust	Quidambar styracifula	American Sweet Gum
'Shademaster'		'Festival'	
Liriodendron tulipifera	Tulip Tree	Pinus canariensis	Canary Island Pine
Pinus sp.	Coulter, Italian, Stone, Eldarica, Nigra	Runus cerasifera 'Krauter Vesuvius'	Purple Leaf Plus
Pistacia chinensis	Chinese Pistache	Quercus agrifolia	Coast Live Oak
Platanus acerifolia	London Plane Tree	Quercus suber	Cork Oak
"Bloodgood"			
Platanus racemosa	California Sycamore	Quercus Virginiana	Southern Live Oak
Phoenix canariensis	Canary Island Palm (in selected areas only)	Pyrus calleryana "Aristocrat" & 'Bradford'	Aristocrat Pear & Bradford Pear, American Linden
Quercus ilex	Holly Oak		
Quercus lobata	Valley Oak	Small Accent Trees	
Quercus ruba	Red Oak	Acer palmatum	Japanese Maple (multi-trunk)
Sophora japonica	Japanese Pagoda Tree	Arbutus marina	Strawberry Tree
Tilia cordata 'Greenspire'	Little Leaf Linden	Eryobotria japonica	Loquat Tree
Ulmus parvifolia	Truegreen Evergreen Elm	Lagerstroemia indica Indian Tribe Varieties	Dwarf Crape Myrtle (multi-trunk)
Washingtonia robusta	Mexican Fan Palm (in selected areas only)	Magnolia stellata	Star Magnolia (multi trunk)
Zelkova serrata	Village Green Zelkova	Prunus caroliniana	Carolina Laurel Cherry
		Rhus lancea	African Sumac

12.2.1.7 Medians

Typical Arterial Median Design

Medians on arterial streets are typically 14-feet wide, except as otherwise specified in each street section. The following design elements shall guide the design of all landscaped medians:

- Median design shall include a primary street tree spaced 30-feet on-center, planted on the center line of the median;
- Accent trees may be used in combination with the primary tree to provide visual interest.
- A combination of groundcover and shrubs that provide seasonal flowers or leaf-color variation and texture shall also be incorporated into median landscaping.
- Groundcover and shrubs shall not exceed 30" in height, or as required by the Public Works Department to ensure that vehicular sight lines are maintained.
- Large expanses of hardscape material (such as pavers, cobble, etc.) shall not be permitted. These types of hardscape materials may be used in small quantities as accents to the landscaping.
- The use of lawn/turf in medians is prohibited.

Figure 12-11 illustrates the typical median landscape design to be utilized on arterial roadways.

Median Breaks

Median breaks on arterial and collector streets are limited to those shown on Figure 12-12. The purpose of controlling the number and location of these breaks, aside from controlling traffic movements, is to ensure that a strong, continuous street tree and landscape treatment can be provided along the streetscape. The following standards shall apply to all median breaks:

- Breaks along streets should be spaced to allow for the minimum stacking distance necessary in a vehicular turn pocket.
- Breaks should be spaced to provide a sufficient area for median landscaping and to prevent the creation of small islands that cannot have landscaping due to size constraints.
- Median design should avoid creating conditions where hardscape must be installed in lieu of landscaping due to site distance requirements.
- A minimum of 5 trees, spaced at maximum intervals of 30' on-center, shall be provided in any one section of median.
- Accent trees may be used in combination with the primary tree to provide visual interest.
- Medians should be no less than 210' long, excluding turn pockets.
- Additional median breaks will not be considered by the City due to constraints associated with meeting the standards outlined above.

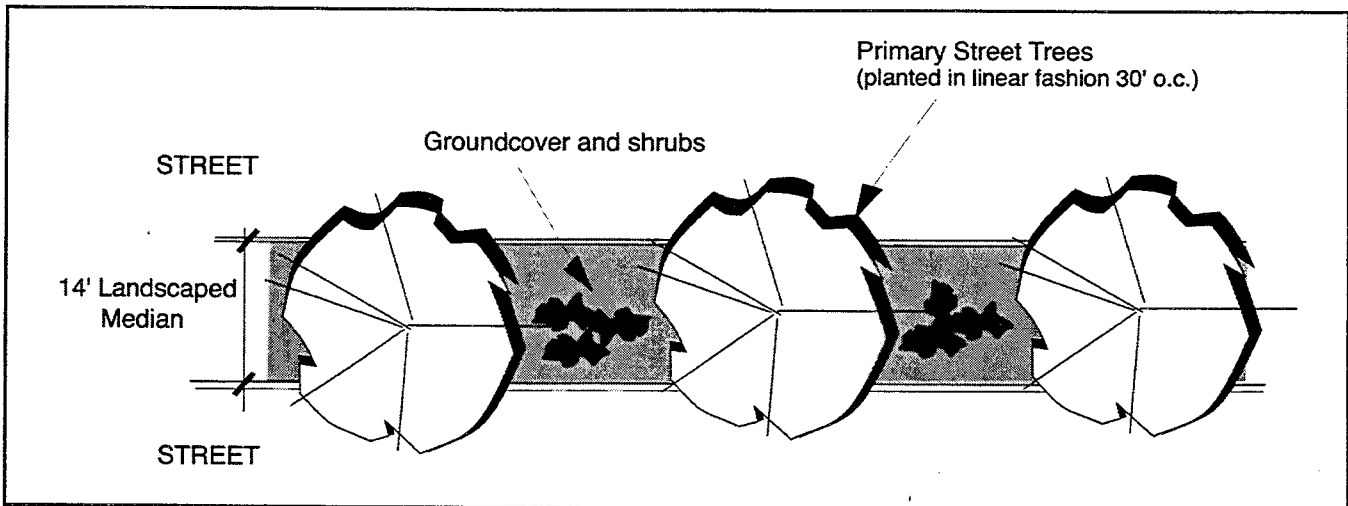
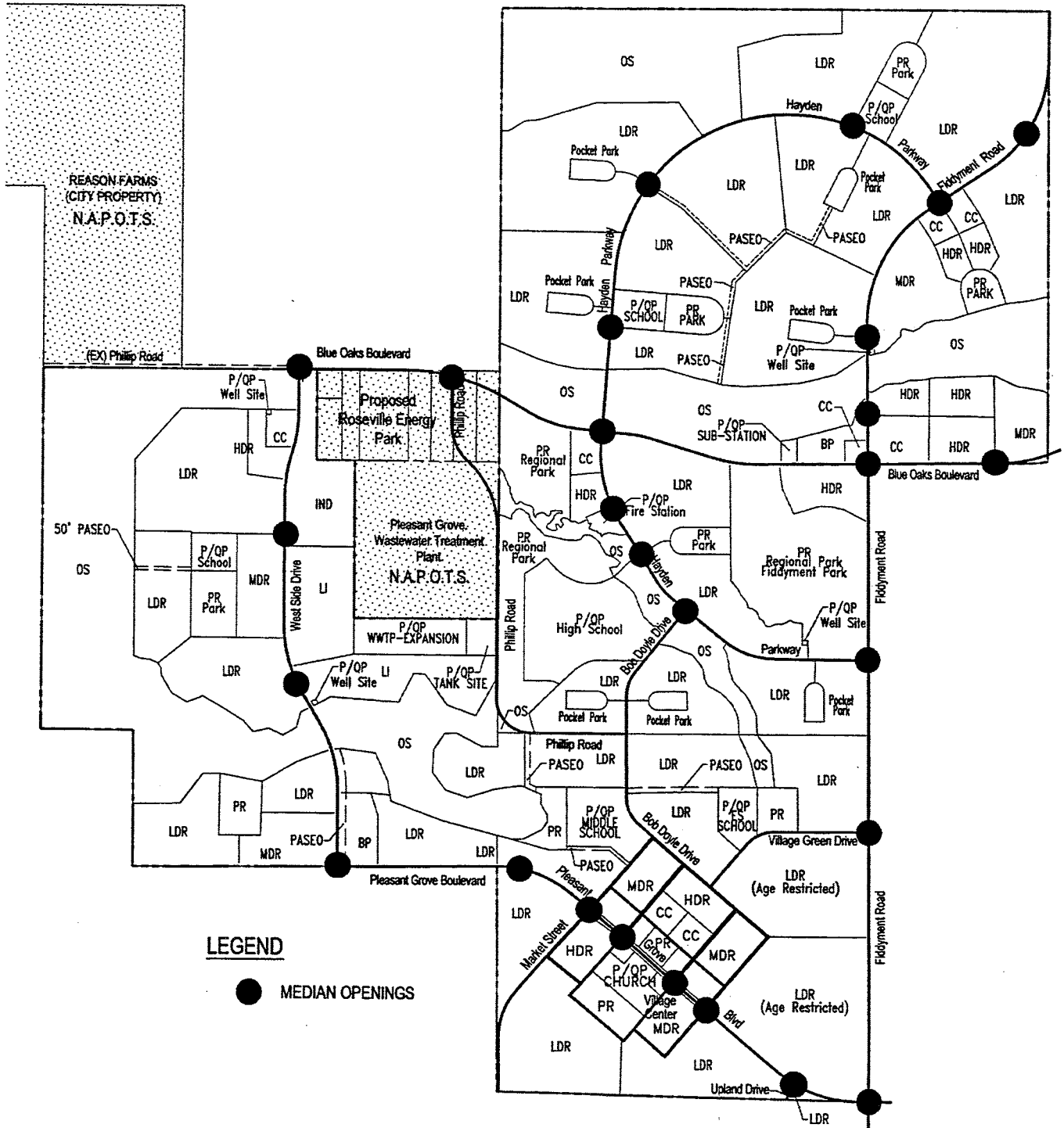


Figure 12-11 - Typical Median Landscape Design

**Figure 12-12
Median Breaks**



12.2.1.8 Landscape Plant Palette

BOTANICAL NAME	COMMON NAME	BOTANICAL NAME	COMMON NAME
Large Shrubs: 5' – 6' Tall		Small Shrubs: 1' – 3' Tall	
Arbutus unedo 'Compacta'	Dwarf Strawberry Tree	Agapanthus "Peter Pan"	Dwarf Agapanthus
Callistemon citrinus	Lemon Bottlebrush	Artemisia 'Powis Castle'	Artemisia
Camellia japonica (in selected areas only)	Japanese Camellia		
Camellia sasanqua (in selected areas only)	Sasanqua Camellia	Azalea 'Southern Indica'	Azalea
Cotoneaster parneyi	Parney Cotoneaster	Baccharis pilularis 'Pigeon Point'	Dwarf Coyote Bush
Cupressus sempervirens	Italian Cypress	'Crimson Pygmy' Barberry	Crimson Pygmy
Dodonea viscosa 'Purpurea'	Hopseed	"Shishi-Gashira" Camelia	Shishi-Gashira
Escallonia fradesii	Escallonia	Chaenomeles 'Stanford Red'	Flowering Quince
Ilex cornuta Chinese Holly		Cistus skanbergii	Rockrose
Juniperus chinensis 'Torulosa'	Hollywood Juniper	Cistus 'Sunset'	Sunset Rockrose
Ligustrum japonicum 'Texanum'	Texas Privet	Cotoneaster dammeri 'Lowfast'	Lowfast Bearberry
Nerium oleander	Oleander	Cotoneaster congestus	Pyrenees Cotoneaster
Photinia fraseri	Photinia	Felicia amelloides	Blue Marguerite
Pittosporum tobira	Mock Orange	Gardenia radicans	Dwarf Gardenia
Pittosporum tobira 'Variegata'	Variegated Tobira	Hemerocallis hybrid	Evergreen Daylily
Podocarpus macrophyllus 'Maki'	Shrubby Yew Pine	Heuchera sanguinea 'Santa Ana Cardinal'	Coral Bells
Prunus caroliniana 'Brite-n-Tite'	Carolina Cherry	Iris germanica	Bearded Iris
Raphiolepis indica 'Majestic Beauty'	Majestic Beauty Raphiolepis	Juniperus conferta	Shore Juniper
Viburnum opulus 'Roseum'	European Cranberry Bush	Juniperus horizontalis "Youngstown"	Youngstown Juniper
Xylosma congestum	Shiny Xylosma	Lavendula angustifolia	English Lavender
Prunus laurocerasus	English Laurel	Liriope muscari 'Big Blue'	Big Blue Lily Turf
		Mohonia aquifolium 'Compacta'	Dwarf Oregon Grape
Medium Shrubs: 3' – 4' Tall		Nandina domestica "Nana"	Heavenly Bamboo
Abelia grandiflora "Ed Goucher"	Glossy Abelia	Penstemon gloxinioides 'Firebird'	Border Penstemon
Agapanthus Africanus	Lily of the Nile	Pittosporum tobira 'Whealers Dwarf'	Dwarf Tobira
Azalea spp.	Azalea (in selected areas only)	Raphiolepis ballerina	Dwarf Raphiolepis
Berberis thunbergii 'Atropurpurea'	Red Leaf Japanese	Rosemary ingramii	Collingwood Ingram
Buxus japonica	Boxwood species	Salvia greggi	Autumn Sage
Cistus purpureus	Orchid Rockrose	Santolina virens	Santolina
Dietes vegeta	Fortnight Lilly	Spiraea bumalda	Anthony Waterer
Escallonia terri	Escallonia	Tulbaghia violacea	Society Garlic
Gardenia jasminoides 'veitchii'	Veitch Gardenia		
Grevillea noellii	Grevillea		
Hypericum moseranum	Gold Flower		
Juniperus sabiana 'Broadmoor'	Broadmoor Juniper		
Nandina domestica	Heavenly Bamboo		
Nerium oleander 'Petite'	Dwarf Oleander		
Pinus mugo mugo	Mugo Pine		
Plumbago capensis 'Otto Luyken'	Cape Plumbago Otto Luyken Laurel		
Raphiolepis indica 'Jack Evans'	Jack Evans		
Rosa spp.	Various Rose species		
Xylosma c. 'Compacta'	Dwarf Xylosma		

12.2.1.8 Landscape Plant Palette, continued

<u>Groundcover</u>		Spacing 1 gal or flats
Acacia redolens	Creeping Acacia	36" o.c.
Arctostaphylos 'Emerald Carpet	Dwarf Manzanita	24" o.c.
Baccharis pilularis 'Twin Peaks'	Coyote Bush	24" o.c.
Coprosma pumila "Verde Vista'	Coptosma	36" o.c./18" o.c.
Hedera Helix	English Ivy	24" o.c./12" o.c.
Hypericum calycinum	St. Johns Wort	14" o.c./12" o.c.
Juniperus conferta	Shore Juniper	36" o.c.
Juniperus chinensis	Chinese Juniper	36" o.c.
Rosmarinus officinalis	Prostrate Rosemary	18" o.c./12" o.c.
Trachelospermum asiaticum	Asian Jasmine	24" o.c./12" o.c.
Trachelospermum jasminoides	Star Jasmine	24" o.c./12" o.c.
Vinca major (in selected areas only)	Periwinkle	24" o.c./12" o.c.
Vinca minor	Dwarf Periwinkle	24" o.c./12" o.c.

<u>Turf</u>	
Preimum 9 5/5 Sod grown by Delta Bluegrass	
Sod shall be a blend of:	
35% Crossfire Dwarf Fescus	
30% Mini-Mustang Dwarf Fescue	
30% Leprechan Dwarf Fescue	
5% Newstar Bluegrass	
Hydroseeded lawn areas shall be allowed at the discretion of the DRB. If hydroseed lawn is used, it shall be composed of the seed mixture noted above.	
<u>Hydroseeded Native Grasses & Wildflowers (for paseos and open space areas)</u>	
Critical Coastal Mix from Albright Seed Company (for erosion control and natural areas)	
30% Cucamonga Brome	
25% California Oats	
20% Zorro Fescue	
10% Common Vetch	
10% Crimson Clover	
5% California Field Flowers	
Wildflower Mix (for erosion control)	
5% Achillea millefolium (Yarrow)	
5% Eschscholzia californica (California Poppy)	
10% Vulpia microstachys (Annual Fescue)	
13% Hordeum brachyathenum (Meadow Barley)	
13% Trifolium microcephalum (Clover)	
16% Festuca rubra (Native Red Fescue)	
10% Elymus glaucus (Blue Wild Rye)	
10% Bromus carinatus (California Brome)	
13% Nassella cernua (Nodding Needlegrass)	
Each seed mix should be reviewed by a seed purveyor for specific application and slurry components.	

<u>Vines</u>	
Campsis radicans	Trumpet Vine
Clematis spp.	Clematis
Clytostoma callistigioides	Violet Trumpet Vine
Ficus Repens	Creeping Fig
Lonicera japonica	Honeysuckle
Parthenocissus tricuspidata	Boston Ivy
Rosa 'Cecile Brunner'	Climbing Rose
Wisteria sinensis	Chinese Wisteria

12.2.1.8 Landscape Plant Palette, continued

Prohibited Plant Palette

The following plants are prohibited, unless approved by the DRB, since they are inconsistent with the major planting themes established for the Plan area. Other trees and plants may be prohibited upon review of the landscaping plans, depending on species, location and quantity proposed.

Trees

Acacia sp.	Acacia
Ailanthus altissima	Tree of Heaven
Catalpa sp.	Indian Bean Tree
Geijera parviflora	Australian Willow
Ligustrum spp.	Privet
Morus alba	Mulberry
Picea sp.	Spruce
Populus sp.	Cottonwood
Robinia pseudocacia	Black Locust
Salix sp.	Willow (except in riparian areas only)
Sygarus romanzoooffianum	Queen Palm
Tamarix aphylla	Athel Tree

Shrubs and Ground Cover

Adenostoma fasticulatum	Chamise
Artemisia californica	California Sagebrush
Centeranthus rubber	Red Valerian
Cytisus sp.	Broom
Phyllostachys sop.	Bamboo (Running)
Spartium janceum	Spanish Broom
Thuja spp.	Arborvitae

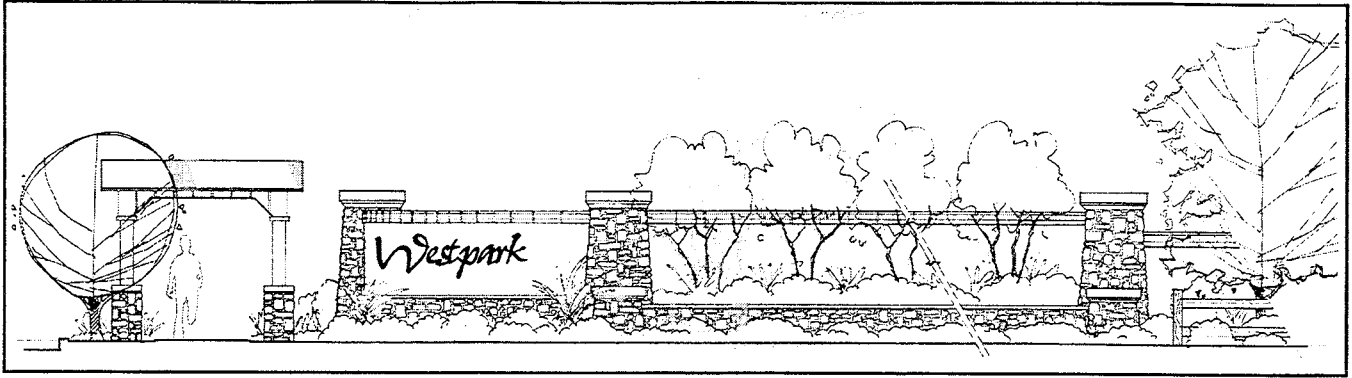


Figure 12-13: City Gateway or Neighborhood Entry Concept - Elevation View

12.2.2. Entrance Features

Entrance features are intended to provide a rhythm to the streetscape; a defining element that reinforces the overall design theme of the Plan Area. A hierarchy of three types of entrance features are provided in the project, ranging from City Gateways, to Neighborhood Entries and smaller Project/Subdivision Entrance Features. Located in landscape corridors along arterial or collector roadways, all entrance features consist of a combination of plant materials, hardscape elements (such as walls and/or monuments), and signage. Figure 12-15 illustrates the location of all entrance features, by type, which are individually described below:

12.2.2.1 City Gateways

City Gateways are the most significant in the hierarchy of entrance features in the Plan. Located along major arterials at project edges, these gateways give a pronounced entrance statement into the City and the West Roseville Specific Plan area. City Gateways are characterized by hardscape and landscape elements that create a "grand" design, themed with plantings, materials, and finishes that are common to the entire Plan Area. The following guidelines shall be employed for City Gateways.

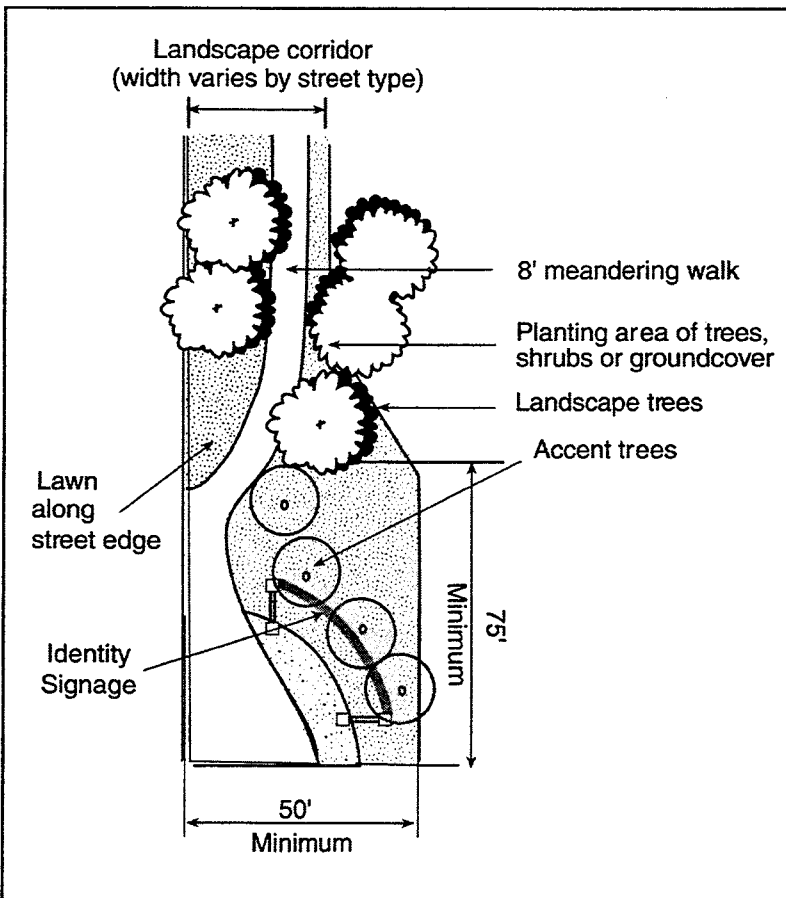
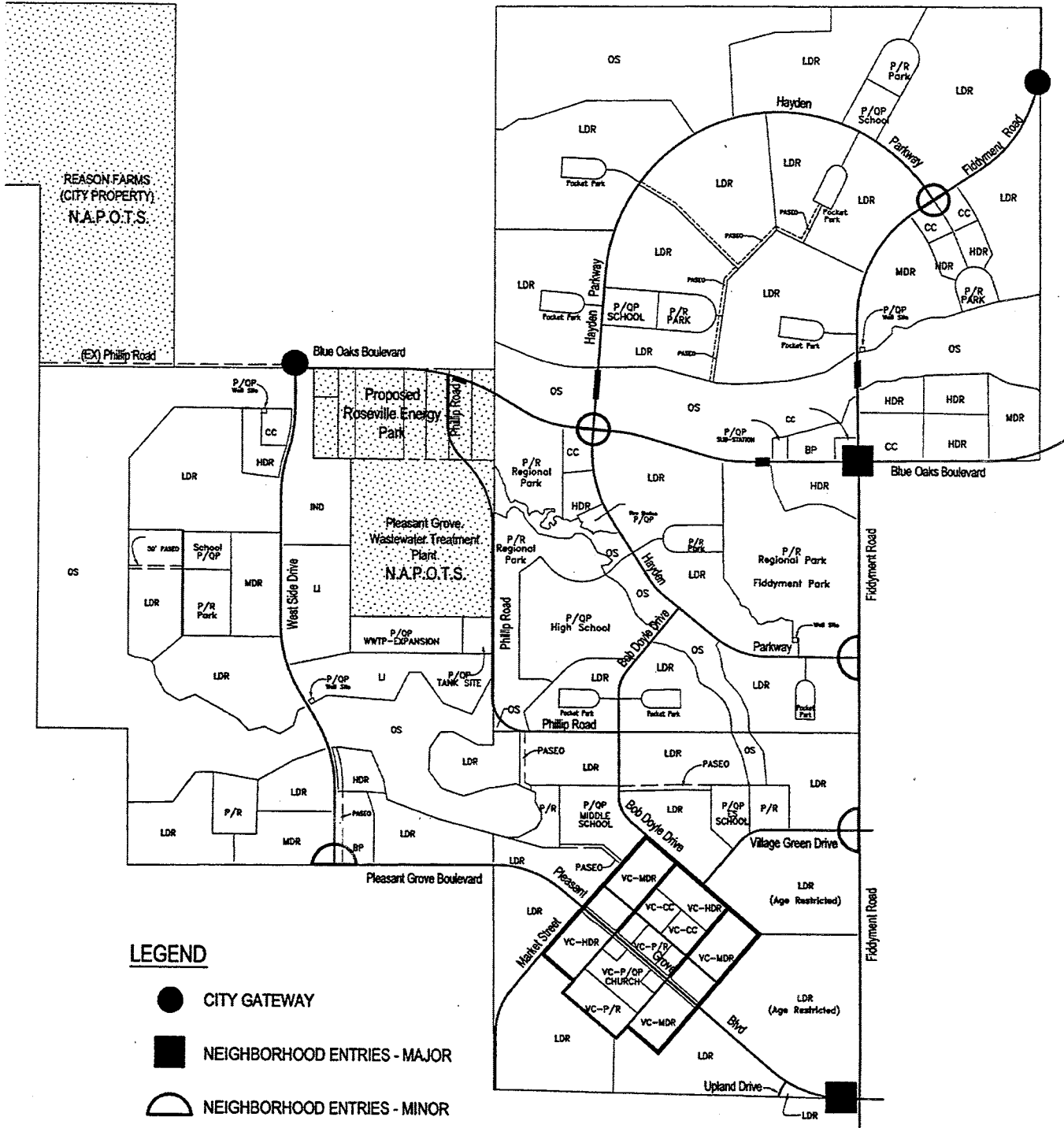


Figure 12-14: City Entrance Gateway Design Concept - Plan View

- Widened landscape corridors at the street edge (see Figure 12-14) that transition to standard landscape corridor width typical for that roadway.
- Themed landscape plantings that to be used at all entrance features within the Plan Area.
- Enhanced landscape and hardscape features derived from a themed palette of monuments, walls, pilasters, raised planters, fountains, plazas, and/or other architectural elements.
- Signage that identifies entrance into the City of Roseville, which may also provide identification for Westpark or Fiddyment Ranch, provided that project signage is at least 20% smaller and complimentary to City signage.
- The city entry features are to be installed by the adjacent parcel developer at the time landscape-corridor improvements are constructed.

**Figure 12-15
Entrance Feature Locations**



LEGEND

- CITY GATEWAY
- NEIGHBORHOOD ENTRIES - MAJOR
- ◐ NEIGHBORHOOD ENTRIES - MINOR

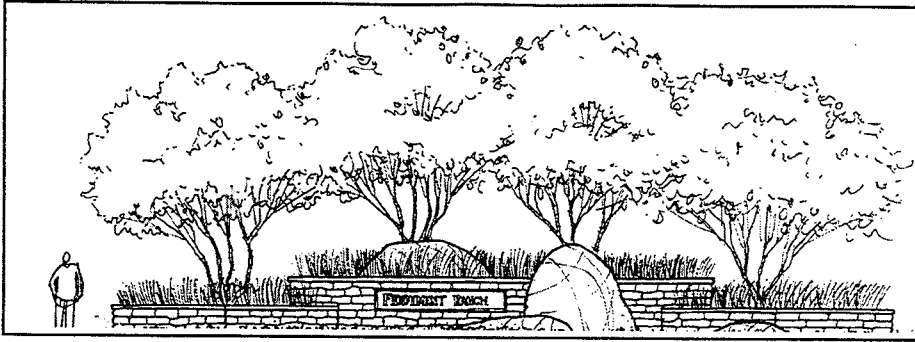


Figure 12-16: Minor Neighborhood Entry Concept

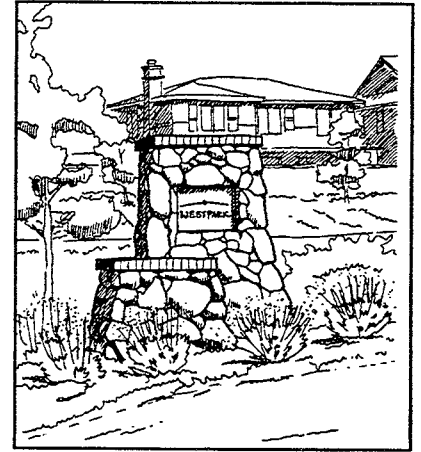


Figure 12-17 Minor Entrance Feature Concept

12.2.2.2 Neighborhood Entries

Neighborhood Entries are dominant streetscape elements that visually define the Plan Area and reinforce the streetscape theme within the Westpark and Fiddyment Ranch neighborhoods. As such, the individual landscape and hardscape themes for these features may differ within each neighborhood, however the intent is that their overall appearance be complimentary to one another. These features are located at major intersections along arterial and collector streets.

Design Intent

Neighborhood Entries are enlarged landscape areas added to the landscape corridors at key intersections throughout the Plan Area. They are intended to provide a distinct gateway and sense of arrival to each neighborhood. Used in conjunction with the themed landscaping along arterial and collector streets, neighborhood entries are a key element in distinguishing the two neighborhoods within the project.

Major and Minor Neighborhood Entries

Two types of Neighborhood Entries are included in the Plan Area:

- Major Neighborhood Entries are intended to be the 'signature' element within each neighborhood that sets the tone for its individual landscape theme. Major entries, one each located in Westpark and Fiddyment Ranch, have a large scale that create a clear gateway and sense of arrival to each neighborhood.
- Minor Neighborhood Entries are located throughout each neighborhood's street network to further define and reinforce their respective theme. These features are smaller in scale than the major entries, but utilize the same landscape design concepts to reinforce the streetscape theme of each neighborhood.

Figure 12-15 illustrates the location of all Neighborhood Entries.

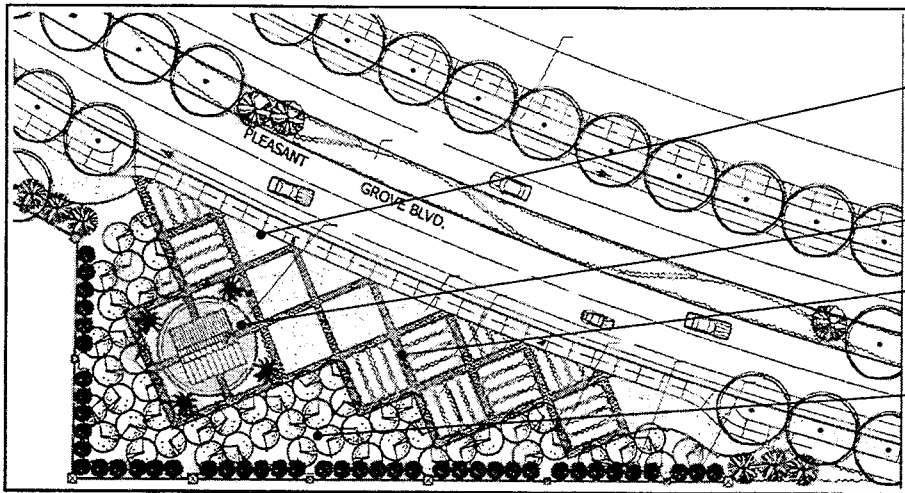
Westpark Neighborhood Entries are characterized by:

- Plaster walls with stone accents and concrete caps executed with rich earth hues;
- Project identification signage/logos are accented with bright, complementary colors (subject to Roseville Sign Ordinance);
- Trellis and shed structures made of wood, metal and tubular steel;
- Raised masonry planters; and
- Significant stands of evergreen and deciduous flowering trees used in regular, linear planting patterns, emulating agricultural planting regularity and reinforcing the community theme.

Fiddyment Ranch Neighborhood Entries are characterized by:

- Masonry walls with brick and stone accents, concrete caps, and executed in rich earth hues;
- Project identification signage/logos are accented with bright, complementary colors (subject to Roseville Sign Ordinance);
- Significant stands of evergreen and deciduous flowering trees used in rigid, orchard configurations, highlighting each neighborhood and community entry. In addition specimen quality native trees, specifically valley and blue oaks, chosen for unique characteristics or shapes will be integrated into the entry theme. Each of these entry areas shall have a single tree species as an identifier.

Major Neighborhood Entry Features



Major Neighborhood Entry - Westpark

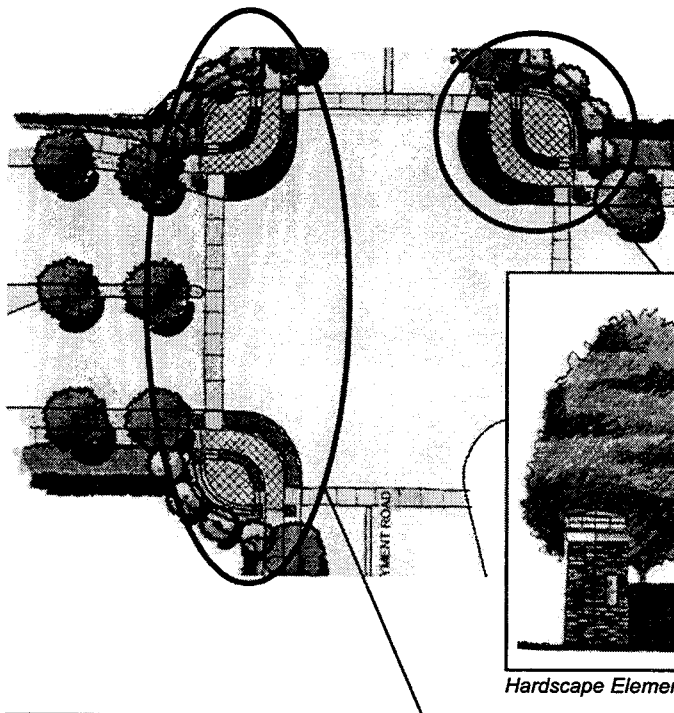
Major Neighborhood Entrance into Westpark - located on south side of Pleasant Grove Boulevard, west of Fiddymment Road.

Entrance feature with signage

Enhanced hardscape materials, including raised walls, planters, pilasters, and paving

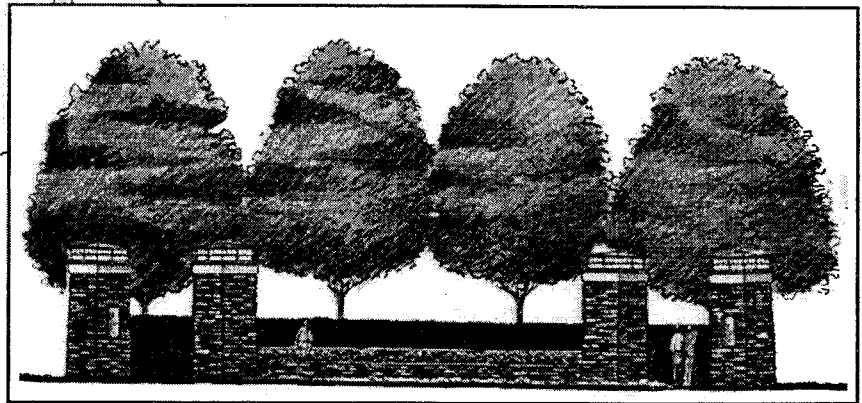
Thematic landscape materials consistent for all of Westpark's plant palette

Figure 12-18: Major Neighborhood Entry Design Concept in Westpark



Major Neighborhood Entry - Fiddymment Ranch

- Located at intersection of Blue Oaks Boulevard and Fiddymment Road
- Street corners incorporate hardscape elements including enhanced paving, pilasters, walls, raised planters.
- Thematic landscaping and accent trees set the tone for Fiddymment Ranch.
- Must not interfere with vehicular sight lines.



Hardscape Elements at each Street Corner - Pilasters and Raised Planters

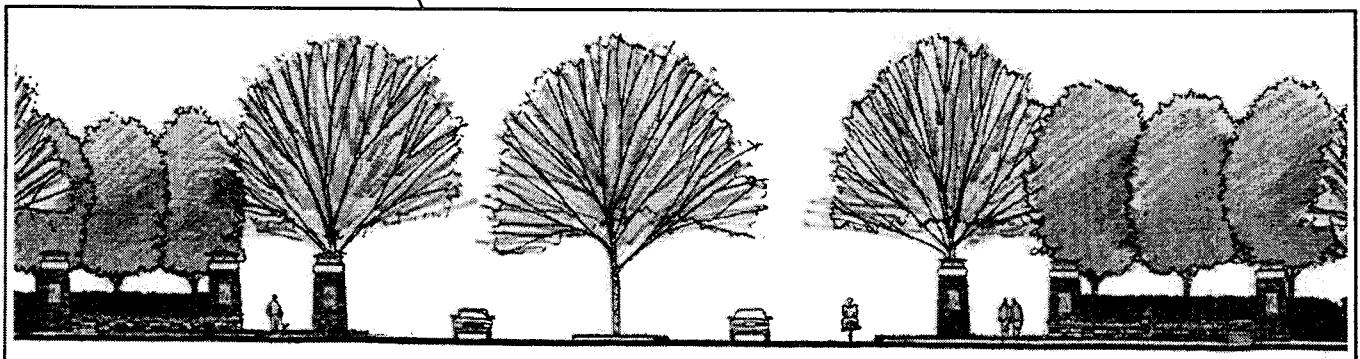


Figure 12-19: Major Neighborhood Entry Concept in Fiddymment Ranch - Looking West on Blue Oaks Boulevard

Minor Neighborhood Entries

Minor Neighborhood Entry - Westpark

- Located at major intersections along arterial and collector roadways in Westpark (Refer to Figure 12-20)
- Street corners incorporate corner clips with decorative walls and pilasters, neighborhood entry signage, and raised planters, consistent with the landscape theme used throughout Westpark
- Accent trees and thematic groundcover and shrubs, consistent with the landscape design themes used in the major neighborhood entries.

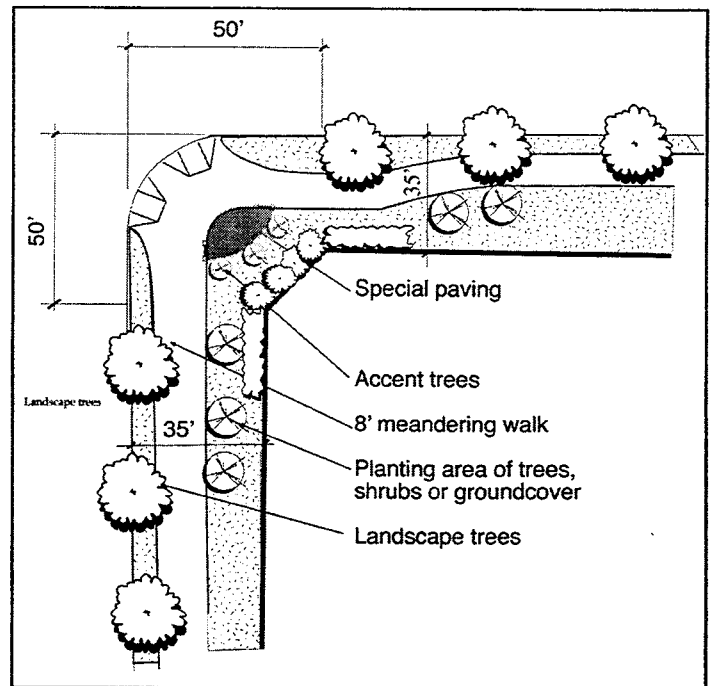


Figure 12-20: Minor Neighborhood Entry Design in Westpark

Minor Neighborhood Entry - Fiddymt Ranch

- Located at major intersections along arterial and collector roadways in Fiddymt Ranch (Refer to Figure 12-21)
- Street corners incorporate hardscape elements including tall pilasters, low walls, and raised planters, clad with thematic stone representative of Fiddymt Ranch
- Accent trees and thematic groundcover and shrubs, consistent with the landscape design themes used in the major neighborhood entries.
- Pilasters and other hardscape elements shall not interfere with vehicular sight lines, as determined by the Public Works Director.

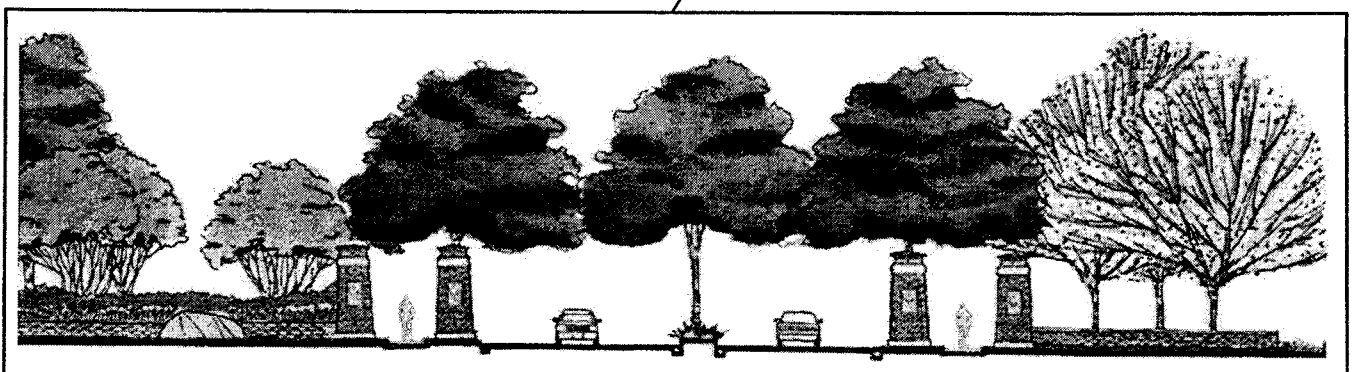
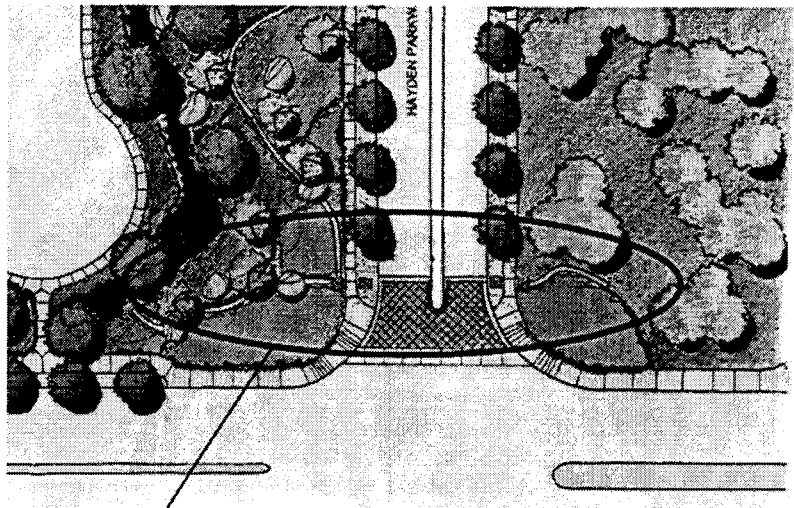


Figure 12-21: Minor Neighborhood Entrance Feature in Fiddymt Ranch

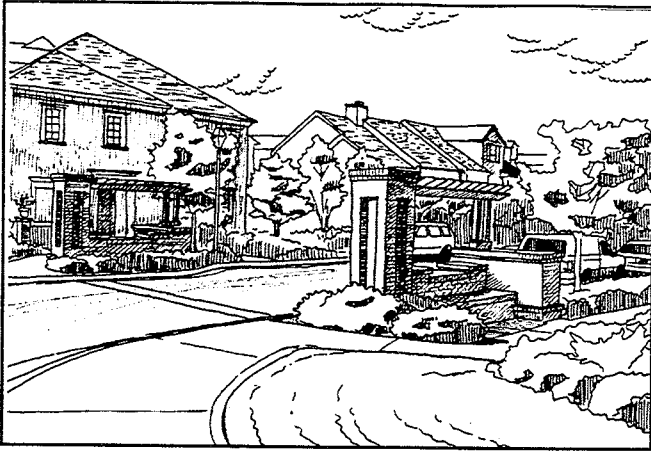


Figure 12-22: Subdivision/Project Entrance Feature Concept

12.2.2.3 Project/Subdivision Entries

Project/ Subdivision Entries are intended to provide a formal entrance into individual residential subdivisions within the Westpark and Fiddymment Ranch neighborhoods.

Project/Subdivision Entries are:

- Typically located at the subdivision entrance, on the subdivision wall or in the entrance median, at the primary access point from an arterial or collector street.
- Thematic wall or other hardscape features (such as trellises, raised planters, pilasters, etc.) that are consistent with the individual theme of Westpark or Fiddymment Ranch.
- Signage that identifies the subdivision name.
- Subdivision entrance signage may be incorporated into a wall or fence.
- Entry features shall not impact site distance requirements for automobiles.
- The number, height, and size of all signs shall be consistent with the requirements of the Roseville Sign Ordinance.

Materials

Material selections shall utilize the plant palette contained herein. Signage shall be consistent with the materials listed below, subject to review and approval by the Parks and Recreation Department and subject to provisions in the Roseville Sign Ordinance.

- Backlit Raised Aluminum Letters
- Uplit Raised Aluminum Letters
- Flush Mount Channel Letters
- Flush Mount Masonry or Metal Wall Plaques
- Cast concrete signage

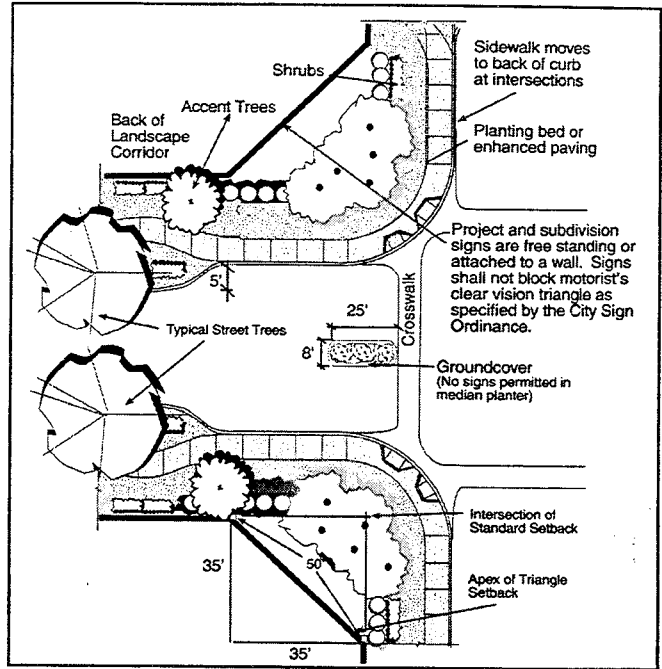


Figure 12-23: Subdivision/Project Entrance - Plan View

Signage

All sign elements on pilasters or walls shall use mounting hardware securely embedded into the surface onto which it is affixed. No epoxy-mounted elements are permitted. Where signs and monuments are to be uplit, such lighting equipment shall be approved by the City.

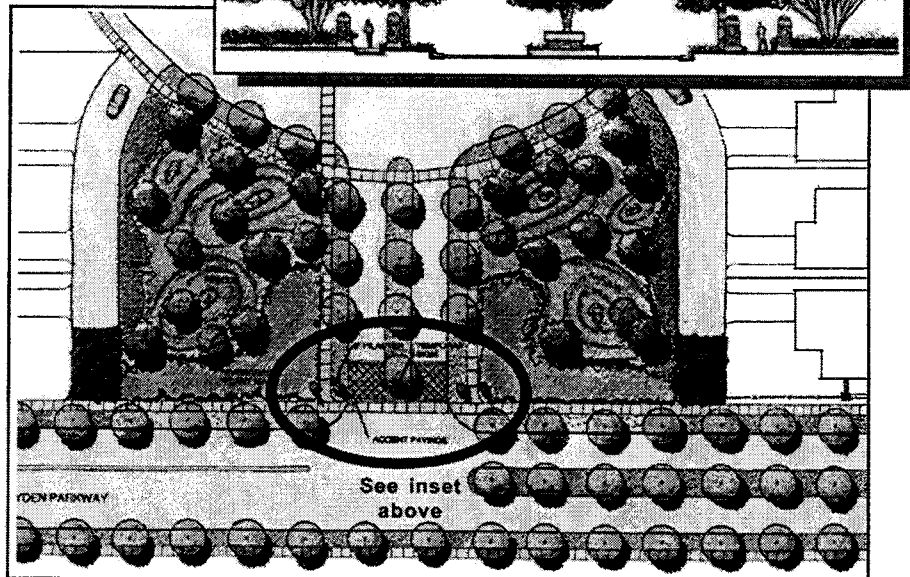


Figure 12-24: Project/Subdivision Entry Design Concept in Fiddymment Ranch

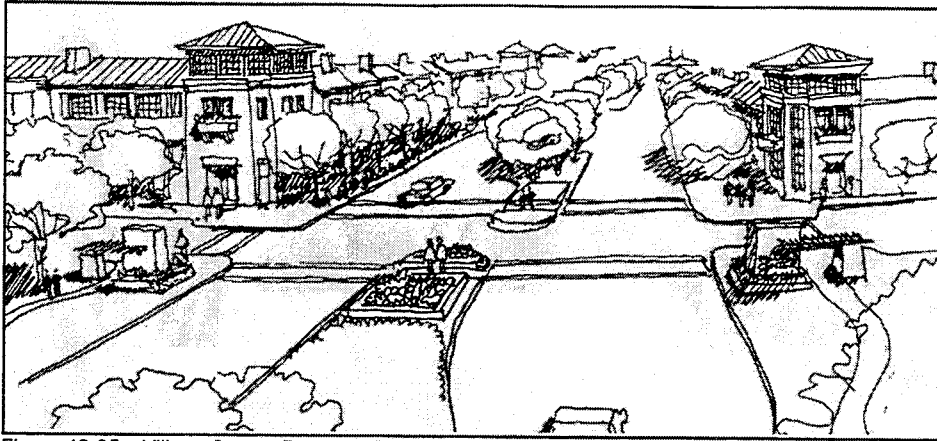


Figure 12-25: Village Center Entrance Gateway Design Concept - Pleasant Grove Boulevard

12.2.2.4 Village Center Gateway Entrance

Along Pleasant Grove Boulevard, significant entrance features shall be incorporated into each edge of the Village Center district. The intent of these gateways are to visually define the Village Center as a unique district within the Plan Area, which is clearly distinguishable from other neighborhoods.

The gateway feature is an important element in creating an appropriate transition along the streetscape. Its design shall incorporate:

- Three-dimensional hardscape features such as monument, pilasters, trellises, raised planters, and/or other features that visually define the gateway along Pleasant Grove.
- Hardscape/landscape features shall not interfere with the City's site distance requirements.
- A change in street trees and landscape materials that are unique to the Village Center district.
- A change in street and accent lighting.
- An enhanced pedestrian crossing with lighted bollards and special crosswalk paving (only at western gateway).
- Figure 12-25 illustrates the design concept that shall be used to shape this gateway's final design.
- Gateway is to be constructed simultaneously with construction of Pleasant Grove Blvd.
- **Refer to Chapter 10 (Village Center Plan) for more information about the enhanced pedestrian crossing in this gateway feature.**

12.2.2.5 Site Design for All Entry Features

Neighborhood and subdivision/project entrance features are to be located in "corner clips" (triangular landscape corridor enlargements at street intersections). The following parameters shall guide the site design and landscape/hardscape elements for all entrance features located at intersections:

- Where fencing is provided at the rear of corner clips, the fencing shall be a masonry wall (with pilasters or columns) to match or accent the adjacent masonry wall.
- Improvements within corner-clip areas shall be placed to allow adequate vehicular lines of sight at intersections.

- Minimum corner-clip offset is 35 feet from the edge of the required landscape corridor. Non-triangular corner clips are permitted (i.e., curved, stepped, etc.) provided they do not encroach into the minimum offset area.
- Corner clips shall be landscaped with accent plantings that are visually compatible with the adjacent landscape corridors.
- Figure 12-26 illustrates the typical design for an entrance feature in a corner clip.

12.2.2.6 Signage on Entries

Project identification signage is permitted on all entrance features. Sign text shall only be permitted to identify the name of each neighborhood (i.e. Fiddyment Ranch) or individual projects/subdivisions. Entrance feature signage is regulated by the Roseville Sign Ordinance and is subject to the permitting requirements of the City.

All signs, including those related to commercial, office, and multi-family use, as well as temporary construction, marketing, and sales signs, are regulated by the Roseville Sign Ordinance.

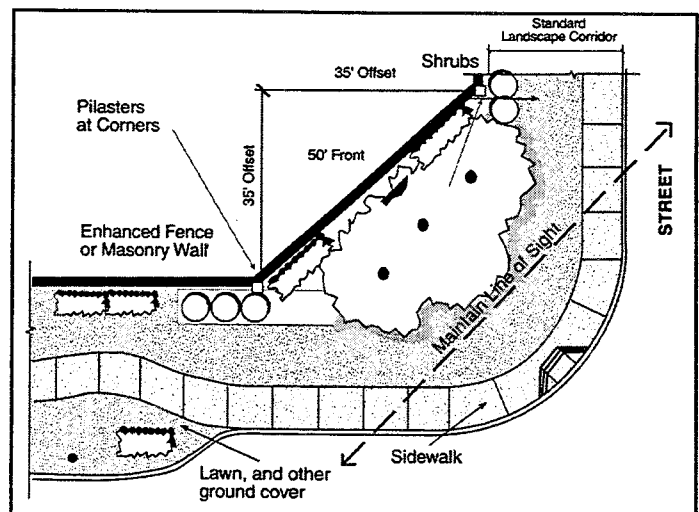
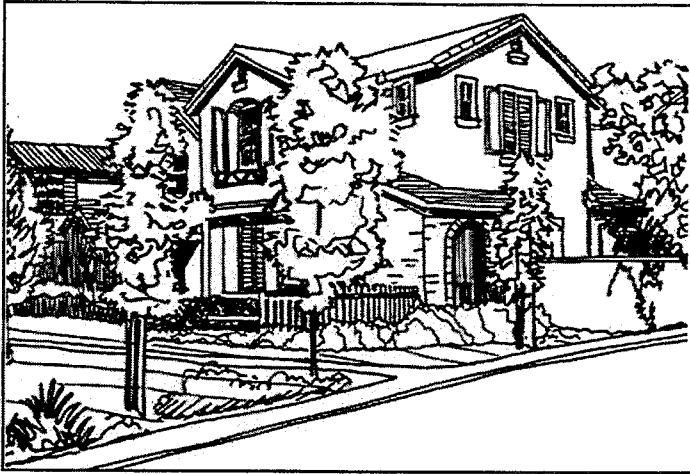


Figure 12-26: Entrance Feature Site Design/Corner Clip Detail



12.2.3 Paseos

12.2.3.1 Design Concept

Paseo's are landscape corridors within residential subdivisions that provide pedestrian and bikeway linkages to the open space preserves and regional parks. Paseos are open to the surrounding neighborhood by always maintaining a street adjacent to one side. This ensures that paseos are not hidden behind residential or commercial areas, which allows them to be viewed by residents and the City for security access. Paseos vary in width from 25' to 75' feet and contain thematic landscape and hardscape materials consistent with those used throughout the Westpark and Fiddymont Ranch neighborhoods.

12.2.3.2 Landscaping In Paseos

Landscaping in paseo's shall use a combination of trees, shrubs, turf, and groundcover that mirrors the design concepts utilized along the streetscapes. Landscaping should also utilize design concepts that do not create secluded areas within the landscape that might generate security concerns.

12.2.3.3 Bikeways In Paseos

All Paseos shall contain a Class I Bike Path, consistent with the City's adopted standards in the Bicycle Master Plan. The bike path within the paseo shall be linked with the streets and sidewalks in the adjacent neighborhoods. Bikeway linkages in paseos are critical to the Plan Area's comprehensive Class I system.

12.2.3.4 Lighting

Paseos shall have lighting, either by bollards or light standards, or a through a combination of both. Light standards from adjacent residential streets shall be used to illuminate paseos.

12.2.3.5 Street and Cul-de-sac Interface

The following standards shall guide the interface of paseos with the surrounding neighborhoods:

- Paseos shall be open to a street or cul-de-sac on one side at all times.
- Paseos shall have a minimum of 50% street-edge frontage on each side.
- Although 50% is the minimum standard for street-frontage along each edge, the goal is to exceed this standard by striving to incorporate at least 75% street frontage along each edge.
- In cases where a paseo is bounded by separate residential subdivisions, the lotting in each subdivision shall be designed to not preclude surrounding subdivisions from meeting their street frontage requirement, or place undue burden on its lotting design.
- Paseos shall not have fencing on opposing edges, unless specific design merits warrant such a case and the resulting design is consistent with the intent of this section, as determined by the Planning Director.

Figure 12-27 illustrates acceptable and unacceptable designs for paseo interface with streets of an adjacent residential subdivision.

12.2.2.6 Interface with Adjacent Land Uses

- Where a paseo backs up to a residential lot, a minimum 10' separation shall be provided between the edge of path and the lot's property line fence.
- To the extent possible, where a paseo is adjacent to a street or cul-de-sac, the path shall be located next to, or be part of, the street sidewalk to ensure the maximum exposure possible.
- Where residential properties side-or back-on to paseos, fences should have a standard wood design.
- Wood fences should be stained on the side facing the paseo.
- Residences should be oriented towards paseos, rather than backing up to them, to the best extent possible.

Figure 12-27

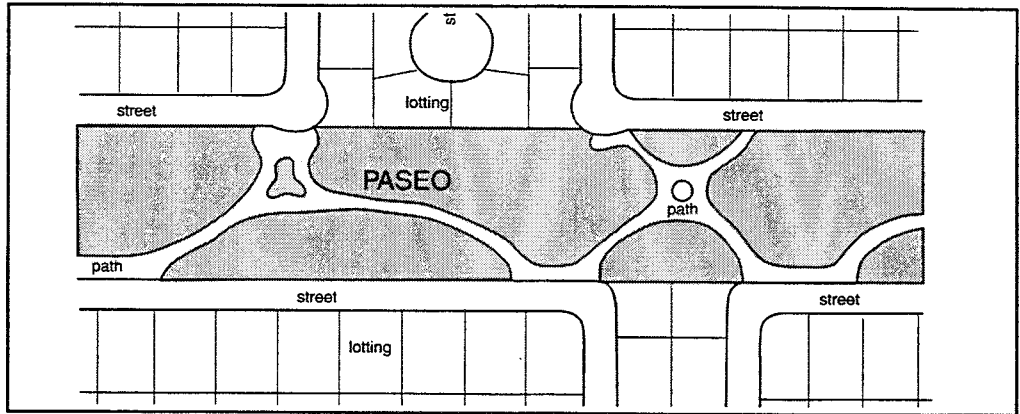
Paseo Interface with Surrounding Residential Neighborhoods

Illustrative examples of successful and unsuccessful residential subdivision integration with paseo edges

Preferred

This paseo has the preferred interface with the subdivision's residential streets. It is open on at least one side at all times and is well-integrated into the residential neighborhood. Its open design and exposure allows the paseo to be viewed by residents and the City for security.

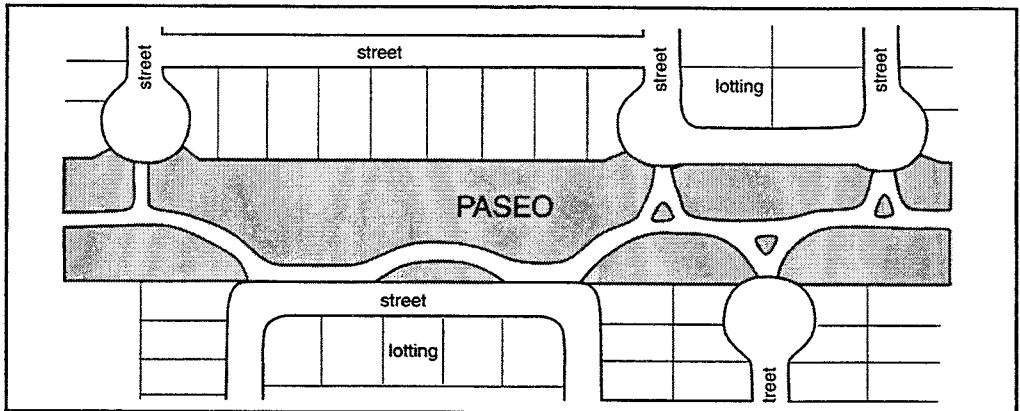
This paseo/subdivision interface meets and exceeds the minimum design standards.



Acceptable

This paseo has an acceptable interface with the adjacent residential streets. It meets the minimum 50% frontage standard and is open on at least one side. Cul-de-sacs do not provide as much visibility for security as single-loaded streets.

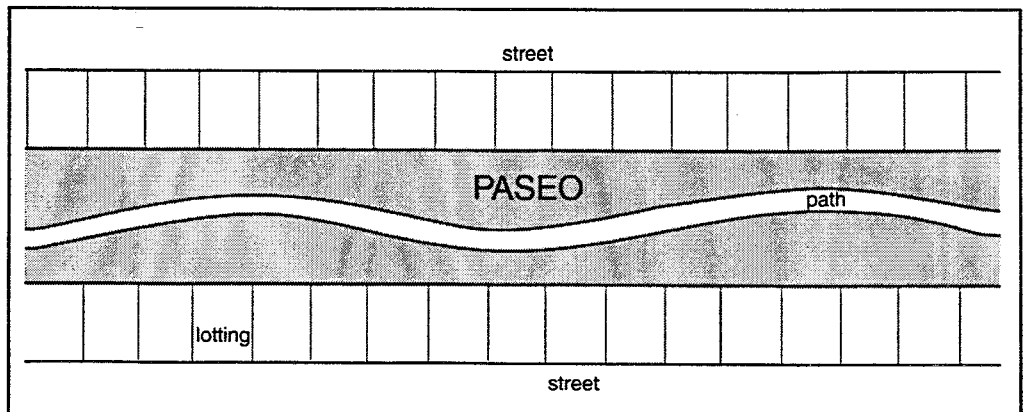
This paseo/subdivision interface meets the minimum design standards.



Prohibited

This paseo does not have an acceptable interface with the adjacent subdivisions. It does not have street frontage on either side, which creates an undesirable "tunnel" between rear yard fences of residential lots. Also, the paseo cannot be viewed for security.

This paseo does not meet the design standards contained herein and shall not be permitted.



12.2.4 Residential Subdivision Design Requirements

All residential subdivisions in the WRSP are subject to the design requirements contained in the City's Subdivision Ordinance. However, given the extensive residential interface with the project's system of open space preserves, several additional design considerations must be employed. Open Space areas are a defining feature of the West Roseville Specific Plan, therefore, this feature should be integrated into the residential neighborhood design to the best extent feasible. To this end, the following criteria shall be employed when reviewing the street design and subdivision layout of each residential subdivision.

- Each neighborhood shall provide access to adjacent parks, natural creek corridors, pedestrian parkway corridors, or paseos.
- A subdivision's internal street system shall be designed to allow residents to walk easily to nearby parks.
- Residential units shall be oriented toward parks, rather than backing up to them.
- Neighborhood parks shall front on at least one single-loaded residential street to provide visibility, create open access for residents, and to incorporate the amenity into the surrounding neighborhood.
- Residential subdivisions located adjacent to open space areas shall provide visual and physical access to the Open Space. This standard shall apply where a pedestrian or bike path is provided in the open space area, but is encouraged in all other instances to improve each neighborhood's visual and physical access to the open space system.
- Where residential subdivisions are located adjacent to an open space preserve, a variety of alternative street patterns and residential lot configurations shall be used to achieve visual and physical access to open space areas. These alternatives provide open space views, and allow for a more direct connection to the Open Space. Alternative street patterns are illustrated in Figure 12-28.
- Where residential lots back up or side onto open space areas, the use of open-style fencing is appropriate. However, where privacy, security, or noise attenuation are of concern (such as adjacent to public trails), solid fencing may be used between residential lots and open space (subject to Fire Department standards).

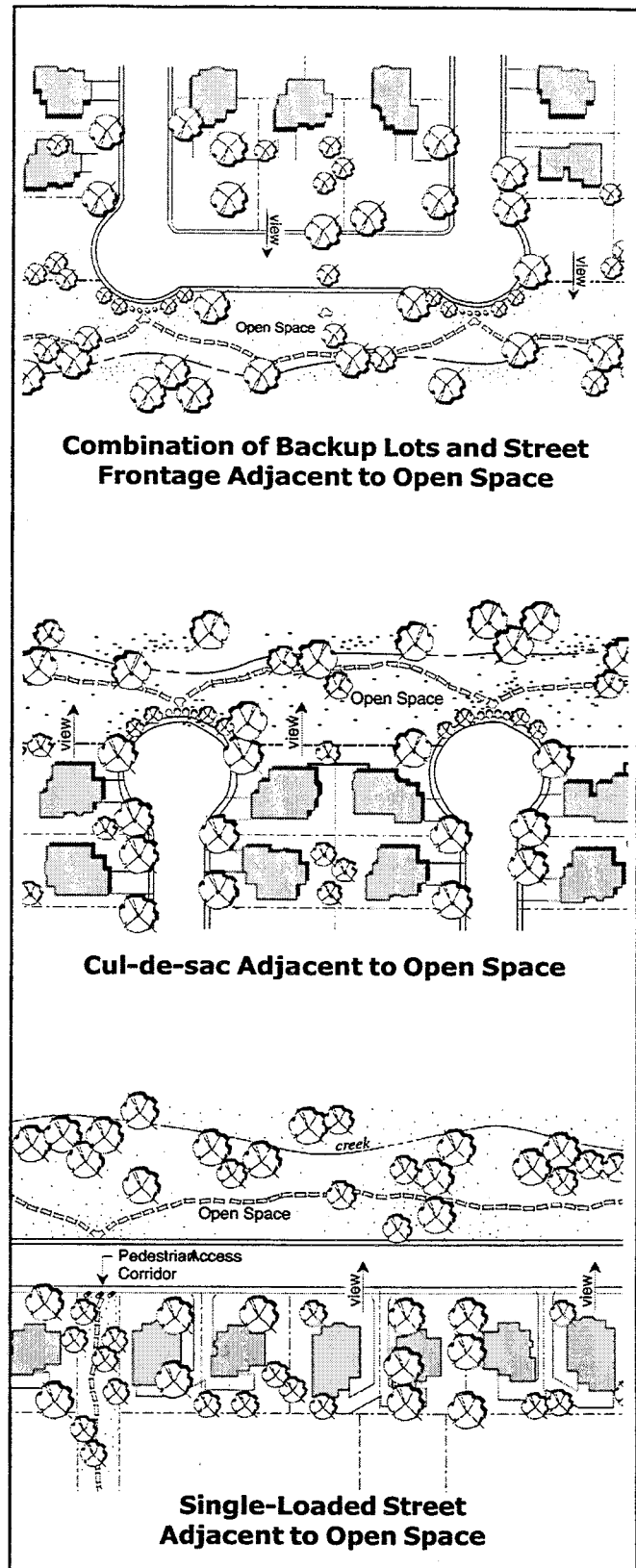


Figure 12-28: Street Pattern Design Alternatives for Integration with Open Space Areas

12.2.5 Pedestrian/Bikeway Network

The bikeway system is designed to provide connections between all of the residential and non-residential neighborhoods in the Plan Area. An extensive Class I Bike Path network is also planned, as illustrated in Figure 7-14 (in Circulation Chapter). The network of Class I, II, III bikeways consist of:

- Class I Bike Paths consist of 10' wide paved lanes with 2-foot wide decomposed granite shoulders. These facilities are typically located in parks, open-space corridors, and paseos.
- Class II Bike Lanes consist of 7'-wide striped lanes located along all arterial and collector streets
- Class III Bike Routes are defined through signage on residential streets and do not have formal lane striping.

Typical design details for Class I Bike Paths and Class II Bike Lanes are illustrated in Figure 12-29 below, per the City's Improvements Standards for bikeway facilities.

Refer to Chapter 7 (Circulation Plan) for complete information on the Plan Area's bikeway system. Figure 7-14 illustrates the system of Class I, II, and Class III bikeways. Site plans for individual parcels will call out specific areas for connections to bike paths when parcels abut open spaces or paseos where consistent with bicycle master plan.

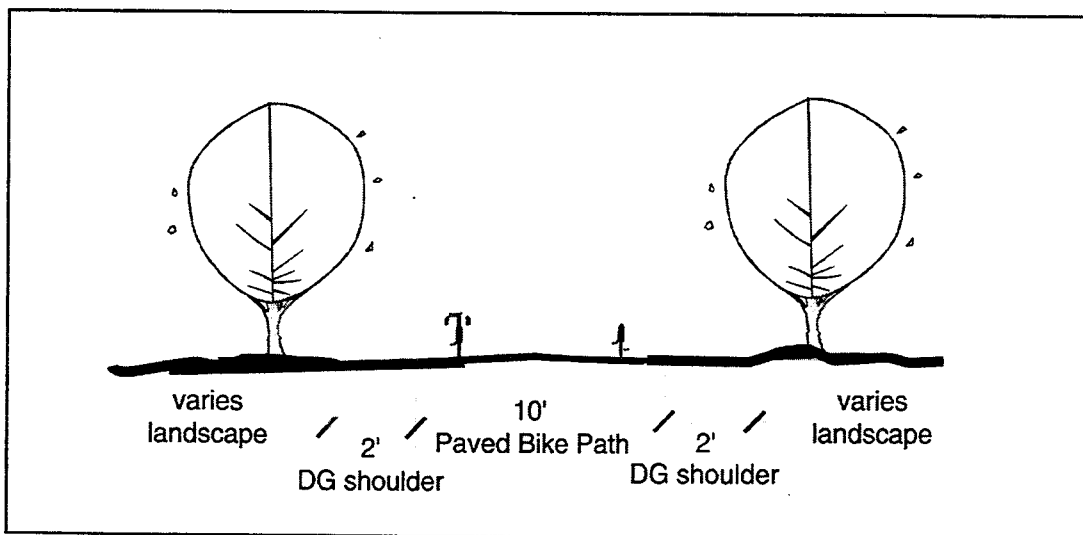


Figure 12-29a: Class I Bike Path - Section View

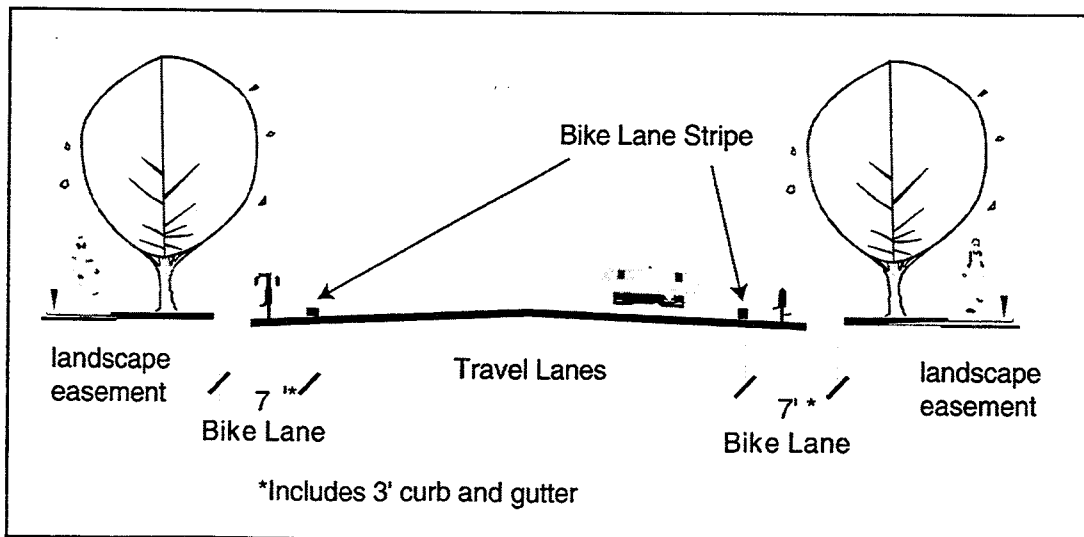


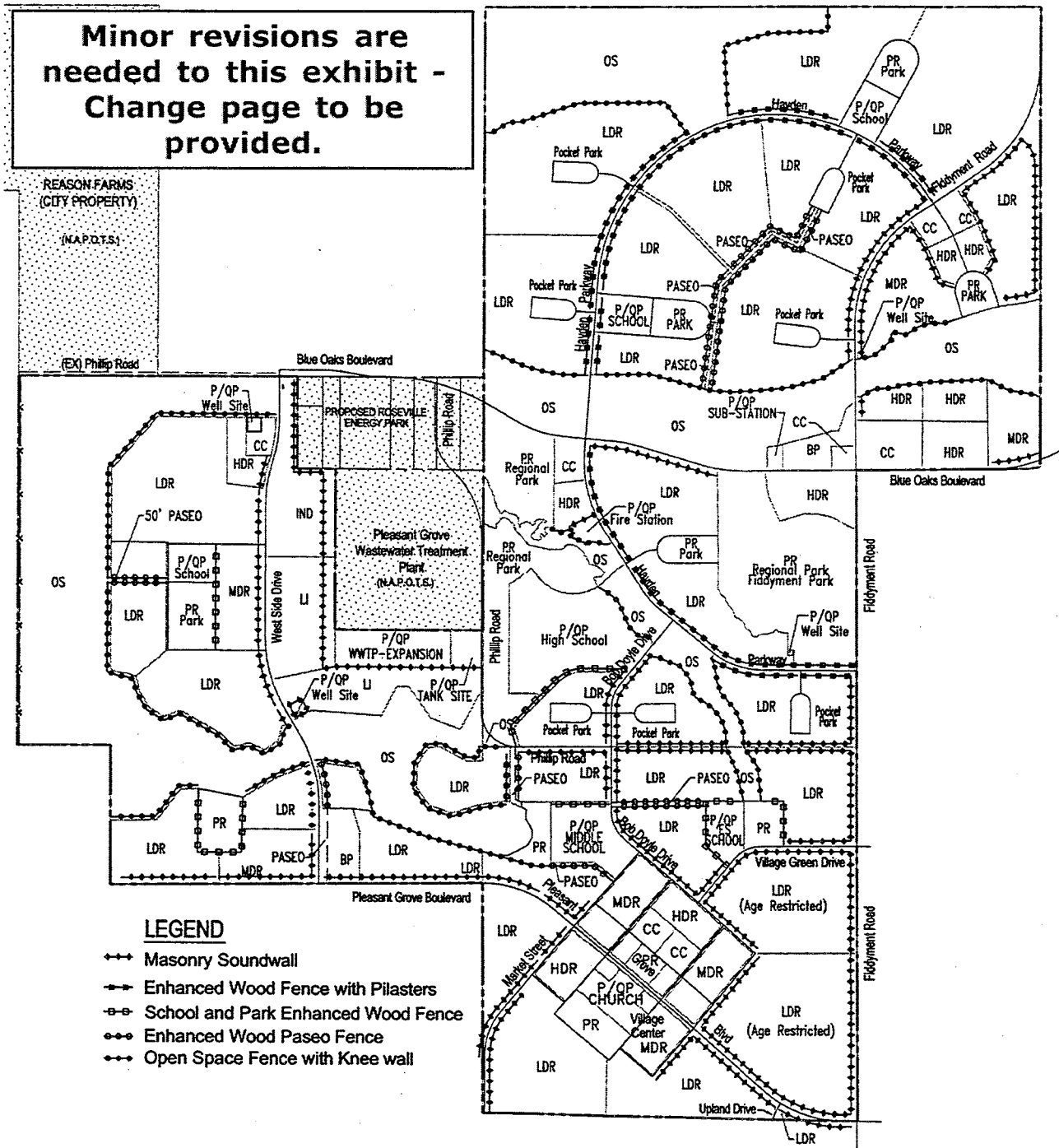
Figure 12-29b: Class II Bike Lane on Roadway - Section View

12.2.6 Walls and Fencing

Walls and fences throughout the Plan Area provide screening and barriers between properties and uses, help define the edges of arterial and collector streetscapes, and provide privacy and security for private property. The material and design of walls and

fencing vary throughout the Plan Area, depending on each location's specific needs. Figure 12-30 below illustrates the location for all fence types throughout the Plan Area. Design details for each fence type are provided on the following pages.

**Figure 12-30
Walls and Fencing Key Map**



12.2.6.1 Masonry Walls

Two types of masonry walls are specified for use in the Plan Area. Both wall types are intended to provide security, screening, and privacy. Standard masonry walls are located in areas that are less visible from public view (such as behind shopping centers). Enhanced masonry walls are located in areas with high visibility to public streets (such as the back edge of landscape corridors along arterial streets).

Standard Masonry Walls

Standard masonry walls shall be used as barriers and screens between dissimilar land uses. This type of wall shall be used in locations that are less visible from public streets.

- Refer to Figure 12-30 for the location of this wall type.
- Refer to Figure 12-32 for a design detail of this wall type.

Enhanced Masonry Walls

Enhanced masonry walls have a high-quality "architectural" design appearance compared to a standard masonry wall. This wall type incorporates decorative pilasters or columns at regular intervals and a cap along the wall top. Enhanced masonry walls shall be used in areas with visibility to public streets.

The following specific design requirements pertain to masonry walls:

- Masonry walls along public streets should be placed to avoid blocking views to the open space corridors and should not obstruct underground or above-ground electric, telephone cable, water or sewer services or equipment.
- Minimum masonry wall height along arterial streets (Blue Oaks Boulevard, Pleasant Grove Boulevard, Fiddymont Road, West Side Drive) shall be 6'. Higher walls may be required based on site specific noise analyses. Higher wall heights may be achieved by either increasing the height of the masonry structure, or placing the structure on a

berm.

- Opportunities for wall openings between land uses should be evaluated where appropriate to encourage and facilitate pedestrian connection/access between land uses (i.e. between high-density residential and commercial sites).
- Masonry walls shall be constructed of site-built, hand-laid masonry blocks (unless deviations are permitted in specific locations).
- Wall materials shall have a textured face such as cast patterns, split-faced or stucco-finished on the side facing the street or public view.
- Variations in wall designs within the Plan Area are acceptable, however continuity in theme and materials shall be incorporated where variations occur.
- Pilasters should be used at each side of neighborhood vehicular and pedestrian entrances to define openings, and at each angle point or change in direction to enhance wall aesthetics.
- Multiple pilasters at neighborhood entries are encouraged, and should be spaced no less than 50-feet on-center along straight runs of walls.
- Pilasters shall be constructed of materials complementary to the masonry wall. Acceptable materials include masonry block, brick, stone, cobble or stucco finishes.
- Pilasters may include embellishments such as logos incorporated in the column or pilaster design and concrete caps.
- Embellishments and logos shall not exceed 24" in any dimension and must show at least 4" of the column round all edges. Embellishments should be constructed of materials and colors compatible with the design of the column or pilasters and clearly be an integral element in the column design. Embellishments may only be located on the end column or pilaster on both sides of pedestrian or vehicular entries.
- All logos and/or embellishments shall be subject to the requirements of the Roseville Sign Ordinance.
- Pilasters should have sufficient bulk and dimensions to appear in proportion to the height and mass of the wall. Pilasters and columns may not be less than 18" in any dimension at the base, and may be circular or square.

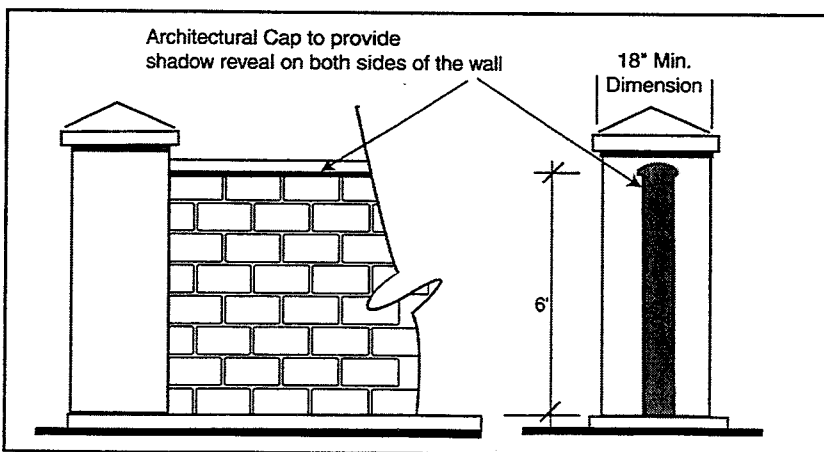


Figure 12-31: Enhanced Masonry Wall Design Detail

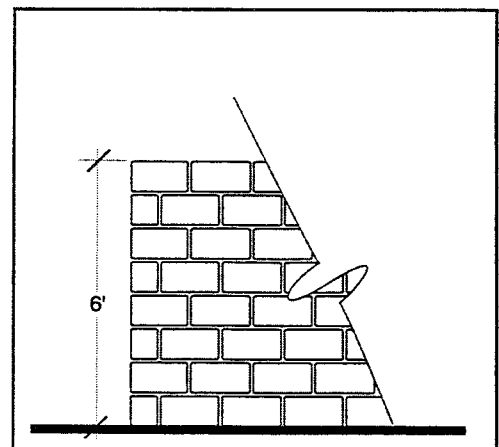


Figure 12-32: Standard Masonry Wall Design

12.2.6.2 Wood Fencing

Two types of wood fencing are specified for use in the Plan Area. Both fence types are intended to provide security, screening, and privacy. Standard wood fences are located in areas that are less visible from public view (such as between residential properties) and enhanced wood fences are located in areas with greater public visibility (such as the back of landscape corridors along residential collector streets).

Figure 12-30 shows the location for wood fencing within the Plan Area.

Enhanced Wood Fence

Enhanced wood fences are a variation of a standard wood fence by incorporating decorative pilasters at regular intervals. This fence type is typically located where lots back or side to a residential collector street. Figure 12-33 illustrates a design detail of this fence type.

Guidelines for enhanced wood fences are:

- Minimum height of solid wood fence along all residential collectors within neighborhoods is 6'.
- Fence sections may be 8' to 10' in length supported by 4-by-4 posts.
- Pilasters or columns should be used at enhanced wood fences at each side of neighborhood vehicular and pedestrian entrances so as to visually define openings, and at each angle point (change in direction) to enhance wall aesthetics. Pilasters are encouraged at regular spaced intervals along lengths of fences.
- Pilasters and columns should be constructed of materials complementary to the wood fence. Acceptable materials include masonry block, brick, stone, cobble and stucco finish. The pilaster material and design shall be consistently applied throughout individual neighborhoods.
- Where residential lots back up to schools, 6' enhanced wood fences consistent with corridor fences should be constructed.
- Enhanced wood fences are to be of redwood construction, and painted or stained in an earth tone color.

Standard Wood Fence

Standard wood fencing has the same design as an enhanced version, but does not incorporate decorative pilasters. This fence type is typically located adjacent to parks, paseos, or other areas with public views. Figure 12-34 illustrates a design detail for this fence type.

Guidelines for standard wood fences are:

- Typically constructed adjacent to parks, paseos, or other areas with public view.
- Minimum solid-wood fence height adjacent to parks is 6-feet.
- Fence sections may be 8 to 10-feet in length supported by a 4-by-4 post.
- Solid wood fences are to be of redwood construction, and if painted or stained, should be an earth tone color.

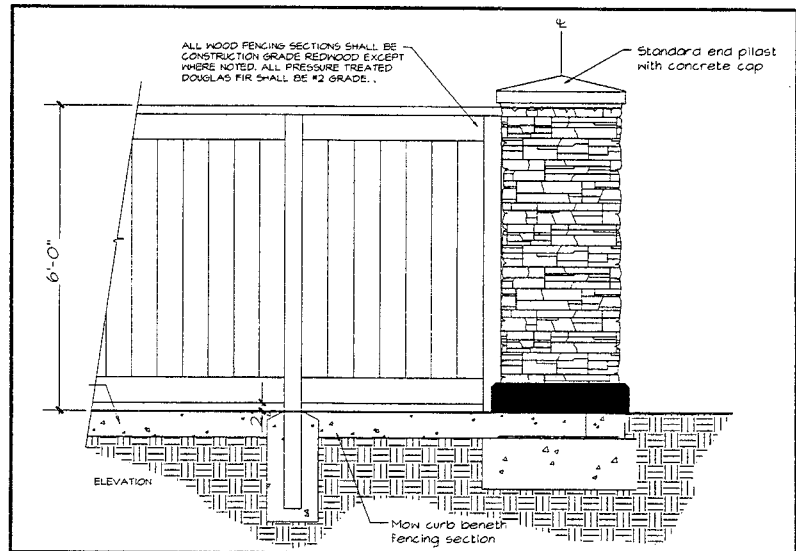


Figure 12-33: Enhanced Wood Fence Design Detail

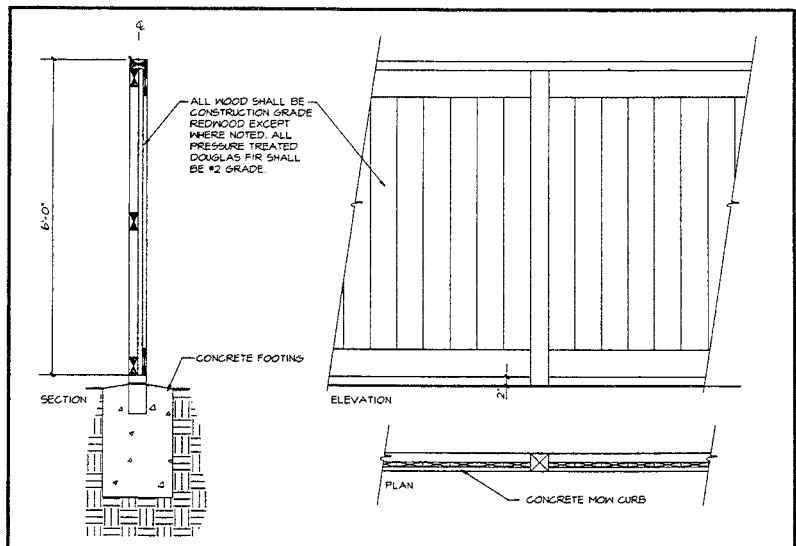


Figure 12-34: Standard Wood Fence Design Detail

12.2.6.3 Open Fencing

Open fences are intended to provide a visually transparent barrier at developed edges adjacent to open space parcels. Depending on the interface, open fencing may be used between open space areas and the rear and side property line of residential parcels, along a street adjacent to open space, or along pedestrian pathways at the edges of open space parcels. Open fences may also be used to separate different functions within landscape corridors (for example, to restrict access of dirt bikes and motorized vehicles) and at other miscellaneous locations within the plan area. Typical open fence sections (concrete rail and wrought-iron) are shown in Figures 12-35.

Fencing between Residential and Open Space

- Open fencing should be 4 to 6 feet in height and constructed of tubular steel or wrought iron.
- Brick or other masonry pilasters or columns may be used as an optional detail with tubular steel or wrought iron fences.
- Both sides of fencing are to be addressed aesthetically if they are visible from streets.
- Where residential lots back up to open space, knee walls with or without a tubular steel fence on top will be used. If tubular steel fencing is required on top of the knee wall, the top of the fence/wall combination shall not exceed 6-feet.

Other Fencing Conditions at Open Space

- Concrete rail or post-and-cable fencing should be used along the street edge adjacent to open space to restrict access of dirt bikes and motorized vehicles.
- Enhanced open fencing is required where visible from public streets or from residential lots across open space preserves.

12.2.6.4 Fence Breaks

Several opportunities exist in the Plan Area to provide pedestrian connectivity between residential and non-residential land uses (for example, a high-density residential complex adjacent to a commercial shopping center). Where pedestrian connectivity between dissimilar land uses is desired, breaks in walls or fencing is encouraged to improve pedestrian access between land uses.

Parcels that should incorporate fence breaks between land uses include:

- W-19 & W-50: Multi-family Residential and Neighborhood Park
- F-34 & F-55: Community Commercial and Regional Park
- F-20 and F-55: Multi-family Residential and Regional Park

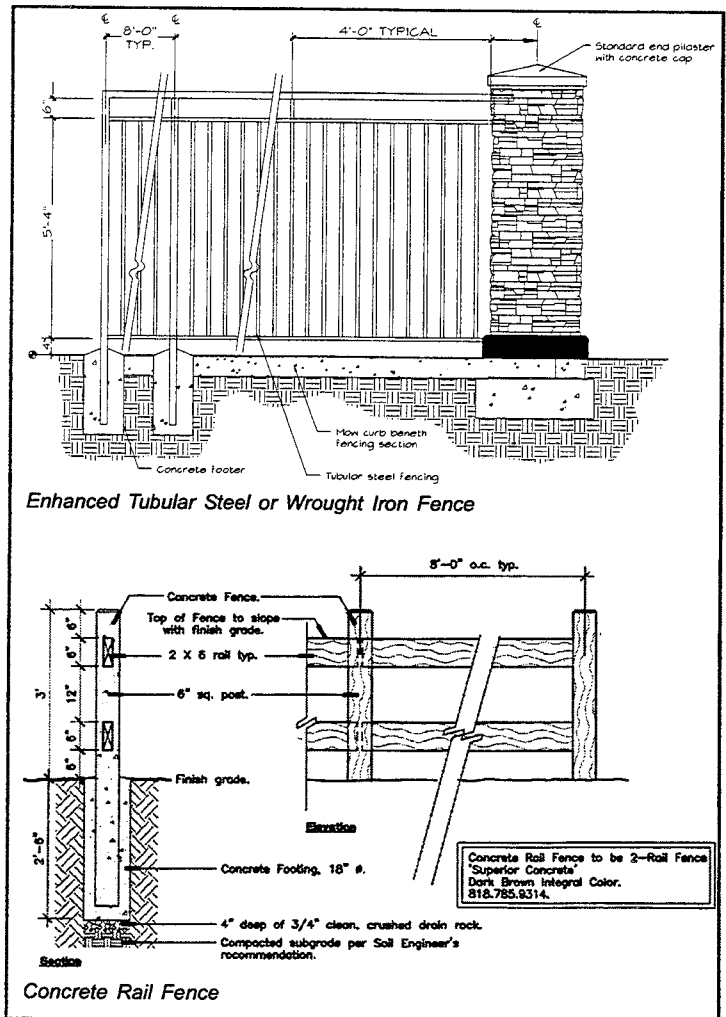


Figure 12-35: Open Fencing Options & Design Details

12.2.7 Street Lighting

All lighting should be themed appropriately within the development while maintaining an overall cohesiveness of the Plan Area. Decorative, "acorn" fixtures are required on all collector and residential streets. Decorative light fixtures are also encouraged on private streets within medium-density and high-density residential developments.

Unique and distinctive street light theming is required throughout the Village Center to distinguish it from the remainder of the Plan Area.

All decorative street lighting shall meet the street lighting standards established by Roseville Electric.

Figure 12-36 illustrates an appropriate design detail of an acorn-style light fixture. The height of light standard will vary depending on the application - along roadways standards are typically higher, and in pedestrian areas, standards are typically lower as illustrated.

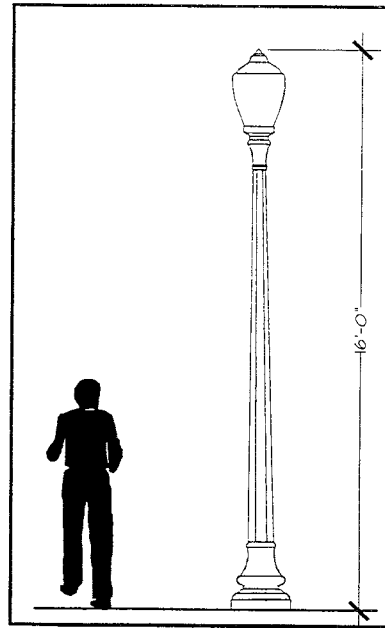


Figure 12-36:
Acorn-Style Lighting Detail

12.3.1 Village Center Interface

12.3.1.1 Intent of Edge Interface

Critical to the Village Center's successful integration into the Plan Area is creating a proper interface between it and the surrounding residential neighborhoods. The Village Center is bounded mostly by low-density residential parcels, which are typically designed with internally-oriented streets, limited connection points to streets outside the subdivision, and walls around the perimeter of the subdivision. This type of development pattern will not be permitted adjacent to the Village Center.

The intent of this section is to ensure that the residential neighborhoods adjacent to the Village Center do not turn their back to the Village Center through the conventional subdivision design described above. Instead, the residential neighborhoods surrounding the Village Center should provide a clear, open connection to this district, such that each subdivision's internal streets become a natural extension of the Village Center's internal street network.

This interface will add continuity to the street network and create a desirable interface between the different housing product types in and out of the Village Center. This will also promote walkability between the Village Center and its adjacent neighborhoods.

12.3.1.2 Residential Lotting Along Village Center Edge

Affected Parcels: W-1, W-2, W-7, W-8, W-10, W-11, & W-12

To achieve a lotting interface along the Village Center edges, as outlined in the design intent above, residential lotting shall front onto the Village Center's perimeter streets to the extent feasible. Although this is a requirement of residential units within the Village Center, siting of low-density residential lots outside the Village Center must have careful design considerations to ensure that the desired interface is created. The lotting orientation differs on the north and south sides of Pleasant Grove Blvd., due to the different street types. The lotting orientation design requirements described below are illustrated in Figure 12-38.

Lotting Orientation South of Pleasant Grove Blvd.

The Village Center's perimeter road south of Pleasant Grove Blvd. has two designations,

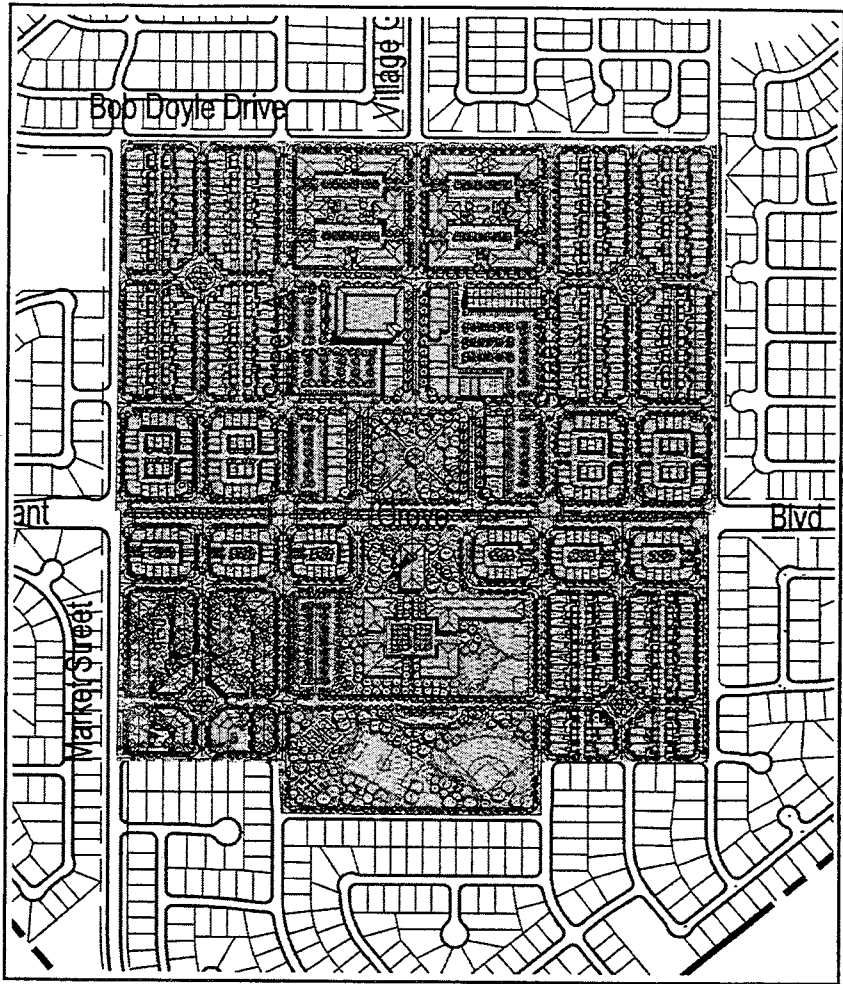


Figure 12-38: Low-Density Residential Lotting Interface with Village

depending on location. Where the street is designated as a Collector (Market Street), low-density residential lotting shall be consistent with the standards used north of Pleasant Grove (described below). Where the street is designated as a residential street, the following standards shall apply:

- Single-family lots shall front directly onto the perimeter street.
- Lots shall have driveway access off the perimeter street (or have access via alleys that extend from Village Center alleyways)
- Street access into the subdivision shall be provided at all locations where an extension from the Village Center street network can be made.

Lotting Orientation North of Pleasant Grove Blvd. and Cul-de-sac Interface with Village Center Streets

Because the Village Center's perimeter road north of Pleasant Grove is designated as a modified collector street, single-family residential lots would not normally have driveway access to this roadway. Therefore, to create the proper interface with the Village Center, the following standards shall guide the lotting design of low-density residential subdivisions adjacent to the

Village Center and north of Pleasant Grove Blvd.:

- Internal streets shall align with the planned internal street network within the Village Center
- Lots shall side on to the Village Center's perimeter street (Bob Doyle Drive), per Figure 12-38.

The streets in the neighborhoods surrounding the Village Center must align with the internal street network planned for the Village Center. In cases where this alignment is only visually achieved with a cul-de-sac, the following standards shall apply to ensure that the proper street interface is created:

- Each cul-de-sac shall abut the back edge of the landscape easement along Bob Doyle Drive or Market Street.

- The cul-de-sac and adjoining street shall remain visibly open to the Village Center streets.
- No solid fencing along the back edge of landscape easement shall be permitted between the face of each opposing house siding on to Bob Doyle Drive or Market Street.
- If fencing is needed in the "no fence" area for security purposes, the fence shall have an enhanced wrought iron design.
- No Primary street trees shall be permitted in the "no fence" area to maintain the desired visual connection to Village Center streets. Accent trees may be used if the visual connection is maintained.
- Figure 12-39 illustrates the proper cul-de-sac design interface with the Village Center's perimeter streets.

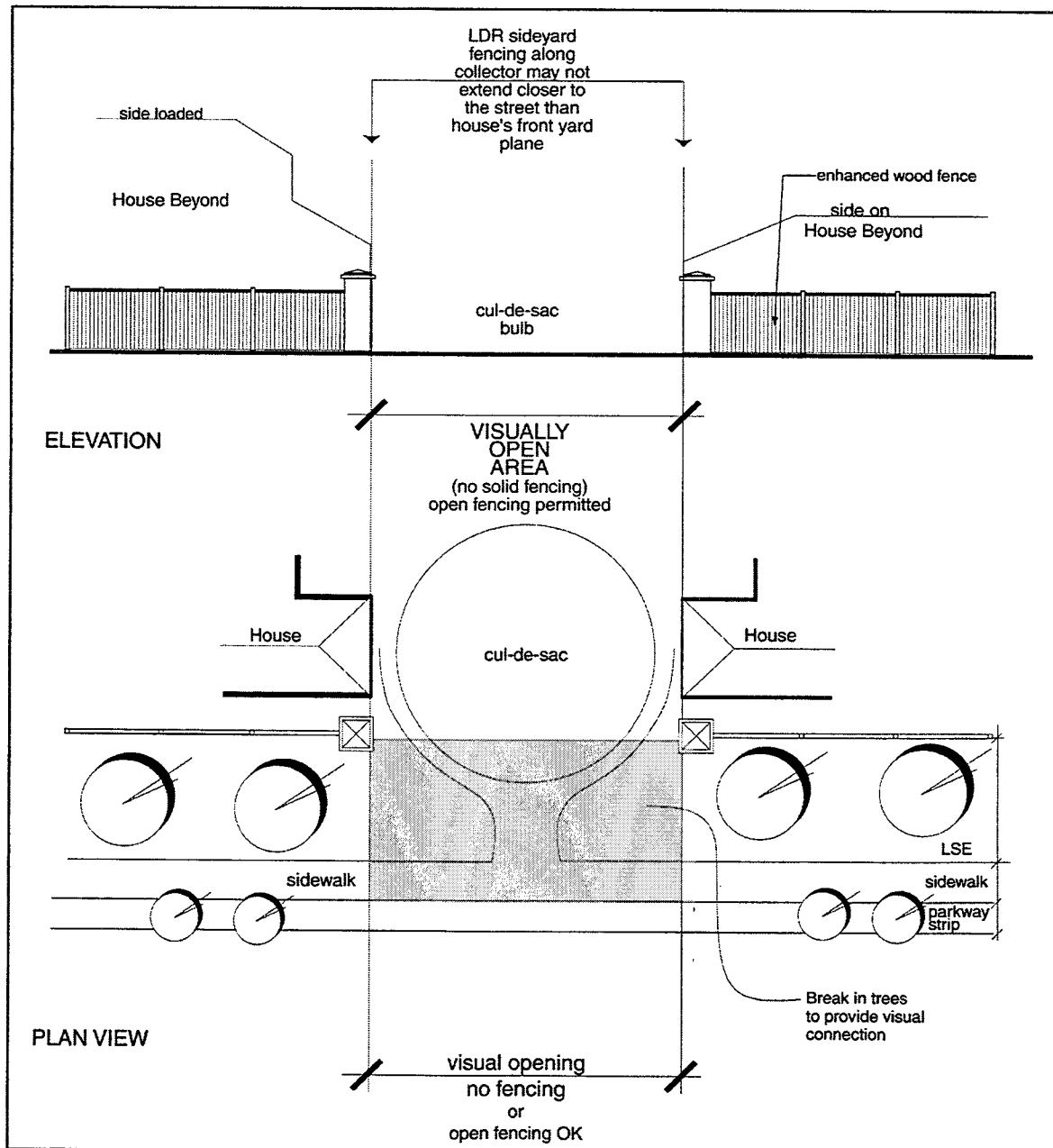


Figure 12-39: Cul-de-sac interface with Village Center Streets

12.3.2 Unique Land Use Interfaces

This section addresses specific parcels where careful design consideration must be given to site design and building layout to ensure that an appropriate interface with adjoining parcels is achieved.

12.3.2.1 Commercial/Business Professional Interface with Open Space

Affected Parcels: F-30 and W-30

Community Commercial and Business Professional sites adjacent to open space areas have unique opportunities to take advantage of this amenity in their site design and building orientation. Development of these parcels should encourage as much interaction between the Commercial and Open Space land uses by using the following guidelines to shape each project's design:

- Buildings shall not "turn their back" to the open space preserves.
- Buildings should be oriented to the open space areas to take advantage of outward views.
- Retaining walls along at the open space edge should not create a visual or physical barrier between the two land uses.
- Outdoor spaces should be incorporated into project design and include elements such as plazas, patios, outdoor seating, or gathering areas that are oriented to the open space and take advantage of viewsheds.
- Pedestrian access should be provided to the open space area, especially for connection to the adjacent Class I bike path.

12.3.2.2 Commercial Interface with Regional Park F-55 (Soccer Complex)

Affected Parcel: F-34

The site design and layout of the Community Commercial site adjacent to the soccer complex should provide a clear, distinct pedestrian connection between the two uses. Guidelines to ensure that this is accomplished include:

- Site design should ensure that buildings do not "turn their back" to the regional soccer complex facility
- Pedestrian connection points from the Commercial site should be provided to the park, which may include a fence/wall break, as described in the Fence

section.

- Site design should establish a clear visual connection between the Commercial and park sites, such that from the park, pedestrians can easily find access to these services without the need to drive from the soccer complex to the commercial site.

12.3.2.3 Multi-Family Residential Interface with Regional Soccer Complex

Affected Parcel: F-20

The intent of this section is to minimize the impacts associated with having a high-use, active park site adjacent to a residential site, particularly with respect to noise and light/off-site glare. To ensure that the site design and building orientation is sensitive to the adjacent regional soccer complex, the following guidelines shall be utilized:

- Site design should utilize the required parking and drive aisle fields as a setback buffer between the residential units and the park.
- Residential units should be oriented such that windows and balconies take advantage of local viewsheds to the park.
- Windows on residential units should be positioned to minimize off-site glare into the units created by park lighting.
- Solid fencing between the two uses is encouraged, including pedestrian pass-throughs to enable residents to have easy access to the park site.
- Figure 12-40 illustrates how these guidelines can be successfully implemented adjacent to the park.

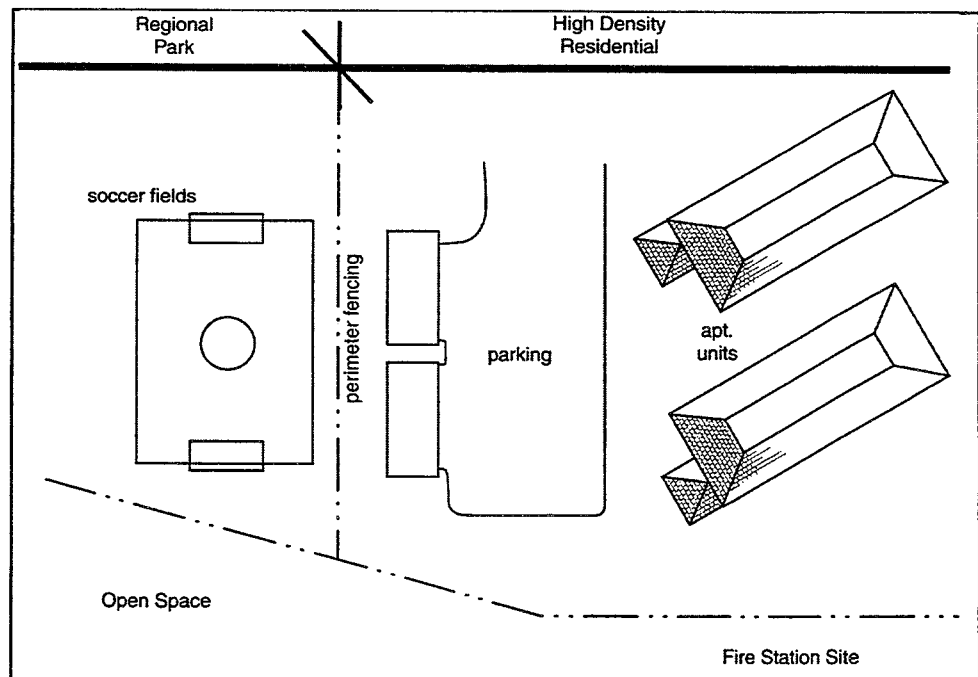


Figure 12-40: High-Density Residential Site Design Adjacent to Soccer Complex

12.3.2.4 Wastewater Treatment Plant Interface with Industrial Sites
Affected Parcels: W-60, W-61 & W-62

A special interface is needed for the site development of industrial properties to the west and south of the Pleasant Grove Wastewater Treatment Plant (WWTP). The purpose of addressing this interface is to ensure that these industrial sites are not negatively affected by the operations at the WWTP. Guidelines have been developed to address two primary goals. The first is to ensure that buildings are located as far from the WWTP as each site allows. The second goal is to provide a significant landscape barrier between each use. To meet these goals, the following guidelines shall steer the site, building, and landscape design of these parcels:

- Buildings should be oriented to streets and open space areas, as far away from the WWTP as possible.
- A landscape buffer consisting of a double-row of evergreen trees, planted a minimum 35' on-center in a staggered pattern shall be located along any property line shared with the WWTP.
- Tree species planted within the electric easement shall be subject to approval by Roseville Electric.
- Parking fields or storage yards shall provide a setback buffer between industrial buildings and WWTP landscape screening.
- Where two-story buildings are envisioned for light industrial and industrial sites adjacent to the WWTP, evergreen landscape screening between buildings, and the WWTP is strongly encouraged.
- Masonry walls shall be constructed on any property line shared between an industrial site and the WWTP. Walls should be planted with vigorous vines for screening.

- Retaining walls are to be avoided. If it does become necessary to install retaining walls, they shall be set back a minimum of 2' from the nearest paved surface.
- Figure 12-41 illustrates the intent of the guidelines above and illustrates how the guidelines should be implemented to achieve the proper interface and screening between these land uses.

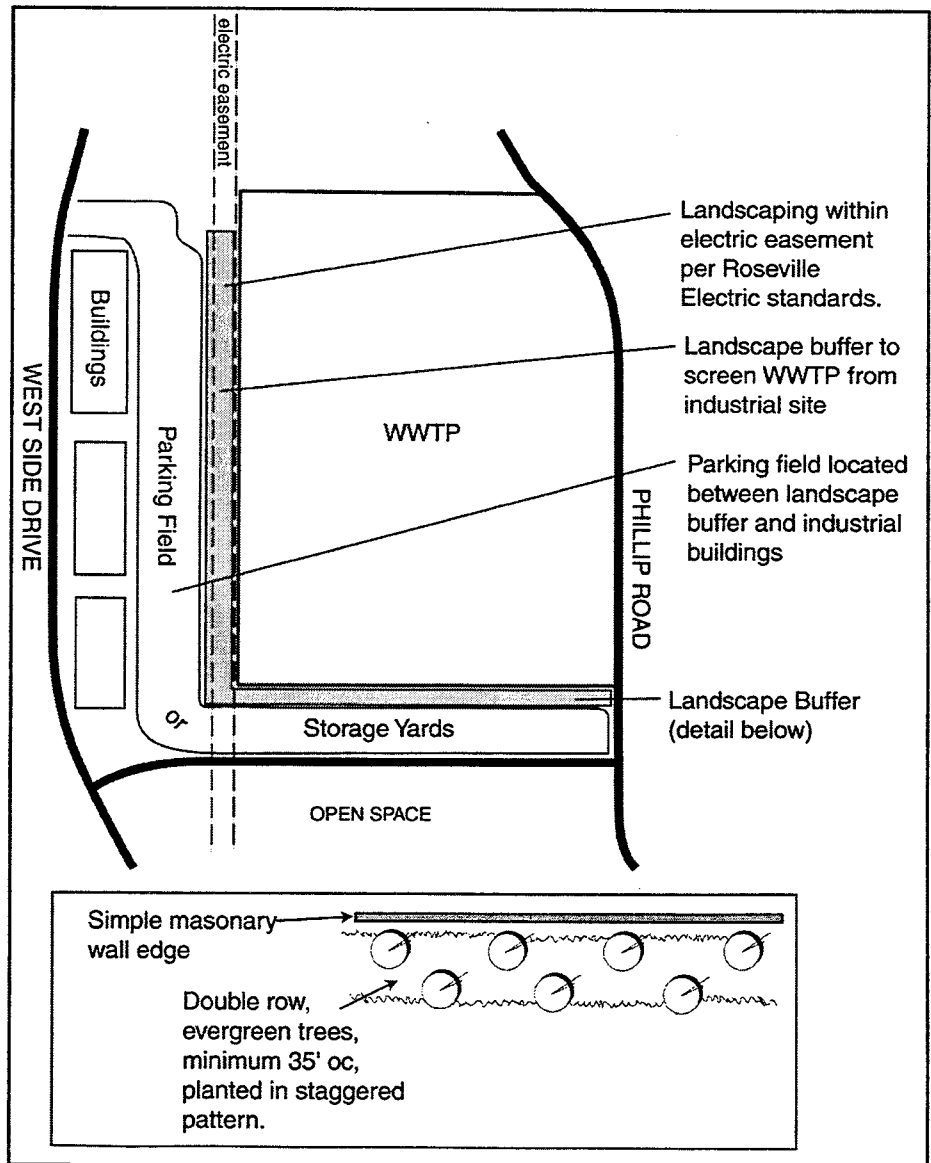


Figure 12-41: Wastewater Treatment Plant/Industrial Site Interface

12.3.2.5 Unique Residential Setback Conditions

Residential Setbacks from County Lands

Affected Parcels: F-13 & F-14

Agricultural-zoned land in Placer County abuts the plan area north of parcels F-13 and F-14 in Fiddlyment Ranch.

Historical agricultural use in this area has been grazing and dry-land grain farming. Tilling, spraying, and other intensive agricultural activities are not typical of this area. Due to this adjacent use, the following standards shall apply to these affected parcels:

- The separation between residential and agricultural uses shall be provided by a minimum 50-foot buffer to the living structure in all neighborhoods immediately abutting agricultural-zoned lands. The buffer includes any existing/future roads, landscape corridors, and rear yard setbacks.
- In the event that the agricultural land use is converted to a non-agricultural use adjacent to these parcels, the 50'-buffer shall not be required.
- Masonry fencing, consistent with the design standards in this document, is required between any residential parcel and agricultural-designated land. (A pre-fabricated panel wall is an acceptable alternative at this location.)

Residential Setbacks from West Side Drive

Affected Parcels: W-13, W-16, W-17, W-18 & W-19

Intent: Right of way for West Side Drive is being reserved for the ultimate construction of a six-lane arterial in the future. To ensure that residential land uses adjacent to this roadway have an appropriate setback, a special residential setback has been developed.

The required landscape corridor along a six-lane arterial in the WRSP is 50-feet. However along West Side Drive, the landscape easement is 35-feet with an additional 15-foot building setback required for all low and medium-density properties abutting West Side Drive. This 15-foot setback is additive to the normally-required setback for these properties (i.e. add 15' to the rear or side yard setback). This easement applies to all low-density and medium-density residential parcels located along West Side Drive. All high-density residential and non-residential land uses are required to have the standard 50-foot landscape setback.

Figure 12-42 illustrates the implementation of the landscape corridor and residential setback easement requirement.

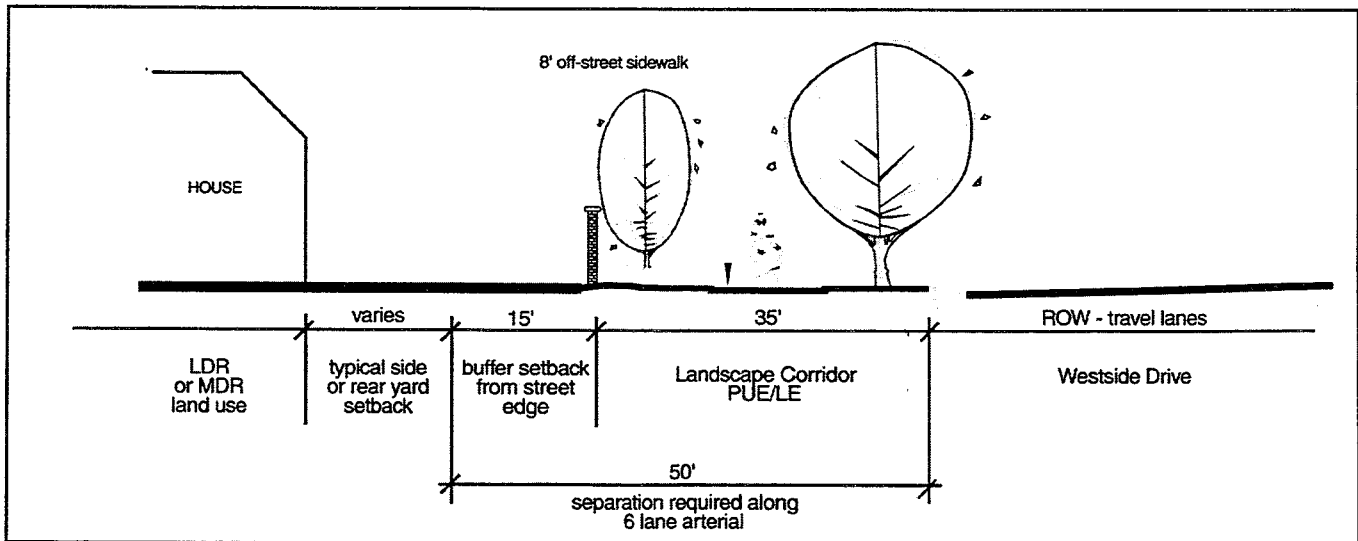


Figure 12-42: Landscape Corridor and Residential Setbacks along West Side Drive

12.3.2.6 Community Commercial & High Density Residential Landscape Setbacks from Hayden Parkway

Affected Parcels: F-34 and F-20

Hayden Parkway is a unique Collector Street in that it includes a 12'-wide landscaped median in addition to the standard 25' landscaped setback along the street edges. Typically landscape setbacks adjacent to High Density Residential and non-residential land uses would require a 35' setback. In consideration of the median landscaping provided along this street, Parcels F-34 and F-20 are permitted to maintain the 25' landscape setback along Hayden Parkway.

This condition is permitted with the provisions that:

- The median will not be broken up with a series of turn pockets that significantly reduce street landscaping, and
- A minimum landscape area is provided between the back of any future bus shelter and the adjacent parcels parking or developed area.

To ensure that an adequate amount of street landscaping is provided along Hayden Parkway adjacent to high-density residential and non-residential land uses, the following conditions shall apply:

- A minimum of 5-feet of landscaping shall be provided behind all bus shelters. This requirements can be met with landscaping from the adjacent parcel.
- Where bus shelters encroach into the landscape setback, a pedestrian walkway shall be provided from the sidewalk into the adjacent site.

12.3.3 Groundwater Well Sites

Interface with Adjacent Land Uses

Groundwater well sites are approximately 5,000 sq. ft. each and are located throughout the plan area. The goal of these guidelines is to ensure that each pump site blends in with the surrounding environment to the best extent possible, so it is not a noticeable feature in the landscape or streetscape. The following guidelines shall shape the design and construction of all well sites.

- Pump heads shall be enclosed in a building that conceals all equipment and provides adequate security of the facility.
- Pump house building shall have a split-face CMU (or similar) exterior with an earth-toned color that blends into the landscape.
- A combination of trees and shrubs, planted in small clusters, shall be located around the perimeter of the pump house building to blend it in with adjacent landscaping.
- Plant materials around the pump house building shall utilize the same plant palette as that used in any adjacent building development or roadway landscape corridor.
- The landscaping on the majority of the site shall blend in with landscaping on adjacent properties, such that the property line between the groundwater well site and any adjacent properties/landscape corridor is not discernable.
- Figure 12-43 illustrates the typical siting and landscape design for a groundwater well site.

12.3.4 Landscaping in Electric Easements/Powerline Corridors

Affected Parcels: F-17, F-24, F-31, F-35, F-30, F-65, F-84, F-85, W-60, W-61, W-62, W-83, W-29, W-87, & W-63

Landscaping in the powerline corridors throughout the Plan Area must be consistent with the landscape design illustrated earlier in this chapter. Special landscape design considerations must be employed within these corridors, in accordance with the specifications of Roseville Electric.

Landscaping in the powerline corridor shall include turf, shrubs, and trees that will be less than 15 feet in height at full growth. Plant materials must also be consistent with the approved plant palette contained herein for use in these areas.

Refer to Figures 12-2 and 12-47 for roadway sections that illustrate the landscape concept for powerline corridors.

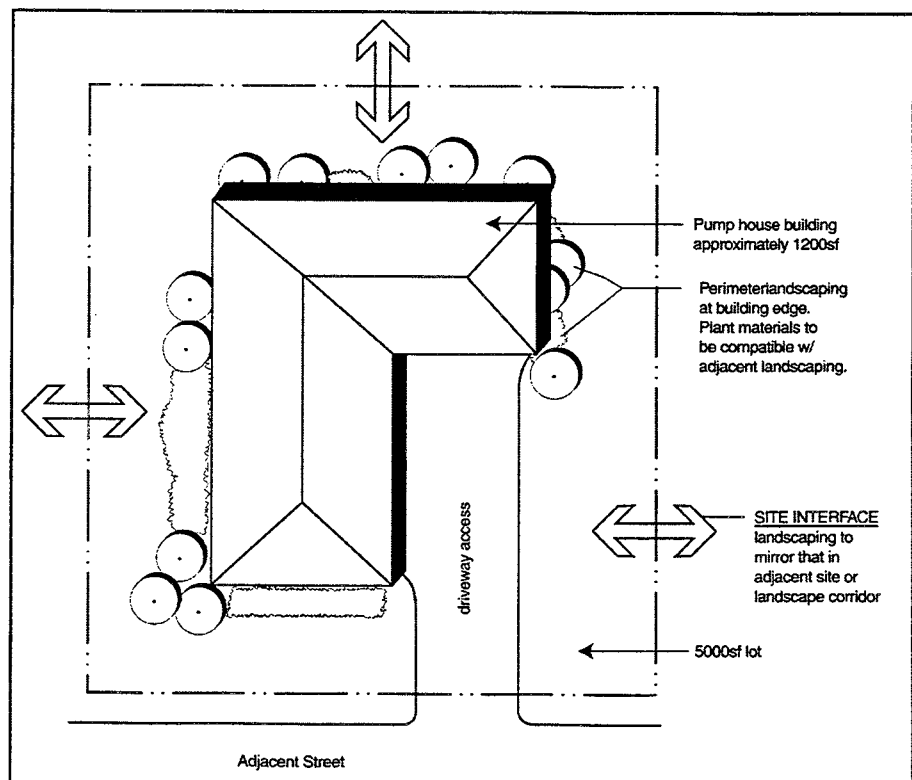


Figure 12-43: Groundwater Well Site Design Concept

12.3.5 Regional Parks Connectivity

Affected parcels: F-4 & F-5

The two regional parks are the centerpiece of the West Roseville Specific Plan. Because each park is separated by low-density residential parcels, it is important that design measures are employed to ensure that they appear as a single, cohesive unit within the Plan Area. To this end, an extensive land use planning effort was made to create a strong visual and "green" connection between these parks, thereby maintaining unity between these two elements.

The Regional Park linkage is created by the following elements:

- Construting a neighborhood park (F-53) that extends from the edge of Regional Park site F-54, into the residential neighborhoods.
- Providing an extra-wide landscape corridor along the entrance roadway from Hayden Parkway to Park F-53, between residential parcels F-4 & F-5.
- Providing an enhanced landscaped entrance driveway from Hayden Parkway to the High School/Soccer Complex site, mirroring the same design theme used between parcels F-4 & F-5.

These combined elements will provide a direct, visual landscape connection that links the two regional parks and provides a clear definition to this centerpiece in the Plan Area.

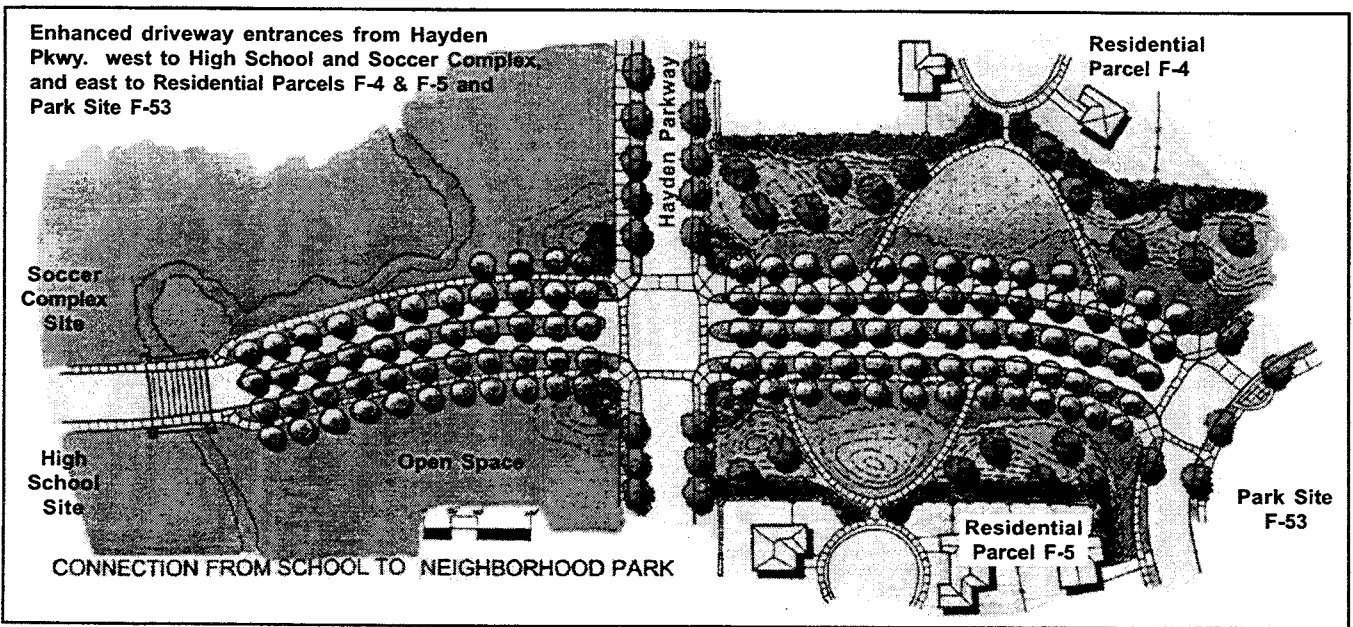


Figure 12-44: Regional Park Connection

12.3.6 Enhanced Bridge Design

All bridges constructed as part of the project must have a high-quality architectural design, enhanced with details, thematic stone cladding, decorative railing and lighting, and similar elements, consistent with the

design concept illustrated in Figure XX below. Articulation in columns and other architectural details is encouraged to give the bridge a three-dimensional appearance and to create strong and interesting shadow lines.

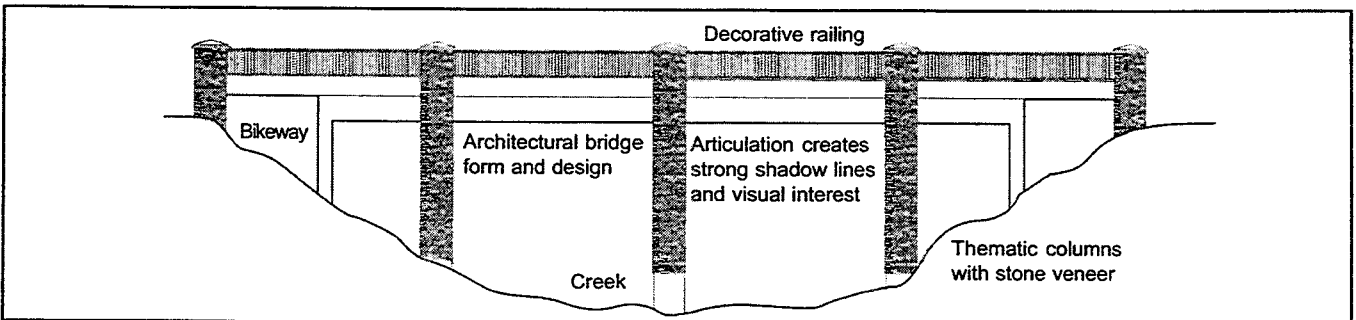


Figure 12-45: Enhanced Bridge Crossing

12.3.7 Landscape Transitions from Roadways to Open Space Areas

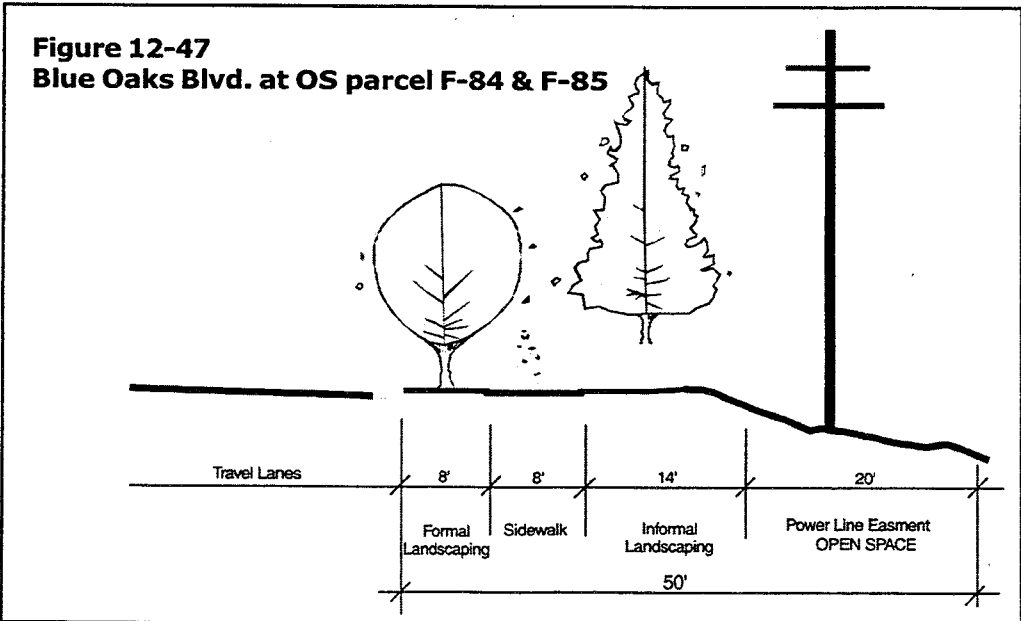
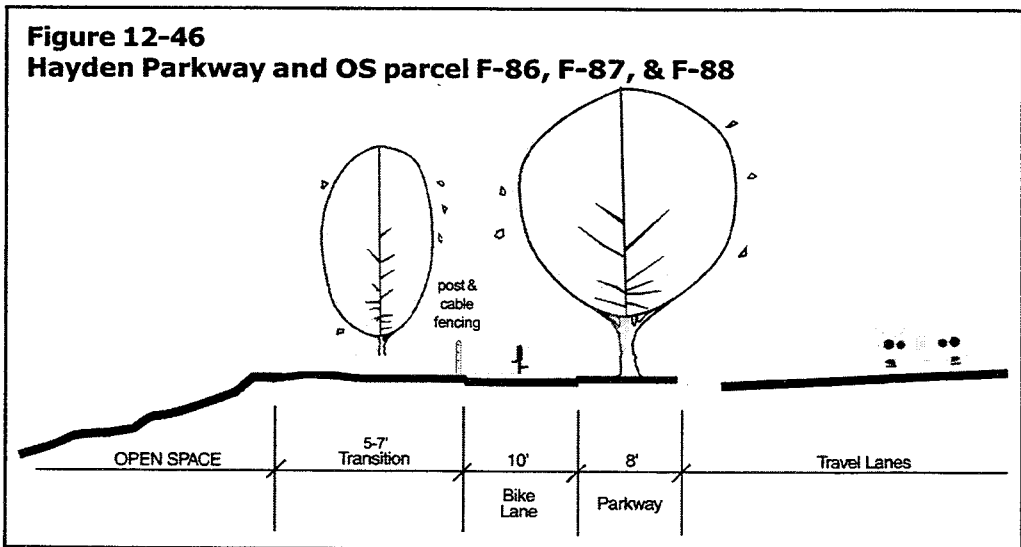
Affected Parcels: F-84m F-85, F-86, F-87, & F-88

A unique landscape treatment shall be utilized along streetscapes where roads are located next to Open Space corridors. The intent of this landscape concept is to provide an appropriate transition from the formal landscape treatment along the street edge to the natural landscape features of the open space preserve.

This special treatment applies to Blue Oaks Boulevard and Hayden Parkway adjacent to open space preserves.

The general concept for this transitional landscape treatment is:

- The strip between the street edge and the sidewalk, landscaping consists of the conventional formal appearance, with primary street trees spaced at regular intervals, and a combination of turf and groundcover beneath.
- The area behind the sidewalk adjacent to open space, the landscaping will have a more natural, informal form. Primary street trees will be utilized behind the sidewalk, but the plant materials and form of the groundcover will not have the same "clean cut" appearance of that along the street edge. Instead, the area behind the sidewalk should utilize evergreen, native-"looking" shrubs and groundcovers that are complimentary to the natural landscaping present in the Plan Area.
- Post and cable or concrete rail fencing shall be utilized behind the back of sidewalk to further define this transition.



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